



Office of the Clerk/Treasurer

W240N3065 Pewaukee Road
Pewaukee, WI 53072
(262) 691-0770 Fax 691-1798

**COMMON COUNCIL
MEETING NOTICE AND AGENDA**

Monday, January 15, 2024

6:30 PM

Common Council Chambers ~ Pewaukee City Hall
W240 N3065 Pewaukee Road ~ Pewaukee Wisconsin

-
1. Call to Order and Pledge of Allegiance
 2. Public Comment - Please limit your comments to two (2) minutes, if further time for discussion is needed please contact your District Alderperson prior to the meeting.
 3. Discussion and Possible Action Regarding Usage of Park Impact Fees for Final Design Work for All-Inclusive Playground and Splash Pad at the Pewaukee Sports Complex [Phalin / Fuchs].
 4. Discussion and Possible Action Regarding **Resolution 24-01-01** Waiving Any Conflict of Interest Between the Village of Lisbon and the City of Pewaukee Related to Future Provisions of Fire and Rescue Services [Klein]
 5. Discussion and Possible Action on Potential Approval of a Conditional Use Permit for Quattro Development / Higher Ground Education for Property Located on Highfield Court (PWC 096-994) for the Purpose of Constructing a Childcare Facility [Fuchs]
 6. Discussion and Possible Action Regarding **Resolution 24-01-02** Releasing a Forty-Five Foot Reserved Right-of-Way Restriction Upon Lots 1 and 2 of Certified Survey Map No. 10366 as Requested by Quattro Development Reservation [Fuchs]
 7. Discussion and Possible Action for Junior Cup Golf, Inc. Related to the Conditional Use Permit for Property Located at W229 N2494 Redford Boulevard (PWC 0915-990-001) for the Purpose of Running a Non-Profit Junior Golf and Athletics Center [Fuchs]
 8. **PUBLIC HEARING**, Discussion and Possible Action for a Comprehensive Master Plan Amendment to Change the Year 2050 Land Use/Transportation Plan Use Designation for the City of Pewaukee for Fox Run Development Partners, LLC for Property Located at N15 W22261 Watertown Road (PWC 0960-987) from Office / Commercial and Floodplains, Lowland and Upland Conservancy, and Other Natural Areas to Manufacturing / Fabrication / Warehousing and Floodplains, Lowland and Upland Conservancy, and Other Natural Areas Which Include the Adoption of **Resolution PC 24-01-03** and **Ordinance 24-01** if approved [Fuchs]
 9. Discussion and Possible Action Regarding **Ordinance 24-02** Rezoning the Property Located at N15 W22261 Watertown Road (PWC 0960-987) from Rs-1 Single-Family Residential and LC Lowland Conservancy to M-2 Limited Industrial and LC Lowland Conservancy for the Purpose of Developing an Industrial Flex Building as Requested by Fox Run Development Partners, LLC [Fuchs]
 10. Discussion and Possible Action Regarding a Certified Survey Map for Yench, LLC for Property

Located on the West Side of Yench Road (PWC 0885-996-006) for the Purpose of Subdividing the Existing Property Into Three Lots and One Outlot [Fuchs]

11. Discussion and Possible Action Regarding a Certified Survey Map for Yench, LLC for Property Located on the West Side of Yench Road (PWC 0885-996-006) for the Purpose of Further Subdividing the Property Into Two Lots [Fuchs]
12. Discussion and Possible Action Regarding **Ordinance 24-03** Revising Section 17.0500 of the City's Zoning Code Related to Review and Approval and Public Hearing Requirements for Conditional Use Permits [Fuchs]
13. Discussion and Possible Action Regarding **Ordinance 24-04** Revising and Codifying Chapter 17 Zoning as Chapter 340 of the City's Municipal Code [Fuchs]
14. Discussion and Possible Action to Adopt Revised Relocation **Resolution 24-01-04** Busse Road Bridge Replacement [Wagner].
15. Discussion and Possible Action Regarding the Gun Club Replacement Project Temporary Easement [Wagner].
16. Discussion and Possible Action Regarding the Accounts Payable Listing Dated January 15, 2024 [Tarczewski]
17. Public Comment - Please limit your comments to two (2) minutes, if further time for discussion is needed please contact your district Alderperson prior to the meeting.
18. Adjournment

Kelly Tarczewski
Clerk/Treasurer

January 12, 2024

NOTICE

It is possible that members of other governmental bodies of the municipality may be in attendance to gather information that may form a quorum. At the above stated meeting, no action will be taken by any governmental body other than the governmental body specifically referred to above in this notice.

Any person who has a qualifying disability under the Americans with Disabilities Act that requires the meeting or materials at the meeting to be in an accessible format must contact the Clerk/Treasurer, Kelly Tarczewski, at (262) 691-0770 three business days prior to the meeting so that arrangements may be made to accommodate your request.

**CITY OF PEWAUKEE
COMMON COUNCIL AGENDA ITEM 3.**

DATE: January 15, 2024

DEPARTMENT: Administration

PROVIDED BY: Nick Phalin

SUBJECT:

Discussion and Possible Action Regarding Usage of Park Impact Fees for Final Design Work for All-Inclusive Playground and Splash Pad at the Pewaukee Sports Complex [Phalin / Fuchs].

BACKGROUND:

The attached quote would complete final design of the all-inclusive playground and splash pad, including engineering, storm water management, utilities, accessibility and general contract work for the project.

FINANCIAL IMPACT:

The proposal is to utilize park impact fees for consultant fees. Current park impact fee balance is ~\$187,000 as of November 2023.

RECOMMENDED MOTION:

To approve use of park impact fees for consultant fees for final all-inclusive playground and splash pad design.

ATTACHMENTS:

Description

Parkitecture Final Design

December 19, 2023

Nick Phalin, CPRP
Director of Parks and Recreation
City of Pewaukee
W240N3065 Pewaukee Road
Pewaukee, WI 53072



Dear Nick:

Thank you for the opportunity to provide this proposal for continued assistance with the development of the new playscape at the Pewaukee Athletic Complex. We are very pleased with the outcome of the schematic design efforts and look forward to seeing this project completed. The following proposal outlines the design development and construction documentation tasks to get this to bidding.

The anticipated schedule would begin design development in early 2024 and position the City for a public bidding process in mid 2024. Assuming favorable bids, construction could begin in fall of 2024 for a completion in spring 2025. We will work with you and your staff to maintain this timeline and look forward to another successful project!

Thank you very much for this opportunity.

Sincerely,



Blake Theisen, PLA, ASLA
Principal

Parkitecture + Planning
901 Deming Way, Suite 201
Madison, WI 53717

E blake@parkitecture.org
P 608.886.6808

SCOPE OF SERVICES

Design Development

- ◆ Attend kick-off meeting with City team via Zoom or other web portal.
- ◆ Preparation of design development plans. Elements to be addressed include:
 - Site layout plans
 - Stormwater conveyance plans
 - Site grading plans
 - Erosion control plans
 - Landscape/restoration plans
 - Site lighting plans
 - Splashpad layout and P/E plans
 - Site utilities to include water and electric
 - Playground envelope plans
 - Hardscape construction details
- ◆ Coordination with manufacturers and selected vendors for project elements such as furnishings, lighting, splashpad equipment, playground equipment, etc.
- ◆ Develop opinion of probable construction costs.
- ◆ Collaborative design review meetings with the City
- ◆ Prepare 60% submittal (plans).

OUTCOMES		
Document Deliverables		
#1	Design Development Plans 60%	11x17 pdf
#2	Estimate of Probable Construction Costs (site elements)	8.5x11 pdf

Construction Documents

- ◆ Prepare construction bidding documents to include:
 - Site layout plans
 - Stormwater conveyance plans
 - Site grading plans
 - Erosion control plans
 - Landscape/restoration plans
 - Site lighting plans
 - Splashpad plans and details
 - Site utilities to include water and electric
 - Playground plans
 - Hardscape construction details
- ◆ Prepare specifications (Special Provisions)
- ◆ Prepare 90% and Final submittal (plans and specifications).
- ◆ Prepare & submit DNR WRAPP permit and local erosion control permit

- ◆ Attend (3) review meetings with design team and client.
- ◆ Post bid package on Quest online for public bid process
- ◆ Assist with answering contractor questions during bidding.
- ◆ Prepare addenda and RFI responses as necessary.
- ◆ Facilitate bid opening meeting and assist with bid analysis.

OUTCOMES		
Document Deliverables		
#1	Construction Plans 90%	11x17 pdf
#2	Construction Plans 100%	11x17 pdf
#3	Project Manual	8.5x11 pdf
#4	Permits as Outlined Above	8.5x11 pdf

Post Design Assistance

- ◆ Construction Administration
 - Attend pre-construction meeting with General Contractor and selected subs
 - Attend regular construction meetings (8-10 assumed)
 - Review submittals and shop drawings for design elements of the project
 - Monitor contractor compliance with issued permits and contract documents
 - Review contractor pay requests
 - Assist with splashpad commissioning
 - Complete final walk-through and punch list at completion of project
 - Provide closeout documentation at completion of project (contractor as-builts and O/M manuals)

OUTCOMES	
Meetings	
#1	Pre-Construction Meeting
#2 – 15	Bi-weekly Construction Meetings (13 anticipated)
#16	Attend Splashpad Commissioning
#17	Final walk-through & punch list
Construction Administration Documentation	
#1	Bid Tabulation & Recommendation Memo
#2	Construction Meeting Agendas, Meeting Minutes & Progress Photos
#3	Final walk-through punch list
#4	Project closeout documents (contractor as-builts and O/M Manuals)

Responsibilities of Owner and Others

The City shall supply Parkitecture with any relevant utility and/or development plans for the existing park and surrounding properties including the Phase 1 project if available. The City will provide an inspector for underground utilities, hardscape elevation verification, and other project elements as needed. Permit fees shall be paid by Parkitecture and submitted as a reimbursable expense.

Proposed Fee

For completion of the scope of services presented above, our proposed fees are lump sum by task.

Design Development	\$ 30,320
Construction Documents	\$ 36,416
Post Design Assistance	\$ 21,000
TOTAL	\$ 87,736

The fees identified above shall be fixed unless substantial changes in the scope of work occur. P+P shall notify Client of substantial changes to the agreed upon scope of work, and obtain approval prior to commencement of additional work. Refer to Terms and Conditions for billing and payment information.

Signature below shall serve as notification to proceed.

Accepted by:

Date:

City of Pewaukee

TERMS AND CONDITIONS

Payment Terms

Progress invoices will be sent monthly or as otherwise discussed for work completed; payment of invoices is due within 30 days after receipt. Parkitecture + Planning (P+P) shall reserve the right to stop work or withhold deliverables until payment is current.

Reimbursable Expenses

Reimbursable expenses will be billed at cost unless otherwise included in the fixed fee. Mileage shall be charged at the current Federal rate unless otherwise included in the fixed fee.

Ownership of Documents

Upon the making of final payment, as required by this Agreement, the Client shall assume ownership of the deliverables as described above. Use of the documents without further involvement of P+P shall be at the sole risk of the Client. The Client shall defend, indemnify and hold harmless P+P, sub-consultants, and the agents, officers, Principals, and employees of each from and against any and all claims, damages, losses, costs and expenses, including but not limited to attorney's fees, costs and expenses incurred in connection with any dispute resolution process, arising out of or resulting from such use of the documents.

Limitation of Liability

In recognition of the relative risks and benefits of the project to both the Client and P+P, the risks have been allocated such that Client agrees to attempt, to the fullest extent permitted by law, to limit the liability of P+P. This applies to any and all allegations, claims, losses, costs, damages of any nature, or claims expenses from any cause or causes, including attorneys' fees and costs and expert-witness fees and costs, so that the total aggregate liability of P+P shall not exceed the total of Insurance carried by P+P.

Authorization and Notice to Proceed

Signing of this document signifies an agreement to the fees and serves as authorization to commence work. If the terms in this fee proposal are acceptable, please sign and return one a signed copy to P+P.

Schedule

Work provided under this Agreement shall commence upon receipt of a signed copy of this document. P+P shall work with Client to meet specified deadlines within a reasonable expectation.

Client's Responsibilities

Client shall provide P+P with all relevant information for the project including but not limited to program statement, prior design files, subsurface exploration information, utility plans, current survey documents (AutoCAD format), and any other past planning documents. Client will review incremental progress documents and provide feedback to P+P in a timely manner. Client shall bear responsibility for any costs and or loses arising from discovery of unforeseen conditions or inaccuracies of existing condition documents.

**CITY OF PEWAUKEE
COMMON COUNCIL AGENDA ITEM 4.**

DATE: January 15, 2024

DEPARTMENT: Clerk/Treasurer

PROVIDED BY:

SUBJECT:

Discussion and Possible Action Regarding **Resolution 24-01-01** Waiving Any Conflict of Interest Between the Village of Lisbon and the City of Pewaukee Related to Future Provisions of Fire and Rescue Services [Klein]

BACKGROUND:

FINANCIAL IMPACT:

RECOMMENDED MOTION:

ATTACHMENTS:

Description

Resolution 24-01-01

STATE OF WISCONSIN CITY OF PEWAUKEE WAUKESHA COUNTY
RESOLUTION 24-01-01
RESOLUTION WAIVING CONFLICT OF INTEREST
BETWEEN THE VILLAGE OF LISBON AND THE CITY OF PEWAUKEE

WHEREAS, the Village of Lisbon (“Village”) and the City of Pewaukee (“City”) are both represented by the law firm of MUNICIPAL LAW & LITIGATION GROUP, S.C.; and

WHEREAS, the Village and City have engaged in discussions related to future provision of fire and rescue services; and,

WHEREAS, the Village and City wish to negotiate contract establishing terms and conditions for the City to provide fire and rescue services to the Village; and,

WHEREAS, both the Village and City wish to continue to receive legal advice about the agreements from MUNICIPAL LAW & LITIGATION GROUP, S.C.; and

WHEREAS, MUNICIPAL LAW & LITIGATION GROUP, S.C., has notified the City that the firm cannot represent both the Village's and the City's interest in part because of Supreme Court Rule 20:1.7, which provides that a lawyer shall not represent a client if the representation of that client will be directly adverse to another client unless the lawyer reasonably believes the representation will not adversely affect the relationship with the other client and each client consents in writing, after consultation; and

WHEREAS, MUNICIPAL LAW & LITIGATION GROUP, S.C., has consulted with the City and advised the City that its interests could be materially impacted by such an agreement.

NOW, THEREFORE, BE IT RESOLVED, the Common Council of the City of Pewaukee hereby consents to the City Attorneys, MUNICIPAL LAW & LITIGATION GROUP, S.C., representing both the Village of Lisbon and City of Pewaukee in their discussions and draft of agreements related to fire and rescue services.

EFFECTIVE DATE.

This resolution shall take effect immediately upon passage and posting or publication as provided by law.

DATED THIS ____ DAY OF _____, 2024

CITY OF PEWAUKEE

By: _____
Steve Bierce, Mayor

Attest: _____
Kelly Tarczewski, Clerk

**CITY OF PEWAUKEE
COMMON COUNCIL AGENDA ITEM 5.**

DATE: January 15, 2024

DEPARTMENT: Planning

PROVIDED BY: Nick Fuchs

SUBJECT:

Discussion and Possible Action on Potential Approval of a Conditional Use Permit for Quattro Development / Higher Ground Education for Property Located on Highfield Court (PWC 096-994) for the Purpose of Constructing a Childcare Facility [Fuchs]

BACKGROUND:

At their December 21, 2023, meeting, the Plan Commission held a public hearing and unanimously recommended approval of the Conditional Use Permit for Quattro Development/Higher Ground Education to construct and operate a childcare facility upon property bearing Tax Key No. 0906994.

The Plan Commission recommended conditions of approval can be found within the attached draft Conditional Use Permit.

The Plan Commission also approved the Site and Building Plans for the proposed development.

FINANCIAL IMPACT:

RECOMMENDED MOTION:

A motion to approve the Conditional Use Permit for the Higher Ground Education Guidepost Montessori School, subject to the conditions within the draft Conditional Use Permit.

ATTACHMENTS:

Description

Quattro Development draft Conditional Use Permit

Quattro Development staff report 12.21.23

Quattro Development staff report 2.16.23

Higher Ground project narrative

Higher Ground response comments

Quattro Development draft Resolution

Civil_Architectural Plans

TIA Alternative Routes Memo

**A COVENANT
REGARDING THE ISSUANCE OF A
CONDITIONAL USE PERMIT
BY THE
CITY OF PEWAUKEE**

**TAX KEY NUMBER(S)
OR PARCEL(S) INVOLVED:** **CONDITIONAL USE
PERMIT:**
NO. CUP-23-12-1

PWC 0906994

LEGAL DESCRIPTION:

LOT 2 CERT SURV 10366 VOL 98/203 2.00 AC PT NW1/4 SEC 11
T7N R19E DOC# 3482373

**PERSON(S), AGENT(S) OR CORPORATION(S) PETITIONING
FOR PERMIT:**

Quattro Development/Higher Ground Education

Recording area

Name & Return Address

City of Pewaukee
W240N3065 Pewaukee Rd
Pewaukee, WI 53072

WHEREAS, It is understood by all parties to this covenant that Section 62.23 of WIS. Statutes prescribes the legal basis for the granting of a conditional use permit by a City and Chapter 17 of the City Codes and Ordinances provides for the issuance of such permits as well as the standards by which all such uses will be measured; and,

WHEREAS, The City Plan Commission has held a public hearing on December 21, 2023; has reviewed the various elements of the petitioner's proposal; and has recommended that a Conditional Use Permit be granted to the above-named petitioner for the property/parcel identified above; and,

WHEREAS, The City Common Council, at their meeting held on January 15, 2024, concurred with the Plan Commission.

NOW, THEREFORE, let it be known that the City Common Council, by its action on January 15, 2024 has, hereby, granted a Conditional Use Permit for the following use(s):

Construction of a childcare facility.

FURTHER, such approved use of the above designated parcel(s) are hereby allowed based on the following conditions being continually met:

1. Final grading, erosion control, utilities, and storm water management plans shall be approved by the Engineering Department, prior to any land disturbing activities.
2. The applicant shall not be responsible for the construction of a bell tower as conditioned within Conditional Use Permit No. COP-05-10-3.
3. The applicant shall include, within their orientation materials, directions and standards for entering and exiting the school that encourages traffic to not cut through the subdivision to the north, with school staff following these same guidelines.
4. Hours of operation shall be limited to 7:00 a.m. to 6:00 p.m. Monday through Friday.
5. No buses shall be utilized for the operation of this use.
6. Signage and striping restricting left turns from Highfield Court onto Highfield Drive shall be included to prohibit traffic entering the subdivision to the north.
7. A revised Landscape Plan shall be submitted, for City Planner review and approval, that provides additional plantings along Highfield Court and to further screen the view of the dumpster enclosure.
8. Common Council may revise the Conditional Use Permit if parking becomes non-compliant with City Zoning Code or becomes a nuisance pursuant to Chapter 7 of the City Code.

The parties hereto, namely the City of Pewaukee and the Equitable Owner of the property for which this conditional use has been sought, set their signatures or the signatures of their representatives below, thereby agreeing to the provisions and conditions set forth in this covenant.

Attest:

Signature of equitable owner

Date

Kelly Tarczewski
City Clerk

Steve Bierce
Mayor, City of Pewaukee

Date

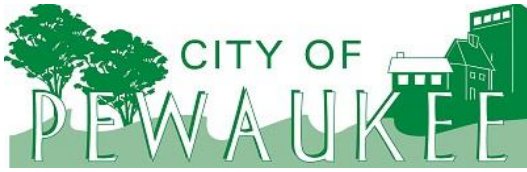
State of Wisconsin
County of Waukesha

Signed or attested before me on _____, 2024 by Steve Bierce, Mayor and Kelly Tarczewski, Clerk.

(Seal)

Ami Hurd
My Commission expires _____

This instrument was drafted by Ami Hurd, Deputy Clerk



Office of the Planner & Community Development Director
W240 N3065 Pewaukee Road
Pewaukee, Wisconsin 53072
Phone (262) 691-0770 Fax (262) 691-1798
fuchs@pewaukee.wi.us

REPORT TO THE PLAN COMMISSION

Meeting of December 21, 2023

Date: December 8, 2023

Project Name: Guidepost Montessori Conditional Use and Site & Building Plan Review

Project Address/Tax Key No.: Highfield Court/PWC 0906994

Applicant: Quattro Development, LLC

Property Owner: KB Pewaukee, LLC

Current Zoning: B-6 Mixed Use Business

2050 Land Use Map Designation: Office/Commercial

Use of Surrounding Properties: Capitol Dr to the south, Highfield Ct to the north, a daycare to the east and bank to the west.

Background

The applicant, Quattro Development LLC, submitted a Conditional Use Application and Site & Building Plans for a proposed childcare facility on a mostly vacant lot located along Highfield Court and Capitol Drive between HWY 164 and Highfield Road.

This project was previously reviewed by the Plan Commission and Common Council and denied due primarily to traffic concerns.

Following denial of the Conditional Use Application, the applicant correctly pointed out to staff that per the Zoning Code, the public hearing should have been held by the Plan Commission, not the Common Council.

As such, the application has been re-noticed for public hearing at the December 21, 2023 Plan Commission meeting. There have not been any changes to the application or plans previously provided by the applicant. Those same plans are attached for Plan Commission review.

Below are excerpts from meeting minutes related to this proposal in order to provide a complete history of this project. Also attached are previous staff reports.

History

February 16, 2023 Plan Commission: Discussion and Action Regarding a Recommendation to the Common Council for a Conditional Use Permit and Site and Building Plans for Quattro Development/Higher Ground Education for Property Located on Highfield Court for the Purpose of Constructing a Childcare Facility (PWC 0906994)

Meeting cancelled due to weather.

March 6, 2023 Common Council: PUBLIC HEARING, Discussion and Possible Action Regarding a Conditional Use Permit for Quattro Development/Higher Ground Education for Property Located on Highfield Court for the Purpose of Constructing a Childcare Facility (PWC 0906-994)

Mr. Fuchs stated this item was supposed to be discussed at the March Plan Commission meeting, but it was cancelled. He asked the Council to table this item to the April 3rd Common Council meeting.

March 16, 2023 Plan Commission: Discussion and Action Regarding a Recommendation to the Common Council for a Conditional Use Permit and Site and Building Plans for Quattro Development/Higher Ground Education for Property Located on Highfield Court for the Purpose of Constructing a Childcare Facility (PWC 0906994)

Mr. Fuchs stated the request is for an 11,836 square foot Montessori school and includes two playground areas and a fence adjacent to Capitol Drive. Staff recommended that fencing be on the north side, which is the Highfield Court side of the property. There is also a 45-footright-of-way reservation, and the applicant is requesting the City remove that reservation. Mr. Fuchs added that there is a condition related to the construction of a bell tower. There was a requirement for the property to the west to construct a bell tower within a certain time frame, and the applicant would like that responsibility removed from the previous Conditional Use Permit. The last condition is the standard approval of the final grading, erosion control, and stormwater by the Engineering Department.

It was pointed out that this would be directly next door to the current childcare center on that road.

The applicant noted the classrooms exit directly to the playground, so it was critical to keep the distance down that the kids must travel between the playground and the classroom. He stated they would be increasing the landscape buffer on the south side so that it would be heavily screened from Capitol Drive. The fencing around the perimeter of the playgrounds would be wrought iron, and the dividing fences between each portion of the playground would be PVC vinyl fencing.

The applicant also pointed out that drop offs in the morning and pick ups in the afternoon are over a two-to three-hour period, so it is not like a school where everyone is funneling in and out at one time.

Commissioner Brown was concerned about having two daycares right next to each other in a residential subdivision.

Brian Schultz (N36W23835 Broken Hill Circle South) stated they have seen an increase in traffic since the first daycare was built and he felt people would cut through their neighborhood.

Greg Phelps (N37 W23840 Broken Hill Circle South) was concerned that the entrance and exits would funnel cars into their street. The likelihood of someone being injured is increased because of the volume of traffic.

Further discussion took place regarding traffic patterns in the area. Chairman Bierce felt a traffic study would need to be done, and the applicant stated they had no problem doing that.

Ms. Wagner stated she had a condition to add that there must be separate laterals on the property. It is currently set up as a shared lateral because it was originally one lot, but once the CSM split the property into two, it became a requirement to have separate laterals.

The applicant confirmed that they would have a traffic study completed and would request to be put on a future agenda.

April 3, 2023 Common Council: PUBLIC HEARING, Discussion and Possible Action Regarding a Conditional Use Permit for Quattro Development/Higher Ground Education for Property Located on Highfield Court (PWC 0906-994) for the Purpose of Constructing a Childcare Facility [Fuchs]

Postpone to 5/1/2023

May 18, 2023 Plan Commission: Discussion and Action Regarding a Recommendation to the Common Council for a Conditional Use Permit and Site and Building Plans for Quattro Development/Higher Ground Education for Property Located on Highfield Court for the Purpose of Constructing a Childcare Facility (PWC 0906994)

Discussion and Action Regarding a Recommendation to the Common Council for a Conditional Use Permit and Site and Building Plans for Quattro Development/Higher Ground Education for Property Located on Highfield Court for the Purpose of Constructing a Childcare Facility (PWC 0906994)

Mr. Fuchs stated this item was before the Plan Commission at the March 16th meeting and was tabled following concerns from the Broken Hill subdivision residents regarding traffic. The school has a maximum occupancy of 152 children and a maximum of 15 full-time employees. The site is approximately two acres, and the single-story building is approximately 11,836 square feet. There are two playgrounds, one on the south side adjacent to Capitol Drive and one on the north side adjacent to Highfield Court. Both are fenced in with a six-foot wrought iron fence. Mr. Fuchs noted there was a condition listed in the staff report to locate the playground areas and any other accessory structure or amenities on the north side of the building, as opposed to being visible from Capitol Drive. He noted that the daycare to the east does have a fence adjacent to Capitol Drive. Mr. Fuchs stated the applicant did provide a traffic impact analysis, and he recommended approval of the Conditional Use and Site and Building Plans.

Discussion took place regarding the traffic impact analysis.

Commissioner Brown felt there was a need for this in the area, and she did like the look of the building, but she felt the location was not right, especially considering there is already a daycare right next door.

John Bieberitz with Traffic Analysis and Design stated he prepared the traffic study, and he gave some background about the report and broke down the information contained in the report.

The Commissioners were in agreement that they were concerned about the traffic situation in the area and could not get onboard with this development.

A motion was made and seconded (C. Brown, D. Linsmeier) to recommend denial of the Conditional Use Permit and Site and Building Plans. Motion Passed: 5-For, 0-Against.

June 5, 2023 Common Council: PUBLIC HEARING, Discussion and Possible Action Regarding a Conditional Use Permit for Quattro Development/Higher Ground Education for Property Located on Highfield Court (PWC 0906-994) for the Purpose of Constructing a Childcare Facility - Applicant Requests Item to be Tabled [Fuchs]

Mr. Fuchs stated the petitioner wishes to have this item tabled. They intend to submit new plans and there is no anticipated date for that as of yet.

September 5, 2023 Common Council: PUBLIC HEARING, Discussion and Possible Action Regarding a Conditional Use Permit for Quattro Development/Higher Ground Education for the Property Located on Highfield Court (PWC 0906-994) for the Purpose of Constructing a Childcare Facility [Fuchs]

Mr. Fuchs stated this was a Conditional Use application for a Montessori school within a B-6 district. He said they are proposing a maximum capacity of 152 students and the hours of operation will be from 7:00 a.m. to 6:00 p.m., Monday through Friday. He said the applicant indicated none of the students will be bused to their facility. Mr. Fuchs stated they originally went before the Plan Commission on March 16th and the neighbors within the Broken Hill Subdivision indicated they had concerns regarding traffic. He said the Plan Commission had the same concerns and asked the applicant to complete a traffic study. Mr. Fuchs stated the applicant completed the traffic study and came back to the Plan Commission on May 18th to present the results of the traffic study, and Plan Commission continued to have concerns with traffic and recommended denial of the Conditional Use Permit. He said the applicant is coming back to the Council requesting approval and is providing a memo summarizing the traffic study.

Attorney Martell noted that Mayor Bierce and Alderperson Brown are on the Plan Commission and have heard the proposal and details of the project. He asked that they be fair and impartial based on what is being heard tonight. Attorney Martell added it is Quasi-Judicial and the decision is entirely made on what is presented at this meeting. He said under §62.23 there is substantial evidence standard, and decisions must be based on substantial evidence. He reiterated the City's code Section 17.0501 stating the purpose and intent are found not to be hazardous, harmful, offensive, or otherwise adverse to the environment or the value of the neighborhood or community.

Mayor Bierce stated Plan Commission meetings discuss more than just the Conditional Use Permit, and they talk about landscaping, lighting, building appearance and general conditions. He asked if this public hearing was strictly on the Conditional Use Permit and asked if they can discuss any other areas of the development. Attorney Martell stated if they are general conditions that would normally be discussed they can be addressed, but you can't bring in what was discussed previously. Mayor Bierce asked if they would have to go back to Plan Commission for approval or if they are able to build it the way they want if it goes in the developers favor and no one weighed in on lighting or the building at Plan Commission meeting. Mr. Fuchs stated based on the Plan Commission motion the applicant would have to go back to Plan Commission for review.

Mayor Bierce opened the public hearing.

Brett Hutter (N37 W23820 Broken Hill Circle South) said he has seen an increase in traffic since the initial daycare went in a couple of years ago. He added there are a large amount of dump trucks cutting through his subdivision to get to the development on Swan Boulevard and Highway 164. He said the traffic light is difficult, and his concerns are with the speed and traffic pattern.

Shannon Myhre (N36 W23526 Oak Hill Lane) said she is concerned with the traffic at the intersection of Highfield Court and Capitol Drive. She said it's not only the volume and complexity of the intersection but the blind hill. She said it is a mess and congested during certain hours of the day. She said the use will cause an extensive traffic increase and during a condensed time frame 150 cars going through that intersection will be very hazardous. She said she hopes the Council votes no.

Rob Myhre (N36 W23526 Oak Hill Lane) said cars are always stacked up and it's always a mess. He said he has written to the State, County and the City and feels there needs to be a light. He said he can't believe the City would even consider this. He feels it is one of the worst plans he has ever heard and asked the Common Council to listen to the residents.

Mayor Bierce asked if it is a City rule or State rule that Council can't consider Plan Commission. Attorney Martell said it is a State rule and how all Quasi-Judicial hearings operate.

Jill Schultz (N36 W23835 Broken Hill South) said she has seen a massive increase in traffic since KinderCare has gone in. She said there is a bus stop in there and the amount of speed going through is dangerous. She said there are many days when cars pass the bus, and traffic is a big concern.

Tom Gardner with Michael Best representing the applicant, noted that during the Plan Commission meeting there was a concern about bus traffic. The applicant proposed that the early childhood school would not use buses to transport students to and from the school.

Brett Dalman with Quattro Development stated the property has its limitations and is not very large. He said he is aware of the childcare center next door and noted the Montessori School has a completely different type of childcare education than KinderCare. He added that he understands the traffic concerns and has followed up with a report to try and compare it to other uses that could go on that property.

John Bieberitz with Traffic Analysis and Design stated he did the original traffic study. He said what is being proposed is not the highest volume generated use for the site. He said comparing other uses such

as a retail store, pharmacy or drive through restaurant would generate more traffic than what is being proposed. He said this is a Monday through Friday operation from 7 a.m. to 6 p.m. and the traffic volume would be a three-hour window in the morning and three hour window in the evening. He added there is no specific hard es start or end time like a traditional school, and you will not see a big peak at drop off or pick up.

Mayor Bierce asked about the morning peak on the study. Mr. Bieberitz said it was based on traditional rush hour traffic from 7 a.m.to 8 a.m. and they tried to represent the worst-case, highest volume of traffic on Capitol Drive.

Mr. Dziwulski asked how they determined the number of ins and outs. Mr. Bieberitz stated it is based on national standards and the ITE Trip Generation Manual, which is a combination of traffic studies done throughout the country at day care facilities and Montessori schools. Mr. Dziwulski stated that is an average and he questioned who's to say it wouldn't be higher. Mr. Bieberitz stated based off start and end times, drop off and pickups are spread out.

Discussion took place regarding peak time and cars per minute.

Ms. Brown said she hasn't checked to see if the DOT has the same numbers as the traffic study. She said according to the exhibit in the study it shows an increase in traffic on Capitol Drive and Highway 164 from 2018-2023. She said she finds it interesting that there is a high volume on these roads but was dumbfounded by the 24-hour counts. She said to add a little bit here and there seems minimal on heavy roads and the concern is keeping them out of the subdivision. Ms. Brown said she has heard complaints that there are not enough day cares in the area, but she struggles with the issue of increasing traffic in a subdivision and feels signage and increased officers are needed. Ms. Brown stated she feels Capitol Drive and Highway 164 can handle the traffic increase, but it's all about keeping traffic out of the subdivision.

A brief discussion took place regarding items to slow traffic down and the importance of enforcement.

Matt Knopf from Higher Ground said he hears the residents' complaints and concerns. He said operationally, all parents get an orientation package and specifics could be put in it regarding traffic flow.

Mayor Bierce asked what would be considered a bus and the age of children that are accepted. Mr. Knopf stated they do not bus children in and out to aftercare. He said it is not in their business model and noted Montessori Schools are different than standard schools. He added the children that are accepted are infants through the age of six. Mr. Knopf has a growth model and if there becomes a need in a community for an elementary school and they feel their families can support it, they will look for a location in the community.

Ms. Brown asked why they chose this property when there are larger properties available. Mr. Knopf stated although there are challenges to the site, the marketing team does extensive demographic studies and looks at what they call a magic mile number. This location met all those requirements and factors. Ms. Brown asked if the high traffic and congestion could hurt the business. Mr. Knopf said operationally Montessori allows flexibility where drop off and pickup times are dispersed, so you don't see typical congestion.

Mr. Dziwulski feels it is great to include the traffic flow in the orientation packet, but asked what happens when they do it for a week and then they start cutting through the subdivision. Mr. Knopf said they want to be a great neighbor and part of the community and if it becomes an issue they will work with the City.

Mr. Vetterkind asked who was going to volunteer in a million-dollar subdivision to have a slowdown sign or flashing light in their yard. He added that officers can't sit and police it every day.

Mr. Clark asked if similar conditions were made for KinderCare and if people were cutting through the subdivisions. Mr. Fuchs stated he was not aware of any conditions related to traffic and was only aware of traffic cutting through by comments made by residents.

Jill Schultz (N36W23835 Broken Hill Circle South) said she is pro-Montessori and would not be here if KinderCare wasn't already there. She said traffic and cars speeding through her neighborhood has increased drastically since KinderCare opened. She added this will increase traffic and everyone is already cutting through, including construction vehicles. Ms. Schultz noted adding more cars to the same time frame of traffic in the morning and afternoon is frustrating.

Rob Myhre (N36W23526 Oak Hill Lane) said he couldn't track the numbers since he didn't have the data that was presented but doesn't understand the math when it comes to the increase in cars. He added that the data is polluted.

Meagan Lind (N37W23740 Broken Hill Circle South) said the peak times that were discussed were also the peak times for buses, as pick up is between 7:45 - 8:15 a.m. She said the corner is bad and people are cutting through from Highway 164.

Shannon Myhre (N36W23526 Oak Lane) stated it is not about the numbers, it is the complexity. She noted there is only one way in and one way out. She added the cut through is real.

John Bieberitz with Traffic Analysis and Design stated the data from the traffic study was done in April of this year, not during Covid. He added the site is zoned B-6 and the trip generation shows other usages that may be allowed on the property could produce more traffic and even include weekend traffic.

Ms. Brown asked what the property was currently zoned and if Mr. Fuchs was considering putting it in B-4. Mr. Fuchs stated it is zoned B-6 and refers to B-4 and B-5 districts for permitting.

A brief discussion took place regarding the uses of B-4 and B-5 and permitted uses that would not require anything but a business plan of operation.

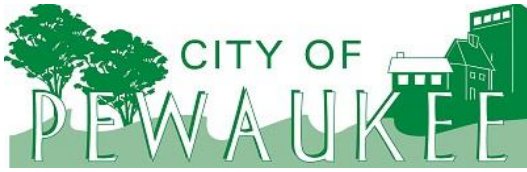
Mayor Bierce closed the public hearing.

Mayor Bierce stated this is a classic example of what small down government does. He said you need to ask yourself if allowing someone to create a business will adversely affect other families around them, or if it would be hazardous or harmful to the value of the neighborhood or community. Mayor Bierce noted there were good opinions and facts, and some facts could be used in either argument.

A motion was made and seconded (P. Vetterkind, B. Dziwulski) to reject the Conditional Use Permit for Quattro Development/Higher Ground Education. Motion Passed: 4-For, 0-Against on roll call vote. Attorney Martell advised each Council member to state their vote and reason for their vote. Mr. Vetterkind voted against the Conditional Use Permit based on it being a hazardous idea to add more traffic to an already congested area. He added the data shows it would make it more hazardous and there are other uses that could go on that site. Mr. Dziwulski seconded the motion based on the traffic in the subdivision. He said roads are narrow and adding more traffic is not acceptable. Ms. Brown voted in favor of the motion because it is adverse to the neighborhood and feels it would be a negative impact on the neighborhood. She encouraged Higher Ground to see if there is another location in the City for the development and stated this type of Montessori school is needed and wanted. She said her vote is based on the neighborhood and not because of the project itself. Mr. Clark voted in favor of the motion and concurred with Alderperson Brown.

September 5, 2023 Common Council: Discussion and Possible Action Regarding Resolution 23-09-14 Releasing a Forty-Five Foot Reserved Right-of-Way Restriction Upon Lots 1 and 2 of Certified Survey Map No. 10366 as Requested by Quattro Development Reservation

Mr. Fuchs stated no action is needed on this resolution since the Conditional Use Permit was not passed. There was no further discussion.



Office of the Planner & Community Development Director
W240 N3065 Pewaukee Road
Pewaukee, Wisconsin 53072
Phone (262) 691-0770 Fax (262) 691-1798
fuchs@pewaukee.wi.us

REPORT TO THE PLAN COMMISSION

Meeting of February 16, 2023

Date: February 9, 2023

Project Name: Guidepost Montessori Conditional Use and Site & Building Plan Review

Project Address/Tax Key No.: Highfield Court/PWC 0906994

Applicant: Quattro Development, LLC

Property Owner: KB Pewaukee, LLC

Current Zoning: B-6 Mixed Use Business

2050 Land Use Map Designation: Office/Commercial

Use of Surrounding Properties: Capitol Dr to the south, Highfield Ct to the north, a daycare to the east and bank to the west.

Project Description/Analysis:

The applicant, Quattro Development LLC, submitted a Conditional Use Application and Site & Building Plans for a proposed childcare facility on a mostly vacant lot located along Highfield Court and Capitol Drive between HWY 164 and Highfield Road. The subject property is zoned B-6 Mixed Use Business District and designated as Office/Commercial on the City's Year 2050 Land Use/Transportation Plan map.

Conditional Use

The proposed use is more specifically a Montessori educational facility. The applicant anticipates 15 full-time employees, with a maximum child occupancy of 152 children. The hours of operation are from 7am to 6pm Monday through Friday.

The B-6 District refers to the B-4 and B-5 districts regarding allowed uses. Educational and childcare facilities is not specifically listed as an allowed use; however, the B-4 and B-5 districts allow uses similar to those that are listed as Permitted Uses within each district. Those uses not listed are required to apply for Conditional Use Permits.

The B-4 District generally allows offices, banks, and studios for fine arts. The B-5 District generally allows auto sales and services, hotels and motels, and restaurants.

Staff finds that the proposed use is most similar to B-4 type uses and would not have any adverse impacts to the surrounding area. The B-4 District is intended to consist of less intensive uses in order to be compatible with neighborhood residential uses, such as the residential subdivision to the north and condominiums to the east. Staff contends that the proposed use meets this intent and fits in with adjacent uses.

Furthermore, the abutting use to the east is also a childcare facility. This property is zoned B-1, which similarly does not specifically allow childcare facilities but allows similar uses as those permitted in

the B-1 District via Conditional Use Permit. The B-1 District generally allows retail and office type uses.

Site Plan

The subject property is about 2-acres and the total site disturbance of the project will be a total of 1.67 acres. The project results in about 50% greenspace, which is in conformance with City standards.

The proposed development consists of a new 11,836 square foot, single-story building. Two large playground areas are proposed at the north and south ends of the childcare facility. A smaller play area is also proposed on the east side of the building.

A dumpster enclosure is located beside the driveway, slightly northwest of the northernmost playground area. A 6-foot wrought iron fence runs from the north side of the building down to the south side of the building and 4-foot white vinyl fencing is used to separate play areas.

Overall, the proposed development complies with B-6 District development standards.

Staff recommends that the playground areas, fencing, and any other accessory structure and use be located behind the building on the north side of the property. The applicant has indicated that due to the building configuration and operation of the Montessori program, the playground locations as proposed are integral to their use.

Access

This site abuts both Capitol Drive and Highfield Court, but is only accessible from Highfield Court.

Additionally, there is an existing and unimproved 45' right-of-way reservation depicted along the south side of the property, adjacent to Capitol Drive, shown on Certified Survey Map No. 10366. The applicant is requesting that this reservation be released by the City.

Staff has reviewed this request and does not have any objections. It is anticipated that a resolution releasing this reservation will be brought forward to the Common Council for their review and approval.

Landscaping

The applicant is proposing to install 19 Deciduous Trees, 5 Evergreen Trees, 31 Deciduous Shrubs, and 29 Evergreen Shrubs. There are at least three different species of Deciduous Trees, Deciduous Shrubs, and Evergreen Shrubs provided and two different species of Evergreen Trees.

The landscape plan focuses many plantings on screening the fencing and playground area along Capitol Drive. Plantings are also provided directly around the dumpster enclosure.

Parking

The site plan includes 25 new parking spaces, of which three are ADA required spaces.

Additionally, there is existing parking located on the northwest side of the site, which is shared with the bank and office building to the west. There is an existing cross parking easement with the property to the west.

Parking spaces are 10' x 18' (180 square feet), which is in compliance with Zoning Code standards.

Architecture

The proposed building exterior consists of Hardie shank siding, thin concrete brick veneer, thin stone veneer, and asphalt shingles. The dumpsters will be enclosed in a decorative masonry enclosure matching the building's exterior materials.

The mechanicals are located in a recessed rooftop penthouse, which screens them from public view.

Signage

A wall sign and monument sign are anticipated and shown for reference only in the plans. All signage must comply with standards set forth in Section 17.0700 of the City's Zoning Code and will require separate review and approval by the City Planner as well as a Sign Permit from the Building Services Department, prior to installation.

Utilities

Public sewer and water are available and will serve the development.

Final grading, erosion control and storm water management plans shall be approved by the Engineering Department, prior to any land disturbing activities.

Lighting

The Lighting Plan consists of three parking lot lights. Light levels are at 0.0 footcandles at the property lines. The peak height of lights is 18-feet, which complies with the 20-foot maximum height requirement of the Zoning Code. The Wattage is under 150 and not visible from adjacent streets or residential properties.

Bell Tower

The subject property was previously part of the property to the west. In 2005, the City approved Conditional Use Permit No. COP-05-10-3, which included this property, and contained the conditions listed below related to the construction of a bell tower. The bell tower was being proposed by the property owner at that time along with the Bell Tower Memorial non-profit organization.

1. The bank would build a bell tower according to the plans that were developed and brought to the Plan Commission at their November 2005 meeting.
2. The bell tower would be constructed prior to occupancy of the second office building proposed for the site.
3. The second office building proposed for the site would be constructed within three (3) years or by October of 2008.
4. If the bell tower is not built by October 2008, the City will build the tower according to the plans submitted on November 17, 2005, and assess the Foundation Bank for the cost of the tower, plus any administrative costs.

The applicant is requesting that these conditions be removed and that there be no requirement to construct a bell tower. It is staff's understanding that the Bell Tower Memorial organization is continuing to fundraise to construct the bell tower within the Pewaukee Kiwanis Village Park, opposed

to this location. The bell that was to be utilized for the tower at this site was also sold or given to another organization.

The Conditional Use Permit has been drafted to include language stating that “*The applicant shall not be responsible for the construction of a bell tower as conditioned within Conditional Use Permit No. COP-05-10-3.*”

Recommendation:

A motion to recommend approval of the Conditional Use Permit for Quattro Development LLC to locate a childcare facility upon property bearing Tax Key No. PWC 0906994.

and

A motion to approve the Site & Building Plans for the construction of a childcare facility upon property bearing Tax Key No. PWC 0906994, subject to the conditions contained within this report.

February 3, 2023

Project Narrative

Project: Higher Ground Education
East of N35W23877 Highfield Ct
Pewaukee, WI 53072
Excel Project No: 2245520

Quattro Development, LLC (partnered with Higher Ground Education) is requesting conditional use permit review and approval for a new childcare facility development located on parcel PWC 0906994, just east of N35W23877 Highfield Ct in the City of Pewaukee. The property is zoned B-6 Mixed Use Business and the proposed use requires a conditional use in this district. The existing site is vacant and undeveloped. Higher Ground Education provides Montessori-powered educational offerings.

The proposed project consists of a new 11,705 SF single story childcare facility. The existing site is 2 acres and the total site disturbance for the project will total 1.67 acres. The site can be accessed via the existing shared drive from Highfield Ct. An existing access easement is in place along with a cross parking easement. The proposed new parking will consist of 38 new stalls. Traffic access will be consistent with the existing facility to the west of the proposed development as the existing shared access drive will be utilized. Some new parking stalls will access directly off the existing driveway with the remaining being placed in a parking lot extension to the south. Truck unloading and deliveries will be at a minimum and when needed, the site drive west of the building will be utilized. The playground areas are strategically located in conjunction with the floor plan and provide ample dedicated area for outdoor play/activities.

Stormwater management for this property was accounted for by the adjacent/neighbor parent development to the west of the subject property. A stormwater wet pond is located west of the existing bank/office complex. Proposed stormwater from the development will surface drain consistent with the original stormwater plan to the neighboring stormwater facility. Final engineering documents will be provided to as-built and model the existing stormwater facility and show conformance with current stormwater management ordinances. Sanitary and water utilities will connect to existing public infrastructure as shown on the proposed Utility Plan.

Exterior materials as represented in the attached colored elevations and rendering will consist of the following: Hardie board lap/vertical siding, Hardie shake siding, thin stone veneer, and asphalt shingles. Refuse and recycling dumpsters will be enclosed in a decorative masonry enclosure to match the building's exterior materials. The enclosure will be located in the north parking field, aligning with perpendicular drive isle for access.

Landscaping will be provided in accordance with the City ordinance, in a fashion which ensures species resiliency and complimentary aesthetics. Site lighting will meet the City ordinance in a fashion that provides appropriate foot candles for safety, cut-off fixtures for zero up-lighting, and a decorative style that compliments the development and building architecture.

The proposed daycare/childcare center will have 15 full-time employees, and 0 part-time employees. The largest shift will be all 15 full-time employees. Maximum child occupancy will be 152. The daycare will be in operation from 7:00 a.m. to 6:00 p.m. during weekdays. No hazardous products or chemicals are located on the property nor is there outside storage. The estimated project value is \$3,000,000 and an approximate start date is Spring of 2023.

In conjunction with the proposed development project, it is requested to remove the bell tower requirement that is tied to the title of the subject property. The bell tower is not proposed for this project and will not be provided.

February 3, 2023

City of Pewaukee
Attn: Nick Fuchs
W240N3065 Pewaukee Road
Pewaukee, WI 53072

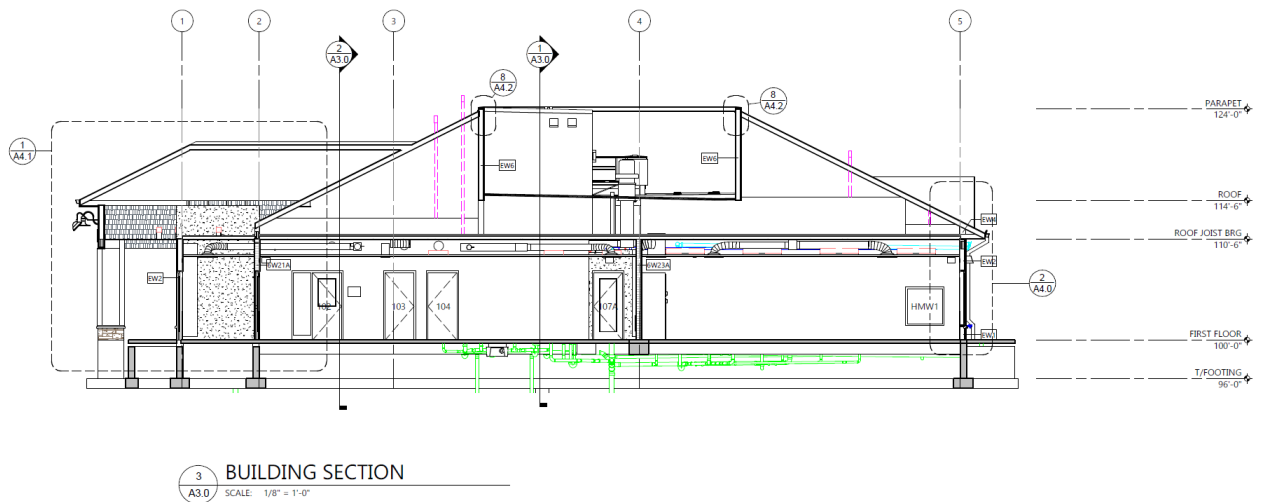
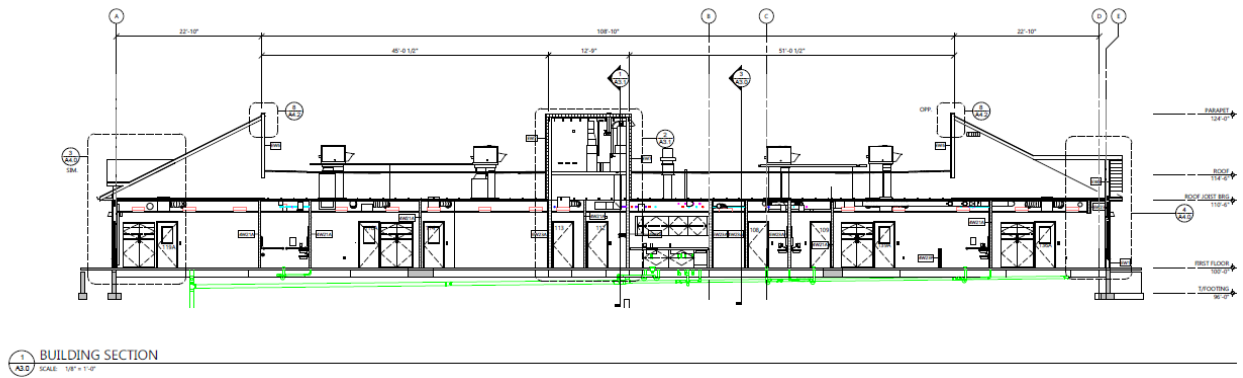
Re: Quattro Development - Guidepost Montessori Conditional Use and Site & Building Plan Review

Dear Mr. Fuchs,

This letter is in response to the staff comments dated January 20, 2023.

General Comments

1. There appears to be many inconsistencies on page C1.1 between the labels and arrows on the site plan and the Site Plan Keynotes. Below are several examples. Please double check these items and revise accordingly.
 - a. 25 is listed as a flagpole yet no flagpole label on the plan.
Excel Response: Number 25 has been removed from the Site Plan Keynotes as there is no proposed flagpole.
 - b. 7 is listed as dumpster pad/apron concrete yet no dumpster pan/apron concrete label is on the plan.
Excel Response: The mentioned label has been adjusted on the site plan.
 - c. 29, 30, & 31 are included within the middle of the building with their arrows not directed at any element on the plan.
Excel Response: The mentioned labels have been adjusted on the site plan.
 - d. 28 is listed as traffic arrows yet they are labeled around the playgrounds. Why are there traffic arrows around the playgrounds?
Excel Response: The traffic arrows designate the direction of travel around the playgrounds for a tricycle loop.
2. What is directly abutting the north playground to the east?
Excel Response: This area is designated as an outdoor seating area.
3. Was the poured rubber playground surface included in the impervious surface calculations?
Excel Response: The poured rubber playground surface was included in the impervious surface calculations.
4. Where are mechanicals located? How are these screened?
Excel Response: The mechanicals are located in a rooftop penthouse recessed out of view.



Site Plan Comments

1. Please illustrate the 30-foot parking and loading setback along the south side of the property adjacent to Capitol Drive and remove any portion of the parking lot encroaching into that setback.

Excel Response: The appropriate adjustments have been made on the site plan.

2. Staff recommends exploring ways to reconfigure the building and site layout to address the comments and recommendations below.
 - a. It is recommended that the playground areas, fencing, and majority of parking be located behind the building.

Excel Response: The site and building have been situated to maintain the intent of stormwater management that was set up at the time of the bank development. Modifications after discussions with staff are implemented in the revised site plans. The building layout cannot change and is specific to the Montessori learning with associated break-out playground areas around the building perimeter.

- b. It is recommended to revise the parking layout to avoid cars having to back out into the main drive aisle. Parking could potentially be configured to follow the layout of the adjacent parking to the west, which extends onto this property.

Excel Response: Several parking stalls on the north side of the proposed parking lot have been removed to accommodate this. The site plan modifications were discussed with staff, the shared parking will be used for employee parking.

- c. Relocate the dumpster enclosure where it can be better screened by landscaping.
Excel Response: The dumpster enclosure has been moved further to the east of its previous position to include landscaping around it.
- d. It is recommended to not have the metal doors and service equipment facing Capitol Drive.
Excel Response: Doors have been removed from the south side of the building facing Capitol Drive.
- e. Will there be a lot of drop off and pick up occurring onsite? If so, the site plan should be revised to accommodate that better.
Excel Response: Drop off and pick will be in the stalls immediately west of the building. Give the age group of the facility, parents will park and escort children into the building. A “drop off” scenario typical to schools is not applicable for this facility.

Architecture Comments

- 1. Consider incorporating additional masonry on the elevation facing Capitol Drive as well as additional elements to provide more variation in the footprint and roofline of the building. Materials and quality should match that of the adjacent buildings.
Excel Response: The Capitol Drive elevations have been updated to address this comment.

Parking Comments

- 1. On page C1.1 it states that three ADA spaces are required yet two ADA spaces are provided. Please revise and provide the appropriate number of ADA accessible parking spaces.
Excel Response: Three ADA spaces have been provided on the site plan.

Landscaping Comments

- 1. Staff suggests adding additional plantings adjacent to the playground areas. These may be more sporadic as the intent is not to necessarily require these areas to be fully screened.
Excel Response: Additional plantings have been included on the south and north side of the site plan.
- 2. As previously discussed, there is a requirement that a bell tower be constructed on this property. As part of the Conditional Use, please revise the project narrative to include a specific request to remove this requirement, assuming the bell tower is not being proposed as part of this development.
Excel Response: The CUP project narrative has been updated to remove the bell tower requirement.

Signage Comments

- 1. Please be aware that signage will require separate review and approval by the City Planner as well as a Sign Permit from the Building Services Department prior to installation.
Excel Response: The appropriate accommodations will be made.
- 2. Also note that per section 17.0706(c) a ground sign located within 660 feet of a residential zoning district boundary shall not be illuminated during the seven-hour period beginning at

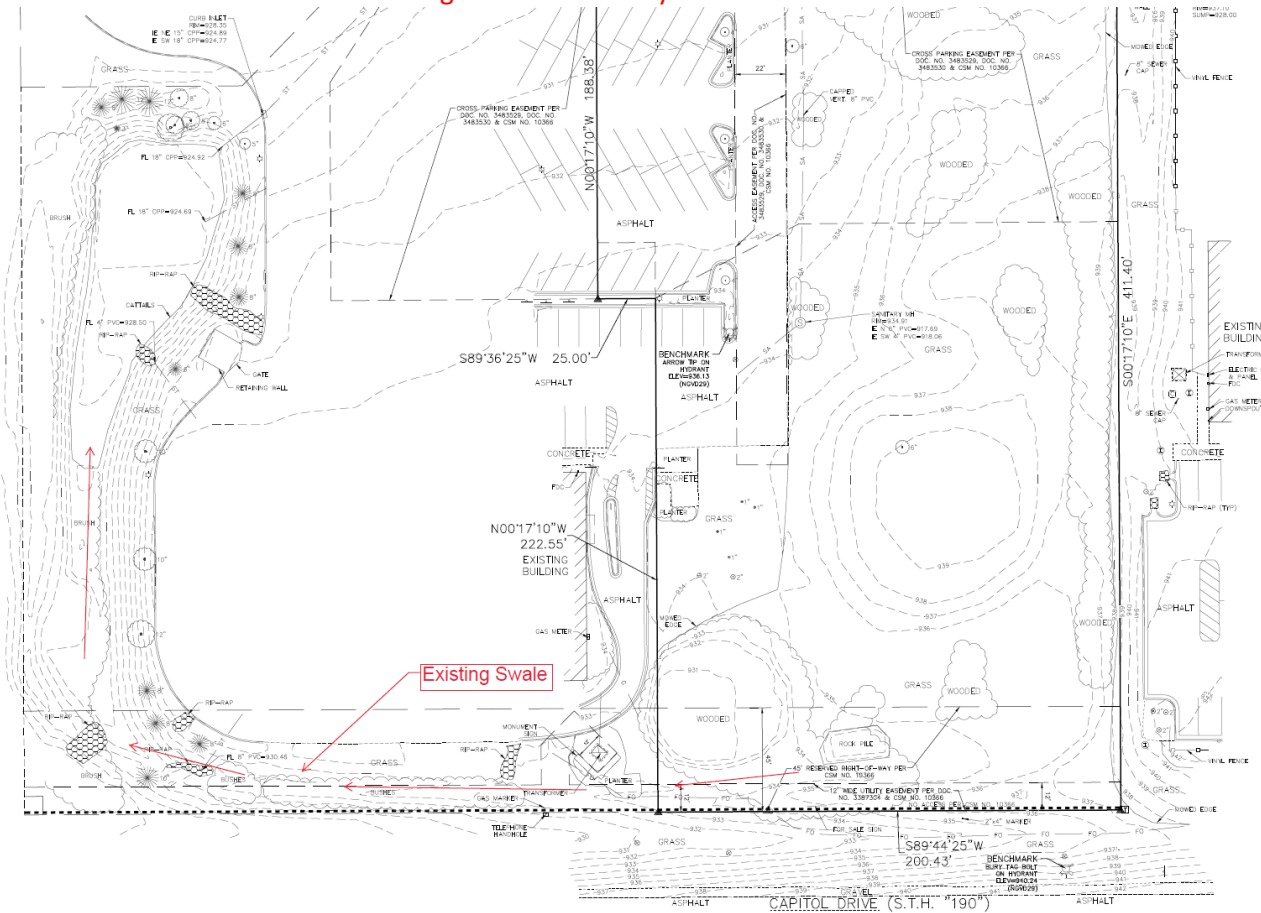
eleven (11) PM and ending at six (6) AM.

Excel Response: The appropriate accommodations will be made.

Engineering Department Comments

1. The project narrative outlines that the existing basin on the adjacent property will be used to address stormwater requirements. Below are comments relating to this strategy.
 - a. A large portion of the proposed site appears to drain to the southwest corner of the site into an existing drainage swale. This drainage swale does not lead to the existing stormwater pond based on existing contours.

Excel Response: There is an on-site drainage swale north of the R/W line that does direct runoff to the west into the existing stormwater facility.



- b. This stormwater facility is off-site. Documentation of agreements with adjacent landowner stating that this development can utilize the existing stormwater facility must be provided to the City. Note that it must also be shown that the existing facility can handle the additional flow.

Excel Response: Understood.

- c. The north portion of the site is planned to sheet flow towards the neighbor's parking lot. Agreements must be provided stating that the neighbor is aware of this design, approves of it, and won't change their site in a way that would restrict the sheet flow. Calculations

shall be provided that show the existing conveyance systems (mostly storm sewer) are adequate for the additional flow.

Excel Response: Understood.

2. There is currently no stormwater maintenance agreement for the existing stormwater facilities. A stormwater maintenance agreement must be provided and signed by both the developer and the owner of the existing stormwater facilities.

Excel Response: Understood.

3. The proposed plan shows the development connecting to the existing sanitary lateral for the adjacent property which connects to a holding tank. Please note the requirements below:
 - a. An easement will need to be created to accommodate the existing sanitary lateral.

Excel Response: An easement will be provided.

- b. This service is currently connected to a holding bank. It will need to be connected to the sanitary sewer system during construction.

Excel Response: Understood, a connection to the public sanitary system will be provided.

- c. Each lot will need to have a separate sampling manhole.

Excel Response: Understood, sampling manholes will be provided.

The submittal documents have been revised per the comments above. Please accept for review and approval. Please let me know if you have any comments, questions, or need additional information.

Sincerely,

Excel Engineering, Inc.



Grant Duchac, P.E.
Project Manager

WAUKESHA COUNTY CITY OF PEWAUKEE STATE OF WISCONSIN

RESOLUTION NO. __-__

**A RESOLUTION AUTHORIZING THE RELEASE, WAIVER, VACATION AND
REVISION OF A FORTY-FIVE FOOT RESERVED RIGHT-OF-WAY RESTRICTION
UPON LOTS 1 AND 2 OF CERTIFIED SURVEY MAP NO. 10366 FOR PROPERTIES
LOCATED AT APPROXIMATELY N35W23877 HIGHFIELD COURT BEARING TAX
KEY NOS. 0906993 AND 0906994**

WHEREAS, the Plan Commission having approved a Conditional Use and Site and Building Plan Review Application upon the application of Quattro Development, LLC on March 16, 2023, and the Plan Commission having conditioned approval thereof in part upon Common Council approval of the release of a 45' Reserved Right-of-Way restriction located upon Parcels 1 and 2 of Certified Survey Map No. 10366, recorded April 13, 2007 in Volume 98 of Certified Survey Maps on Pages 03 to 209, inclusive as Document No. 3472291 with the Office of the Register of Deeds for Waukesha County, Wisconsin, as further described below:

Release of the 45' Reserved Right-Of-Way located along the south property line of Parcel 1 of CSM No. 10366.

Release of the 45' Reserved Right-Of-Way located along the south property line of Parcel 2 of CSM No. 10366; and

WHEREAS, the City Engineering Department and City Planner have reviewed the proposed release of a 45' Reserved Right-of-Way restriction and recommended approval thereof to the Common Council; and

WHEREAS, the 45' Reserved Right-of-Way has been determined to no longer be needed considering the developments of the subject properties and adjacent parcels to the east; and

WHEREAS, the previously recorded 45' Reserved Right-of-Way restriction described above are located upon Certified Survey Map No. 10366, which properties are legally described as follows:

Commencing at the Southwest corner of the Northwest ¼ of said Section; thence N00°20'35"W along the West line of the Northwest ¼ of said Section 671.90 feet; thence N89°36'25"E 430.02 feet to the Northwest corner of Parcel 2 of said Certified Survey Map; thence S00°17'10"E along the West line of Parcel 2 of said Certified Survey Map 155.00 feet to the point of beginning; thence N 89°36'25"E along the South right-of-way line of Highfield Drive; thence N44°14'24" E along the Southeasterly right-of-way line of said Drive 116.99 feet; thence N66°55'27"E along the Southeasterly right-of-way line of said Drive 30.47 feet; thence

N89°36'25"E along the South right-of-way line of said Drive 268.82 feet to a point on the East line of Parcel 2 of said Certified Survey Map; thence S00°17'10"E along the East line of Parcel 2 of said Certified Survey Map 411.40 feet to the Southeast corner of Parcel 2 of said Certified Survey Map and a point on the North right-of-way line of Capitol Drive; thence S89°44'25"W along the South line of Parcel 2 of said Certified Survey Map and the North right-of-way line of said Drive 474.81 feet to the Southwest corner of Parcel 2 of said Certified Survey Map; thence N00°17'10"W along the West line of Parcel 2 of said Certified Survey Map 315.31 feet to the point of beginning. Containing 4.17 acres of land more or less.

WHEREAS, Wis. Stats section 236.293 provides in part that any restriction placed on platted land by covenant, grant of easement or in any other manner, which was required by a public body vests in the public body the right to enforce the restriction at law or in equity and that the restriction may be released or waived in writing by the public body having the right of enforcement; and

WHEREAS, the Common Council has determined that the release, waiver, vacation and revision of the prior recorded 45' Reserved Right-of-Way is reasonable as it is no longer necessary for the orderly development of the subject properties or appropriate following the approval of the aforesaid site and building development plans submitted by Quattro Development, LLC.

NOW, THEREFORE, BE IT RESOLVED, by the Mayor and Common Council of the City of Pewaukee, Wisconsin, that the 45' Reserved Right-of-Way upon Parcels 1 and 2 of Certified Survey Map No. 10366, recorded April 13, 2007 in Volume 98 of Certified Survey Maps on Pages 03 to 209, inclusive as Document No. 3472291 with the Office of the Register of Deeds for Waukesha County, Wisconsin, be and the same are hereby waived and released.

BE IT FURTHER RESOLVED that the City Clerk is hereby directed to record this Resolution in the Office of the Register of Deeds for Waukesha County, Wisconsin following the approval of the proposed Quattro Development, LLC Conditional Use and Site and Building Plan Review applications.

Passed and adopted this ____th day of _____, 2023.

FOR THE COMMON COUNCIL OF THE CITY OF
PEWAUKEE, WAUKESHA COUNTY, WISCONSIN

ATTEST:

Steve Bierce, Mayor

Kelly Tarczewski, Clerk/Treasurer

SPECIFICATION NOTE:
SEE SHEET C0.1 FOR PLAN
SPECIFICATIONS AND REQUIREMENTS

EXISTING SITE DATA

	AREA (AC)	AREA (SF)	RATIO
PROJECT SITE	2.00	87,120	
BUILDING FLOOR AREA	0.00	0	0.0%
PAVEMENT (ASP. & CONC.)	0.00	0	0.0%
TOTAL IMPERVIOUS	0.00	0	0.0%
LANDSCAPE/ OPEN SPACE	2.00	87,120	100.0%

PROPOSED SITE DATA

	AREA (AC)	AREA (SF)	RATIO
PROJECT SITE	2.00	87,120	
BUILDING FLOOR AREA	0.27	11,836	13.6%
PAVEMENT (ASP. & CONC.)	0.73	31,783	36.5%
TOTAL IMPERVIOUS	1.00	43,619	50.1%
LANDSCAPE/ OPEN SPACE	1.00	43,501	49.9%

SITE INFORMATION:

LEGAL DESCRIPTION: Lot 2 of Certified Survey Map Number 10366 filed in Volume 98 of Certified Survey Maps on Page 203 as Document Number 3472291;

PROPERTY AREA: AREA = 87,120 S.F. (2.00 ACRES).

EXISTING ZONING: B-6 MIXED USE BUSINESS

PROPOSED ZONING: B-6 MIXED USE BUSINESS

PROPOSED USE: DAYCARE/CHILDCARE FACILITY

AREA OF SITE DISTURBANCE: 72,650 S.F. (1.67 ACRES)

SETBACKS: BUILDING: FRONT = 55'
SIDE = 30'
REAR = 55'

PAVEMENT: FRONT = 30'
SIDE = 10'
REAR = 10'

PROPOSED BUILDING HEIGHT: 25' (MAX. HEIGHT ALLOWED: 45')

PARKING REQUIRED: 1 SPACE PER 2 EMPLOYEES (8 SPACES REQ.)

PARKING PROVIDED: 25 SPACES (3 H.C. ACCESSIBLE) & 33 SHARED STALLS

HANDICAP STALLS REQUIRED: 3, HANDICAP STALLS PROVIDED: 3

HOURS OF OPERATION: 7:00 A.M. - 6:00 P.M.

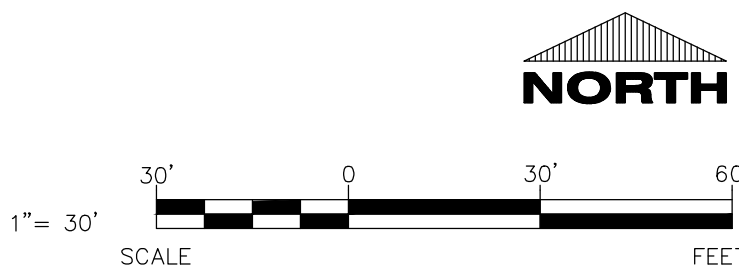
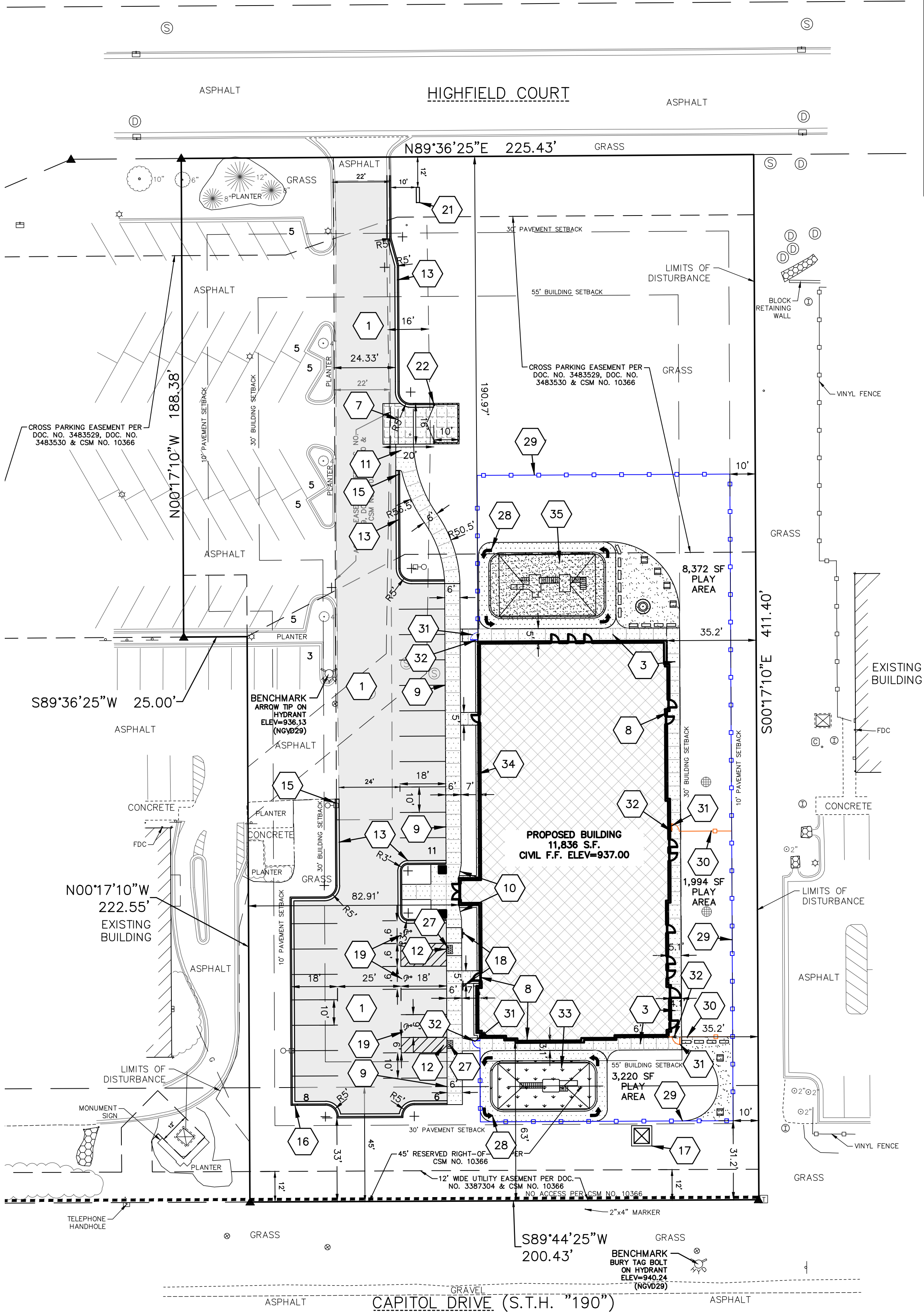
BUILDING OCCUPANCY CLASSIFICATION = E

CLASS OF BUILDING CONSTRUCTION = 5B

LANDSCAPE REQUIREMENTS: MIN. LANDSCAPE SURFACE RATIO: 40%
MAXIMUM IMPERVIOUS SURFACE: 60%
MAXIMUM LOT COVERAGE - BUILDING ONLY: 35%

SITE PLAN KEYNOTES

- STANDARD ASPHALT SECTION (TYP.)
- CONCRETE SIDEWALK (TYP.)
- DUMPSTER PAD/APRON CONCRETE (TYP.)
- CONCRETE STOOP (TYP.) SEE ARCH. PLANS FOR DETAILS.
- RAISED WALK (TYP.)
- TRANSITION WALK FROM 5' TO 6' WIDE
- WALK RAMP
- ADA RAMP
- 18" CURB & GUTTER (TYP.)
- CURB TAPER (TYP.)
- CURB CUT (TYP.)
- CONCRETE TRANSFORMER PAD BY UTILITY SUPPLIER (CONTRACTOR TO VERIFY FINAL LOCATION & DESIGN PRIOR TO CONSTRUCTION)
- HANDICAP SIGN (TYP.)
- HANDICAP STALL & STRIPING PER STATE CODES.
- MONUMENT SIGN (DETAILS, FINAL LOCATION, & APPROVAL BY SIGN VENDOR)
- DUMPSTER ENCLOSURE (SEE ARCH PLANS FOR DETAILS)
- STOP SIGN (TYP.)
- DETECTABLE WARNING PLATE
- TRAFFIC ARROWS (TYP.)
- 6' TALL WROUGHT IRON FENCE
- 4' TALL WHITE VINYL FENCE
- FENCE GATE
- PANIC HARDWARE
- POURED RUBBER PLAYGROUND SURFACE OVER 4-6" CLEAR STONE AGGREGATE BASE (FOLLOW DESIGN SPECS. BY SUPPLIER)
- 12" WIDE CONCRETE STRIP ALONG WEST FOUNDATION
- WOOD FIBER SURFACE BY PLAYGROUND VENDOR



CIVIL SITE PLAN



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COLLABORATION



PROJECT INFORMATION

PROPOSED EARLY CHILDHOOD SCHOOL FOR:
QUATTRO DEVELOPMENT
23877 HIGHFIELD COURT • PEWAUKEE, WI 53072

PROFESSIONAL SEAL

PRELIMINARY DATES

NOV. 29, 2022
DEC. 12, 2022
JAN. 4, 2023
FEB. 3, 2023

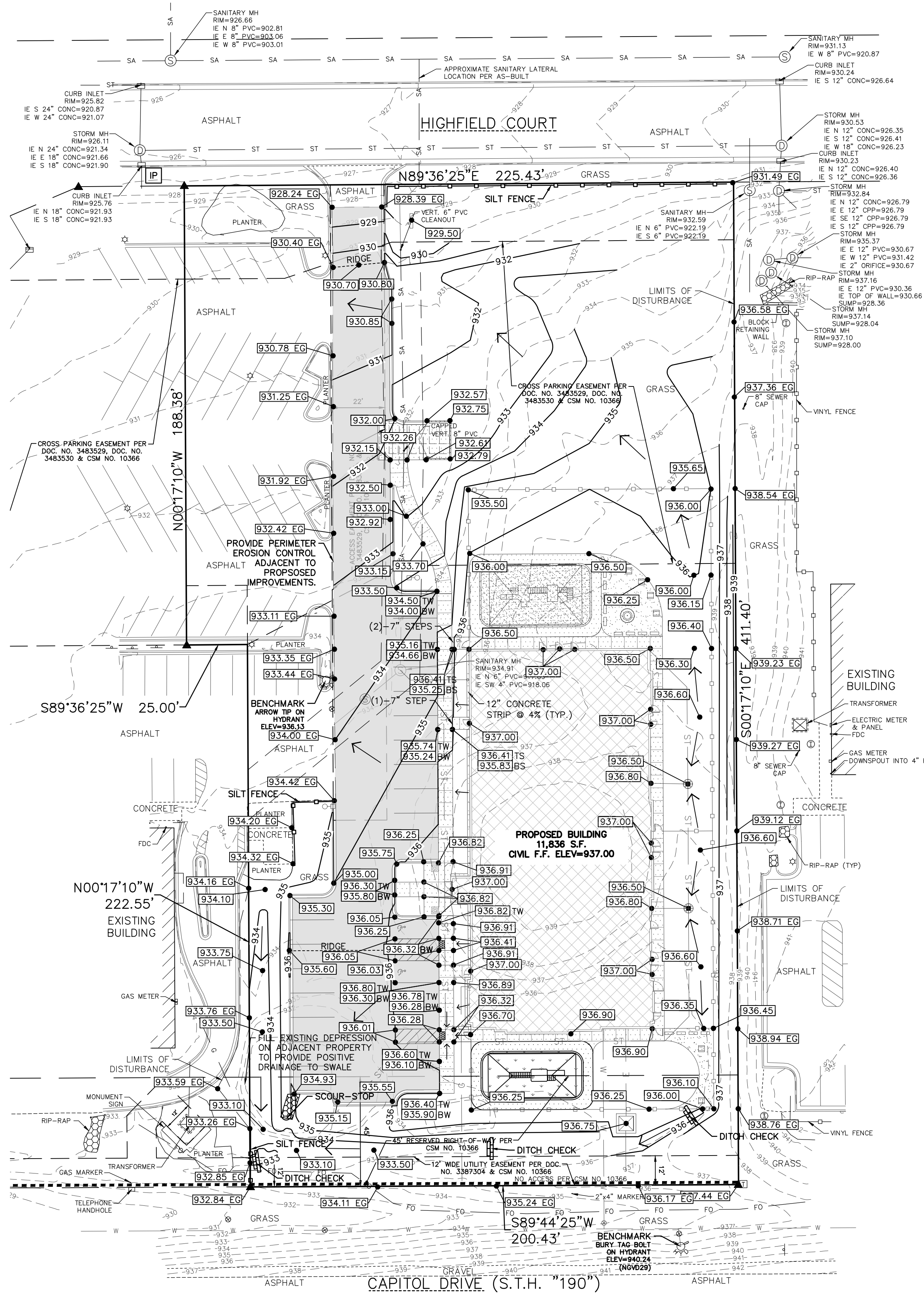
NOT FOR CONSTRUCTION

JOB NUMBER

2245520

SHEET NUMBER

C1.1



SPECIFICATION NOTE:
SEE SHEET C0.1 FOR PLAN
SPECIFICATIONS AND REQUIREMENTS

- NOTES:
- HANDICAP STALL AND ACCESS AISLES SHALL NOT EXCEED A SLOPE OF 1.50% IN ANY DIRECTION. HANDICAP STALL & ACCESS AISLES SHALL CONFORM TO ADA REQUIREMENTS (CURRENT EDITION)
 - ALL SIDEWALKS SHALL NOT EXCEED A MAXIMUM CROSS SLOPE OF 1.50% AND RUNNING SLOPE OF 4.50% UNLESS OTHERWISE SPECIFIED.

INLET PROTECTION NOTE:
CONTRACTOR SHALL PROVIDE TEMPORARY INLET PROTECTION FOR ALL CURB INLETS & CATCH BASINS ONSITE & OFFSITE IMMEDIATELY DOWNSTREAM OF THE PROJECT SITE PER LOCAL CODE.

STABILIZED CONSTRUCTION ENTRANCE NOTE:
CONTRACTOR SHALL PROVIDE STABILIZED CONSTRUCTION ENTRANCE AT CONSTRUCTION ENTRANCE FOR PROPOSED IMPROVEMENTS AS REQUIRED PER CODE.

CONCRETE WASHOUT NOTE:
CONTRACTOR SHALL PROVIDE CONCRETE WASHOUT AS REQUIRED PER CODE. FINAL LOCATION TBD BY CONTRACTOR.

STORMWATER NOTE:
SITE STORMWATER DRAINS TO EXISTING STORMWATER FACILITY ON NEIGHBORING PROPERTY.



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23877 HIGHFIELD COURT • PEWAUKEE, WI 53072

PROFESSIONAL SEAL

PRELIMINARY DATES

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FEB. 3, 2023

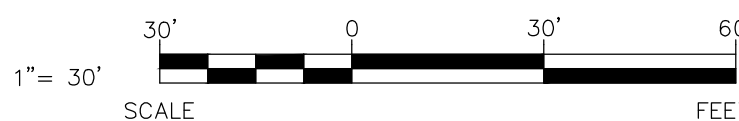
NOT FOR CONSTRUCTION

JOB NUMBER

2245520

SHEET NUMBER

C1.2



CIVIL GRADING AND EROSION CONTROL PLAN



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PROJECT INFORMATION

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JOB NUMBER

2245520

SHEET NUMBER

C1.3

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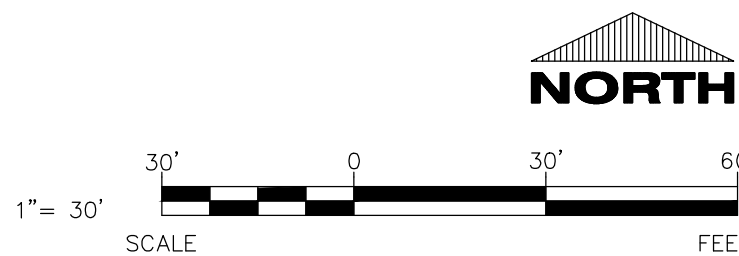
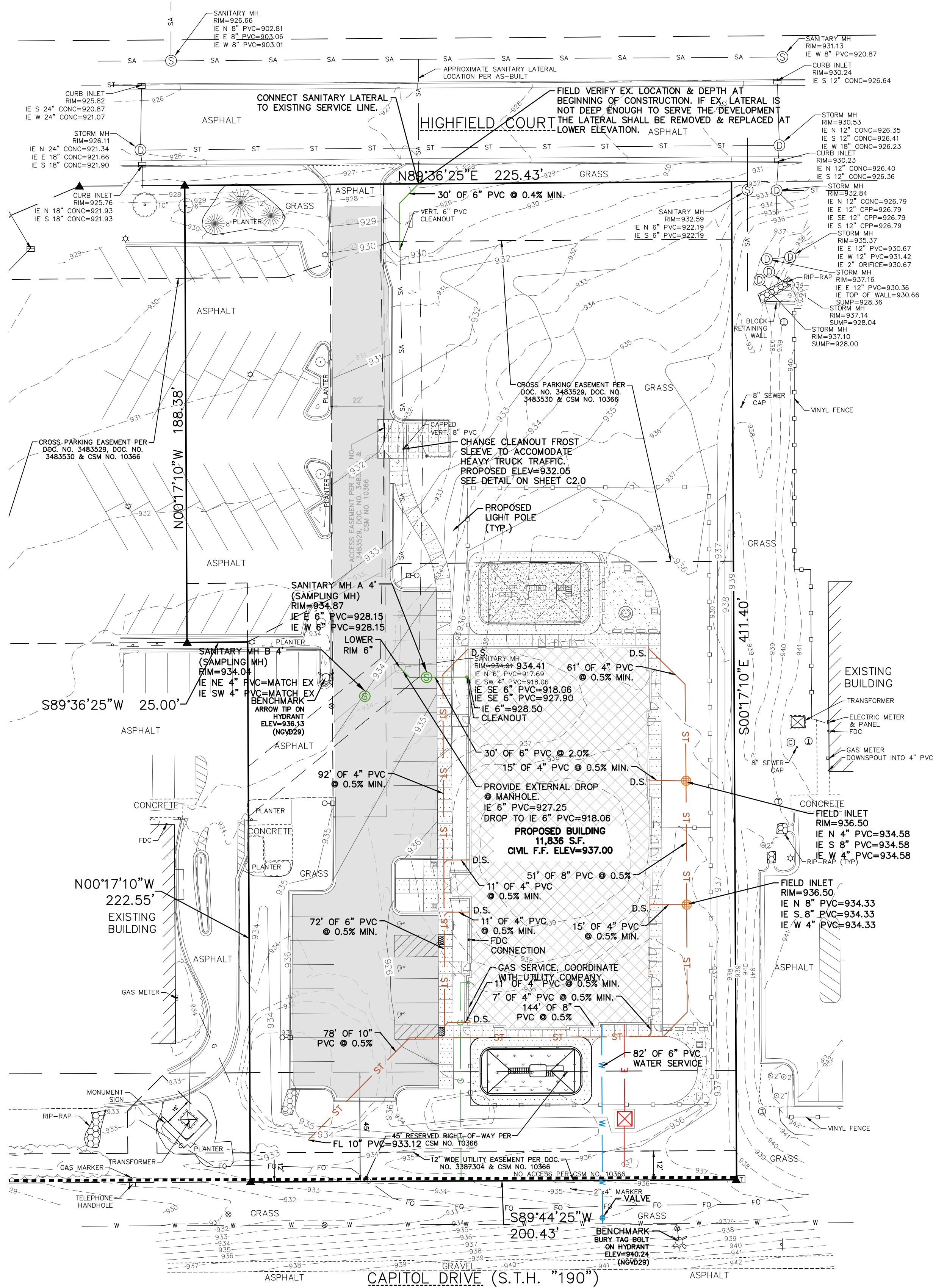
SPECIFICATION NOTE:
SEE SHEET C0.1 FOR PLAN
SPECIFICATIONS AND REQUIREMENTS

DOWNSPOUT NOTE:

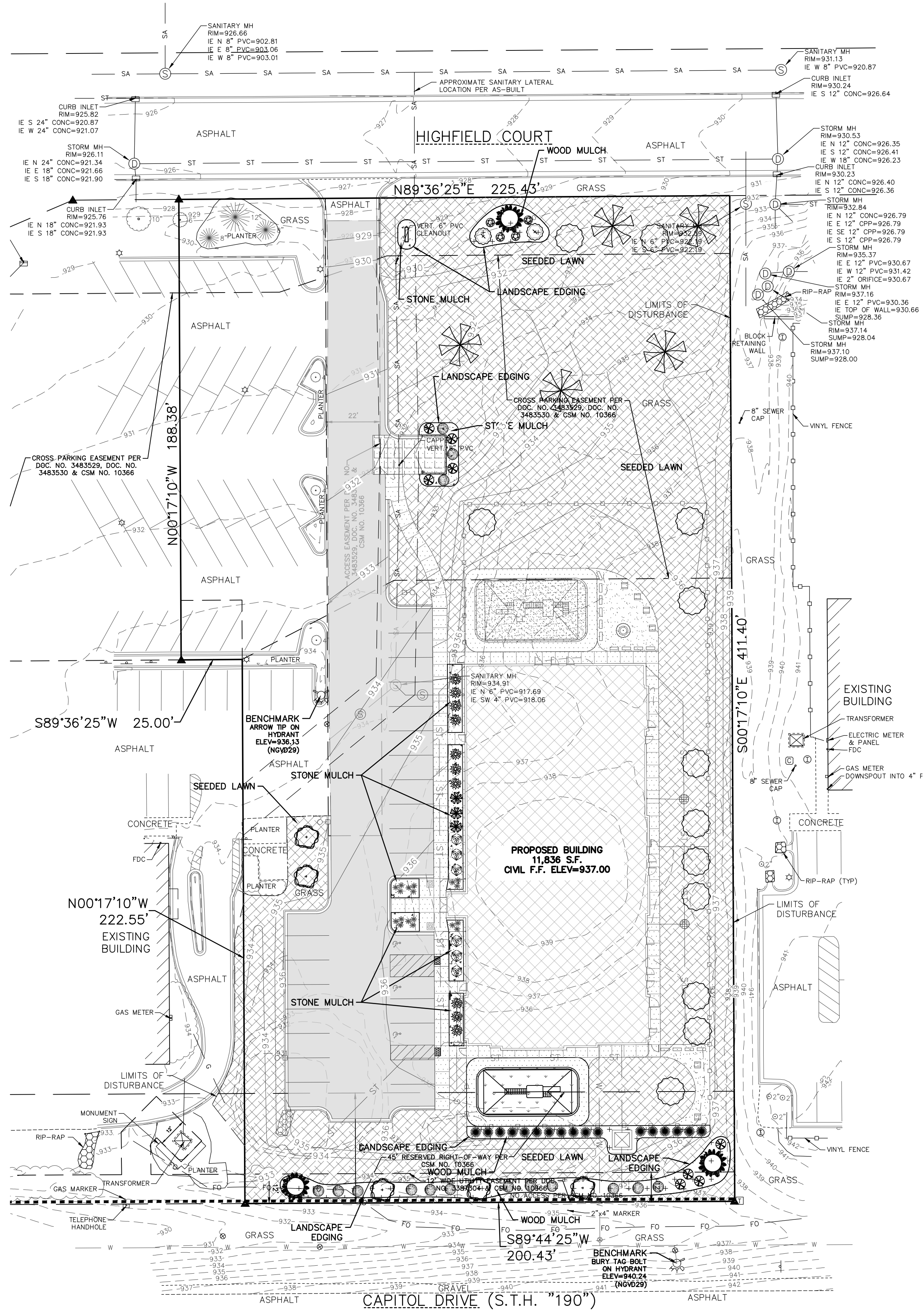
DS = DENOTES DOWNSPOUT TO
GRADE LOCATIONS. PROVIDE SPLASH BLOCKS AT
ALL DS TO GRADE LOCATIONS. SEE ARCH PLANS
FOR FINAL LOCATIONS.

CLEANOUT NOTE:

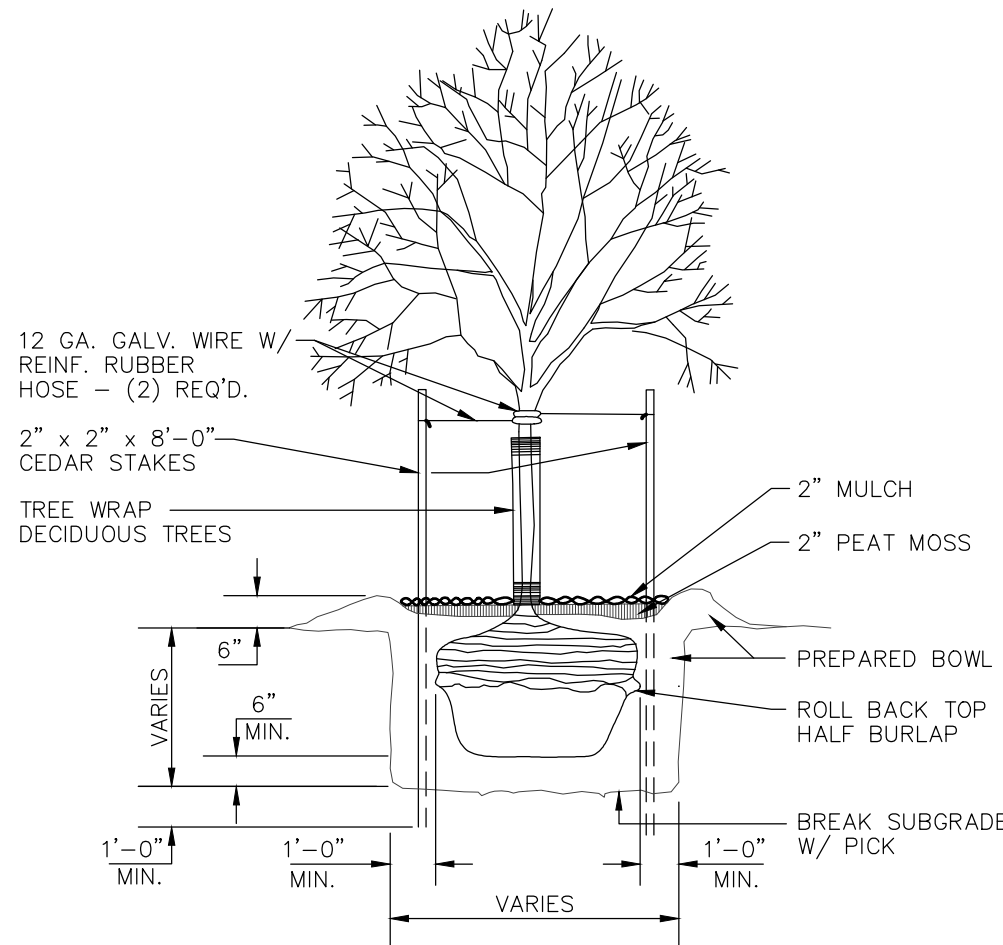
CO = DENOTES LOCATIONS WHERE
CONTRACTOR SHALL INSTALL CLEANOUTS, SEE
C0.1 FOR SPECIFICATION.



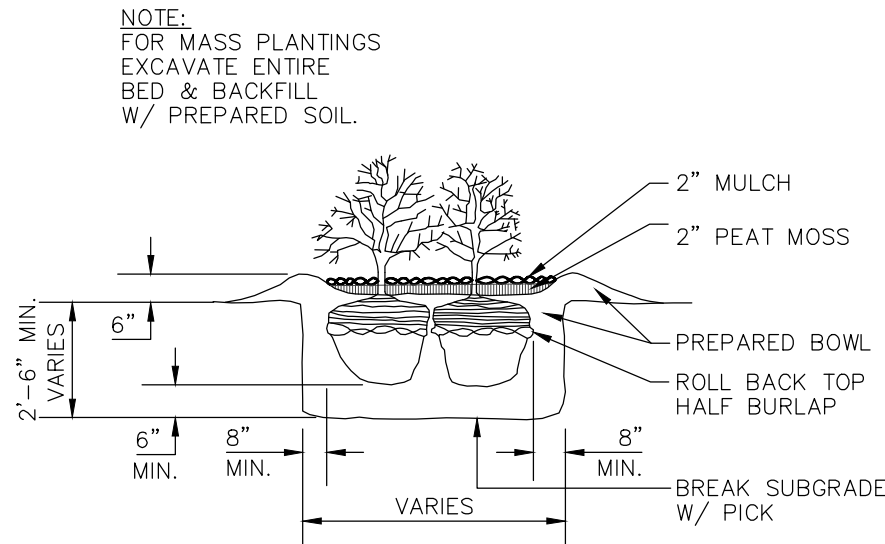
CIVIL UTILITY PLAN



SPECIFICATION NOTE:
SEE SHEET C0.1 FOR PLAN
SPECIFICATIONS AND REQUIREMENTS



TREE PLANTING DETAIL
NO SCALE



SHRUB PLANTING DETAIL
NO SCALE

LANDSCAPING PLANTING SCHEDULE					
SYMBOL	COMMON NAME	BOTANICAL NAME	PLANTED SIZE	QUANTITY	MATURE SIZE
DECIDUOUS TREES					
	Jack Flowering Pear	Pyrus calleryana 'Jazzam'	2"	5	15'-20'
	Redmond Linden	Tilia americana	2"	9	25'-30'
	Sugar Maple	Acer saccharum	3"	5	30'-45'
EVERGREEN TREES					
	Arborvitae - Nigra	Thuja occidentalis 'Nigra'	2"	2	4'-10'
	Colorado Blue Spruce	Picea pungens	4"	3	25'
DECIDUOUS SHRUBS					
	Emerald Mound Honeysuckle	Lonicera x xylosteum 'Emerald Mound'	15"-18"	10	4'-6'
	Sunrise Forsythia	Forsythia 'Sunrise'	18"-24"	6	5'
	Arrowwood Viburnum	Viburnum dentatum	30"-36"	15	6'-8'
EVERGREEN SHRUBS					
	Taunton Yew	Tauntonii	24"	4	5'-6'
	Techny Arborvitae	Thuja occidentalis	42"-48"	8	4'-6'
	Dwarf Alberta Spruce	Picea glauca 'Conica'	2"	17	6'
PERENNIALS					
	Purple Dome Aster	Aster novae-angliae 'Purple Dome'	1 gal pot	6	12"-24"
	Daylilies 'Stella de Oro'	Hemerocallis 'Stella de Oro'	1 gal pot	3	



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QUATTRO DEVELOPMENT
23877 HIGHFIELD COURT • PEWAUKEE, WI 53072

PROFESSIONAL SEAL

PRELIMINARY DATES

DEC. 12, 2022
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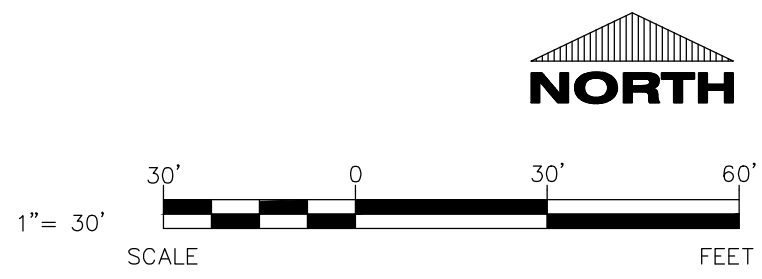
JOB NUMBER

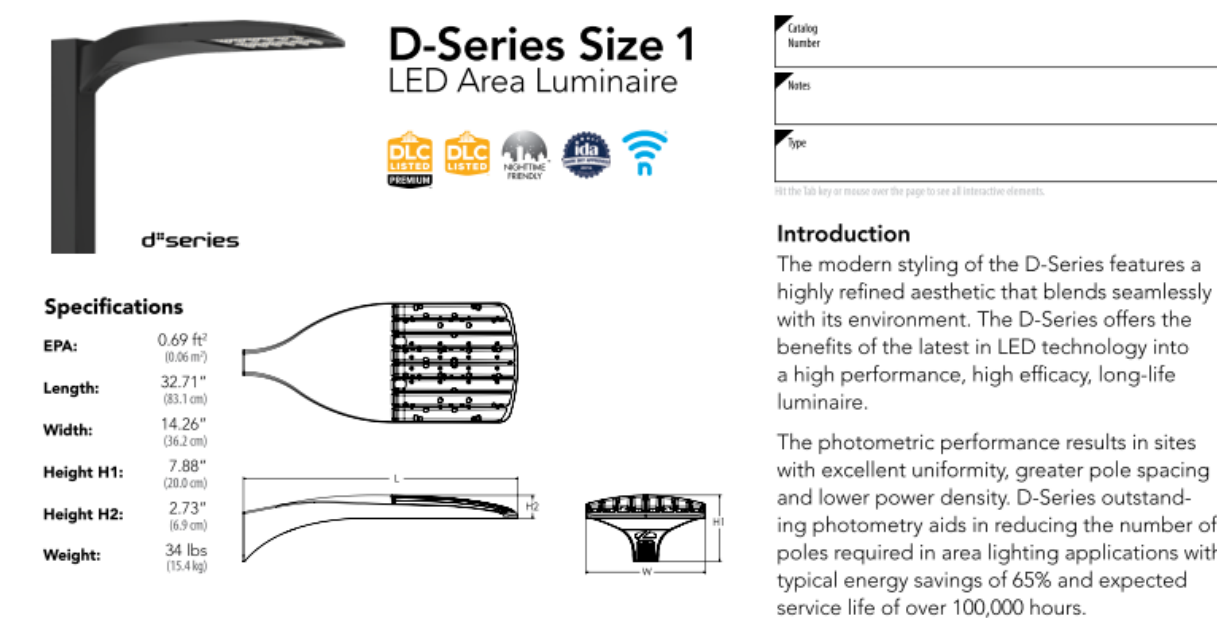
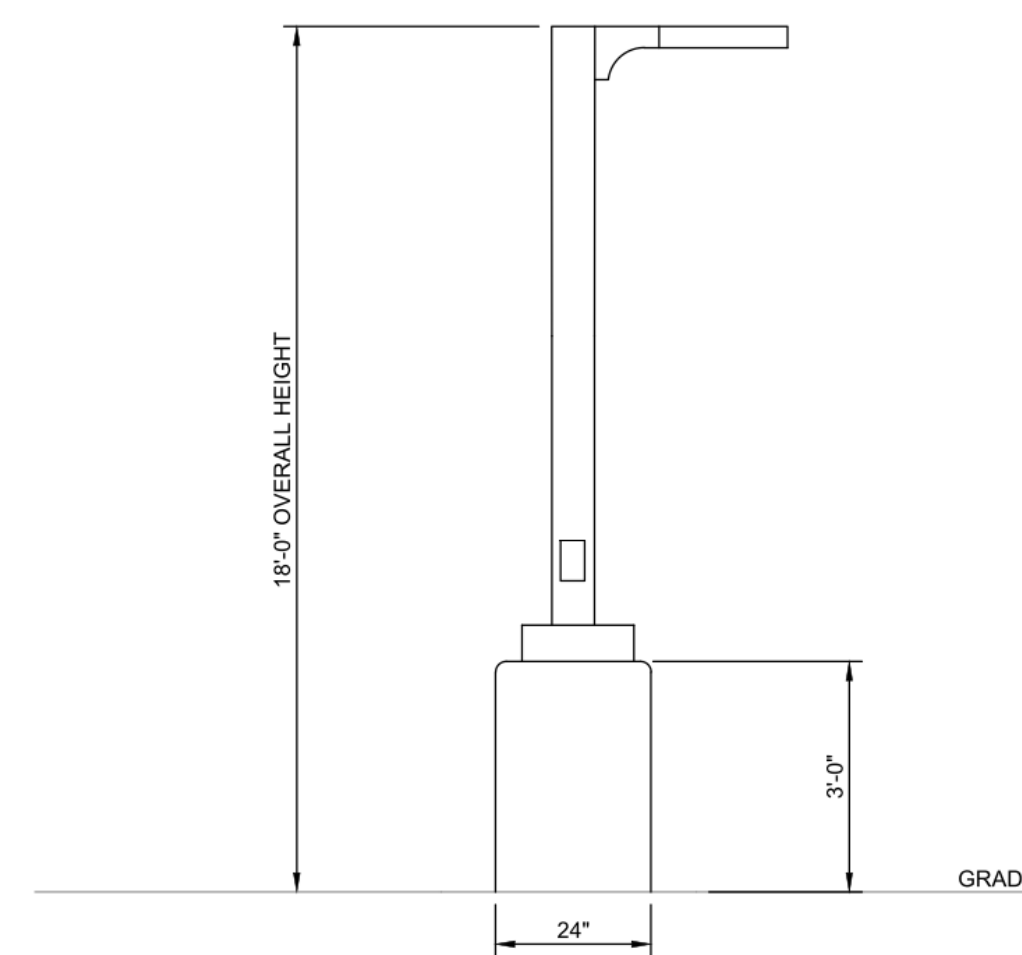
2245520

SHEET NUMBER

C1.4



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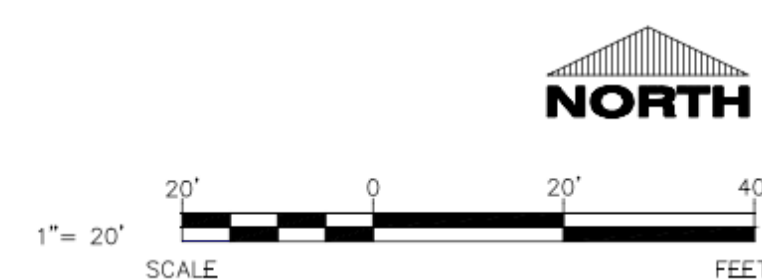
[illegible]

LIGHT POLE DETAIL

NO SCALE

Schedule										
Symbol	Label	Quantity	Manufacturer	Catalog Number	Description	Number Lamps	Lumens Per Lamp	Light Loss Factor	Wattage	Wattage
	L14	2	Lithonia Lighting	DSX1 LED P5 40K 80CRI T4M	D-Series Size 1 Area Luminaire P5 Performance Package 4000K CCT 80 CRI Type 4 Medium	1	16418	0.9	138.16	
	L14FT	1	Lithonia Lighting	DSX1 LED P5 40K 80CRI T7TM	D-Series Size 1 Area Luminaire P5 Performance Package 4000K CCT 80 CRI Forward Throw	1	16531	0.9	138.16	

Statistics						
Description	Symbol	Avg	Max	Min	Max/Min	Avg/Min
Calc Zone #1	+	1.1 fc	9.9 fc	0.0 fc	N/A	N/A
PARKING LOT	X	2.6 fc	4.9 fc	0.4 fc	12.3:1	6.5:1



CIVIL SITE PHOTOMETRIC PLAN & DETAILS



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PROJECT INFORMATION

PROPOSED EARLY CHILDHOOD SCHOOL FOR:
QUATTRO DEVELOPMENT
23877 HIGHFIELD COURT • PEWAUKEE, WI 53072

PROFESSIONAL SEAL

BREEDING DATES

SHEET ISSUE

REVISIONS

JOB NUMBER

224552

STREET NAME

○

C

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PROJECT INFORMATION

PROPOSED EARLY CHILDHOOD SCHOOL FOR:
QUATTRO DEVELOPMENT
N35 W23824 CAPTIOL DRIVE • PEWAUKEE, WI

PROFESSIONAL SEAL

SHEET DATES

HEET ISSUE DEC. 19, 202

REVISIONS

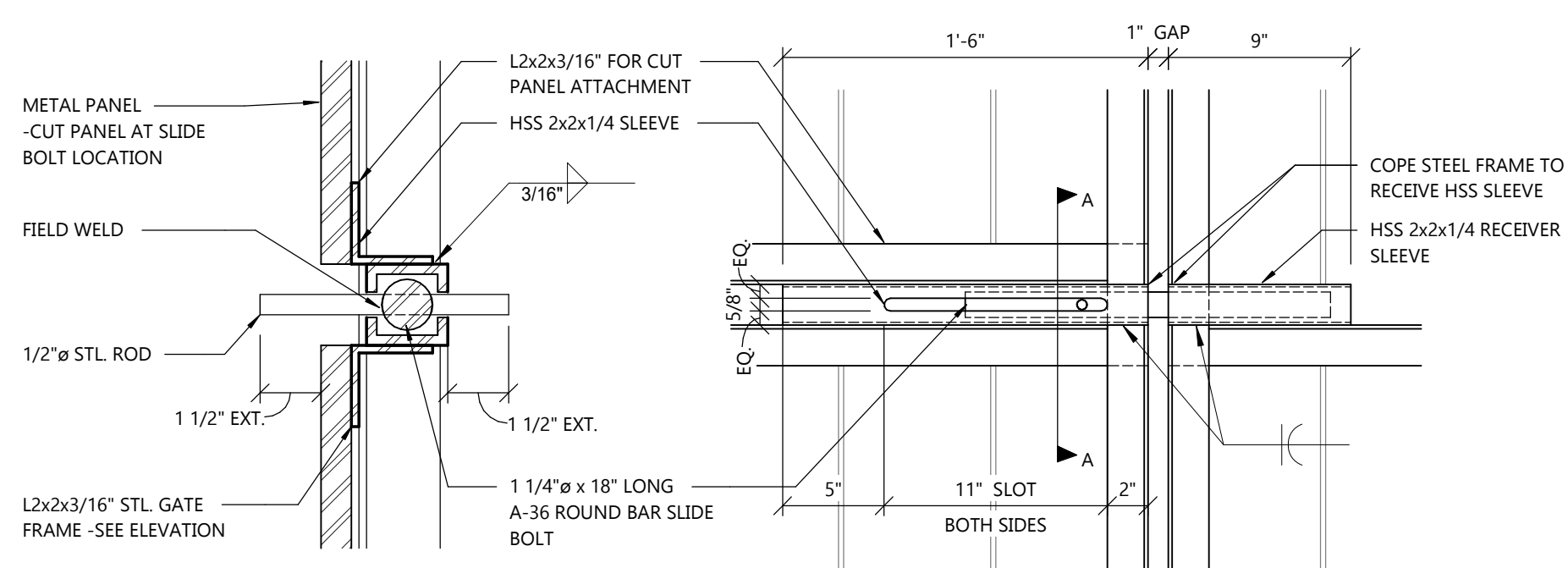
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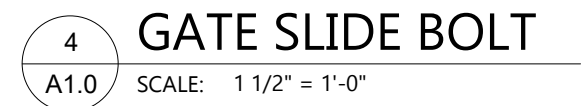
SHEET NUMBER

A1.0

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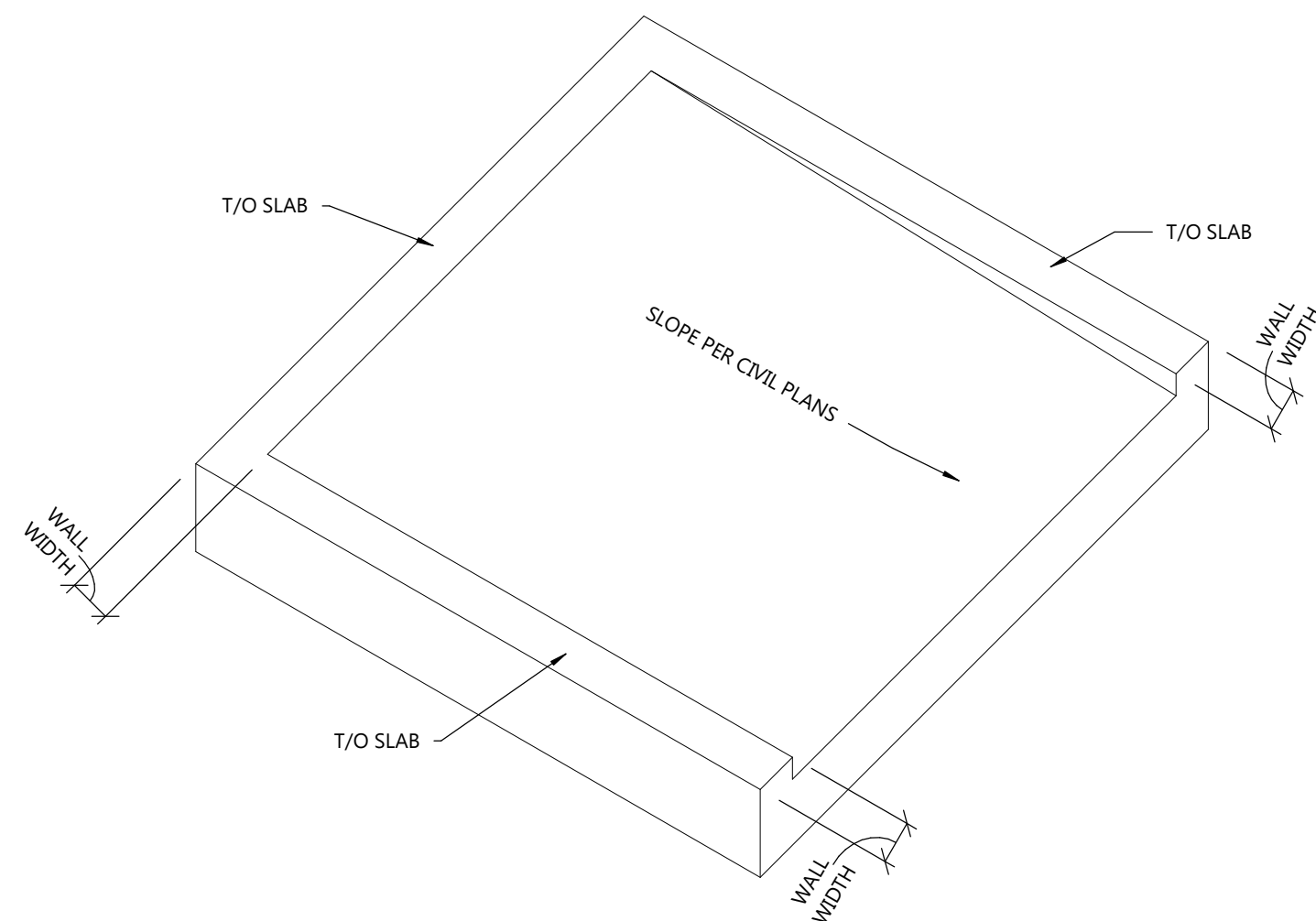


SECTION A-A
N.T.S.

ELEVATION

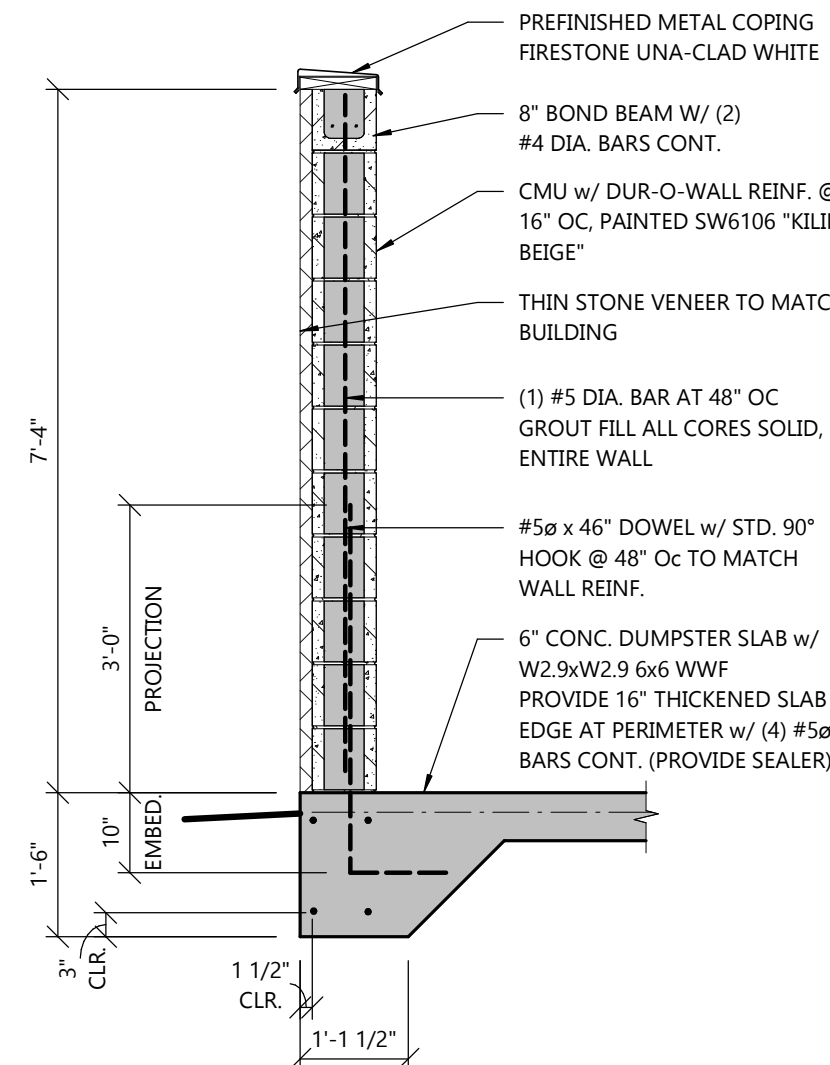
GATE SLIDE BOLT

SCALE: 1 1/2" = 1'-0"



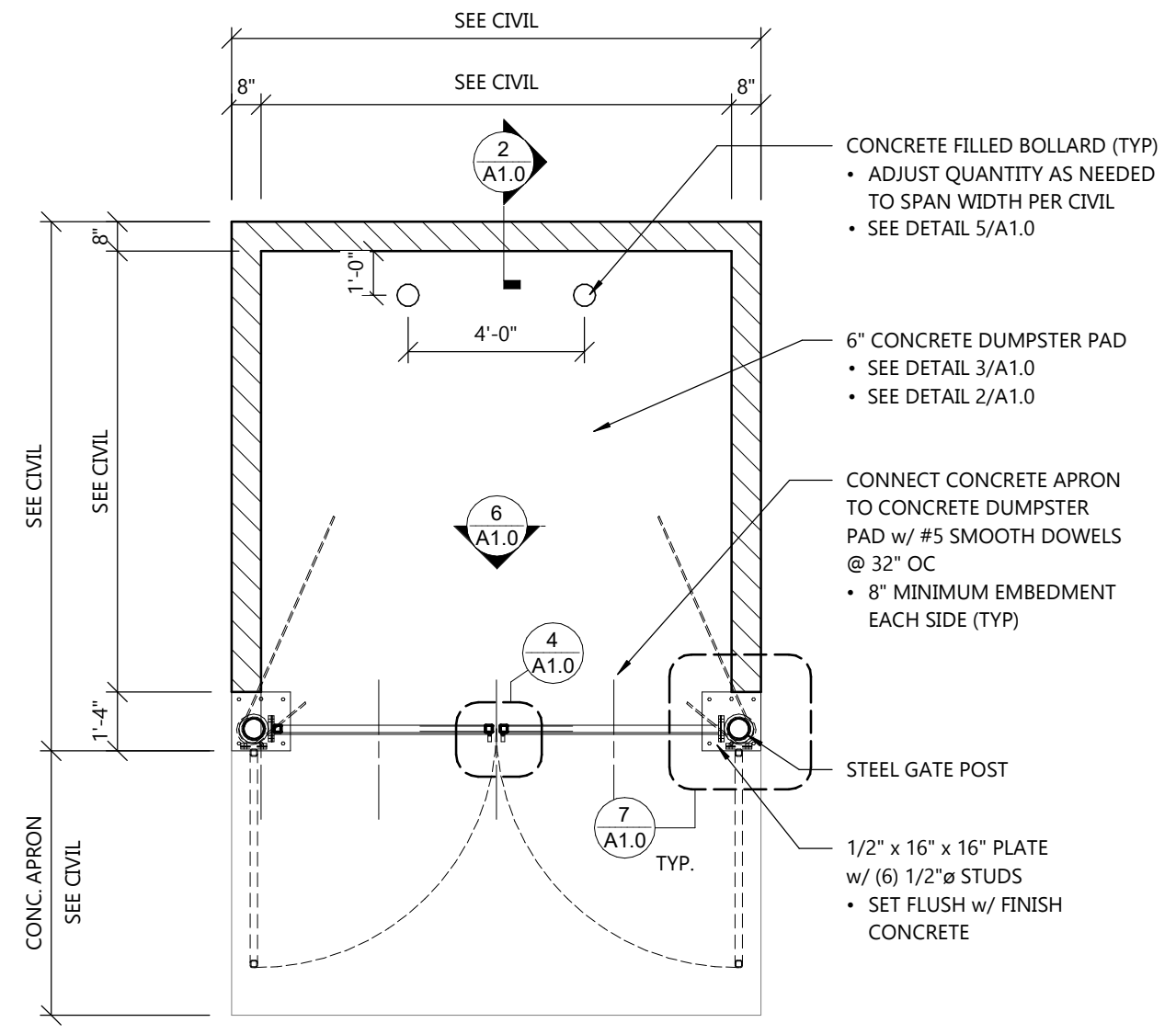
3 DUMPSTER PAD DETAIL

A1.0 NOT TO SCALE



DUMPSTER WALL SECTION

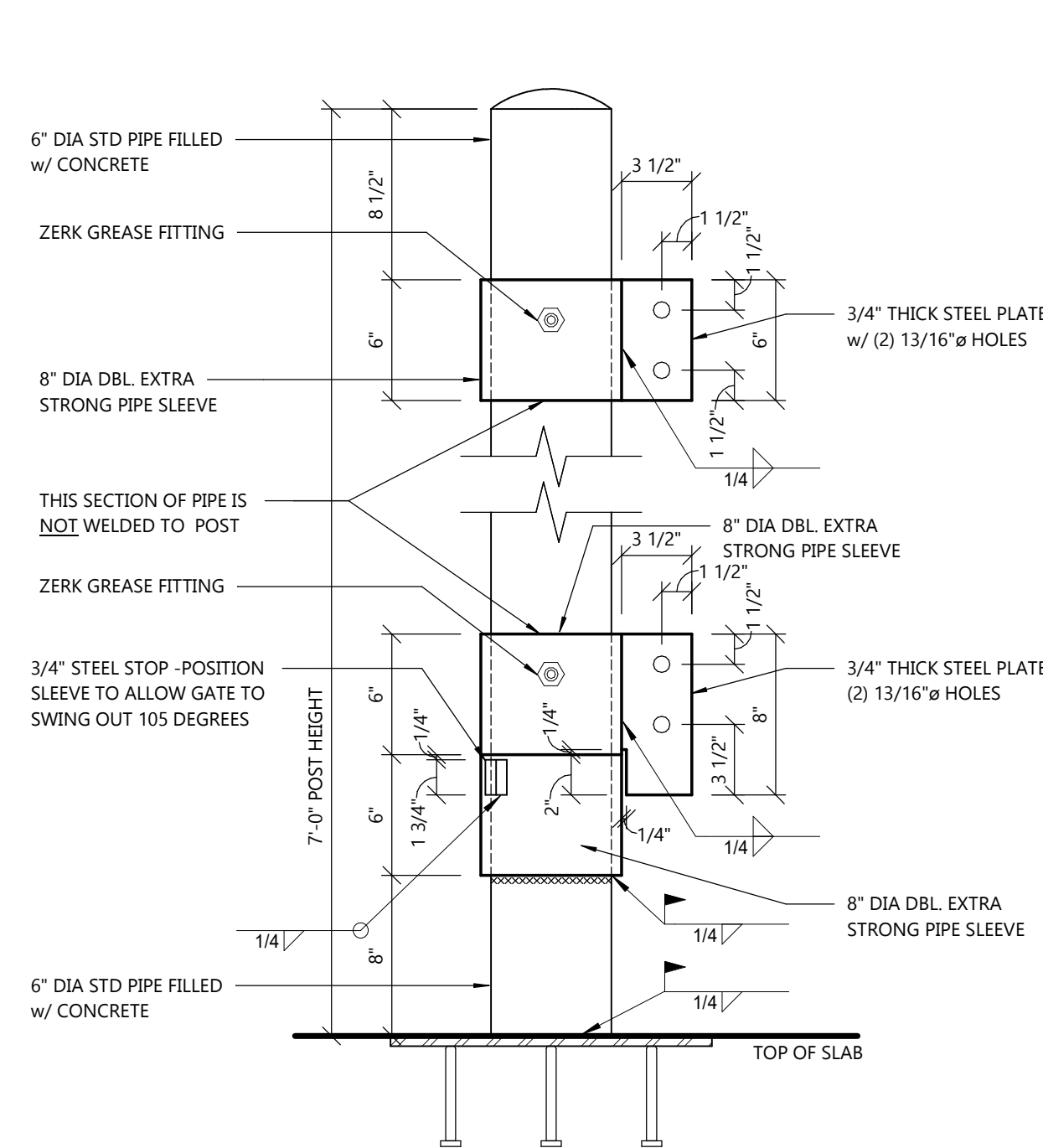
SCALE: 1/2" = 1'-0"



1 DUMPSTER PLAN

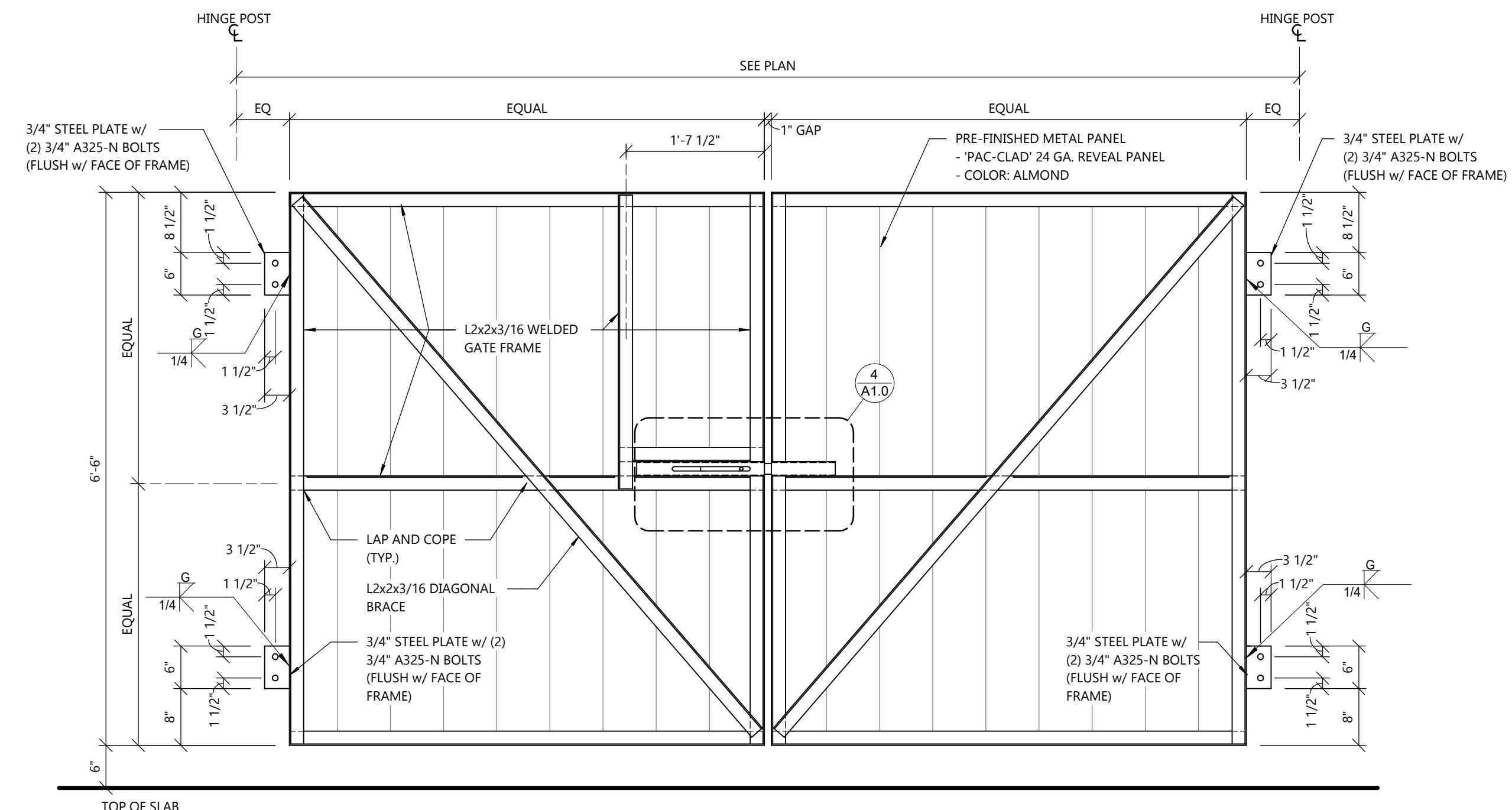
A1.0 SCALE: 1/4" = 1'-0"

ALL EXPOSED METAL SURFACES ON
DUMPSTER ENCLOSURE TO RECEIVE ONE
COAT RUST-INHIBITING PRIMER AND (2)
COATS PAINT: SW6106 "KILIM BEIGE"
(UNLESS NOTED OTHERWISE)



7 GATE HINGE DETAIL

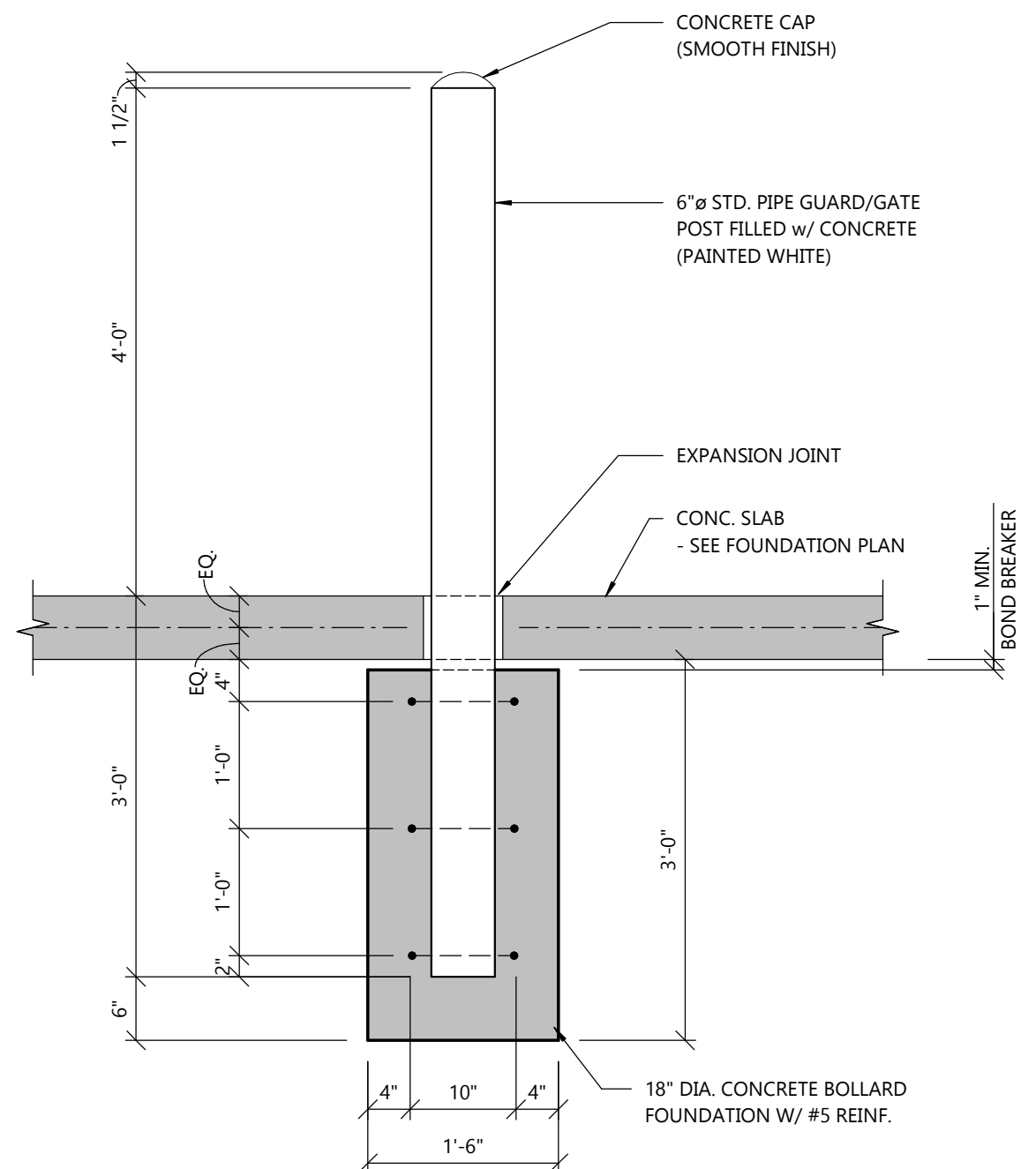
A1.0 SCALE: 1 1/2" = 1'-0"



INSIDE ELEVATION

6 DUMPSTER GATE ELEVATION

A1.0 NOT TO SCALE



5 BOLLARD DETAIL

A1.0 SCALE: 3/4" = 1'-0"

GENERAL NOTES

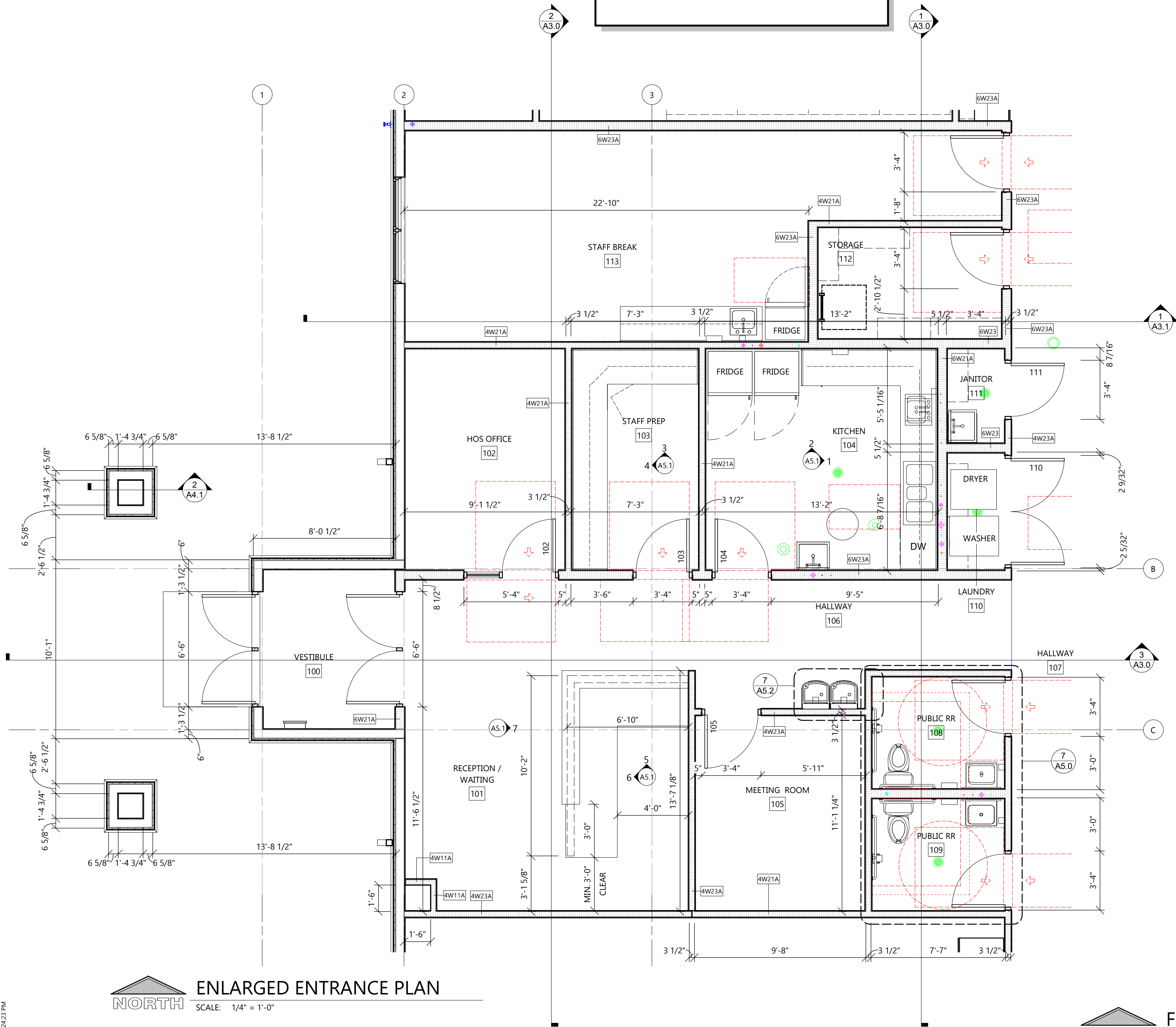
- ALL INTERIOR DIMS. ARE FROM FACE-OF-STUD TO FACE-OF-STUD.
- ALL INTERIOR WALLS TO BE 2x4 OR 2x6 @ 16" O.C. (SEE FLOOR PLAN FOR SIZE) W/ 1/2" OR 5/8" GYPSUM BOARD BOTH SIDES - EXTEND TO BOTTOM CHORD OF TRUSSES / UNDERSIDE OF DECK.
- ALL INTERIOR WALLS TO BE 3 5/8" OR 6" METAL STUDS @ 16" O.C. (SEE FLOOR PLAN FOR SIZE) W/ 5/8" GYPSUM BOARD BOTH SIDES - EXTEND TO BOTTOM CHORD OF TRUSSES.
- MISCELLANEOUS HARDWARE INCLUDED: HANDICAP HARDWARE.
- PROVIDE WOOD BLOCKING FOR ANY FURNISHINGS BY OWNER. (VERIFY LOCATIONS)
- ALL EXTERIOR WINDOWS TO HAVE ALUMINUM FLASHING RETURNS AT HEAD, JAMBS, AND SILL OF ALL WINDOWS.
- ALL EXTERIOR WINDOWS TO HAVE GYPSUM BOARD RETURNS AT HEAD AND JAMBS AND PLASTIC LAMINATE COVERED WOOD SILL.
- ALL CABINETS AND COUNTERTOPS TO HAVE PLASTIC LAMINATE FRONTS AND TOPS.
- KITCHEN AND CABINET W/ COUNTERS BY GENERAL CONTRACTOR AS REQUIRED
- ALL CABINETRY AND EQUIPMENT BY OTHERS - SEE EQUIPMENT DRAWING BY INTERIOR DESIGNER FOR REQUIREMENTS.
- FOOD PREP AREA TO BE VERIFIED W/ OWNER. PROVIDE SHOP DRAWINGS ON EQUIPMENT.

SYMBOLS LEGEND

	SEE SHEET A.X.X FOR WALL TYPES		CHILD HEIGHT TOILET
	EXIT SIGNAGE		ADULT HEIGHT TOILET
	FIRE EXTINGUISHER -SEE AS SHEETS		CHILD HEIGHT SINK
	FIRE EXTINGUISHER w/ CABINET -SEE AS SHEETS		ADULT HEIGHT SINK
	STUD WALL		
	CONCRETE WALL		
	MASONRY VENEER		
	C.M.U. WALL		
	SEE PLAN FOR ALL WALL WIDTHS		
	NEW DOOR		

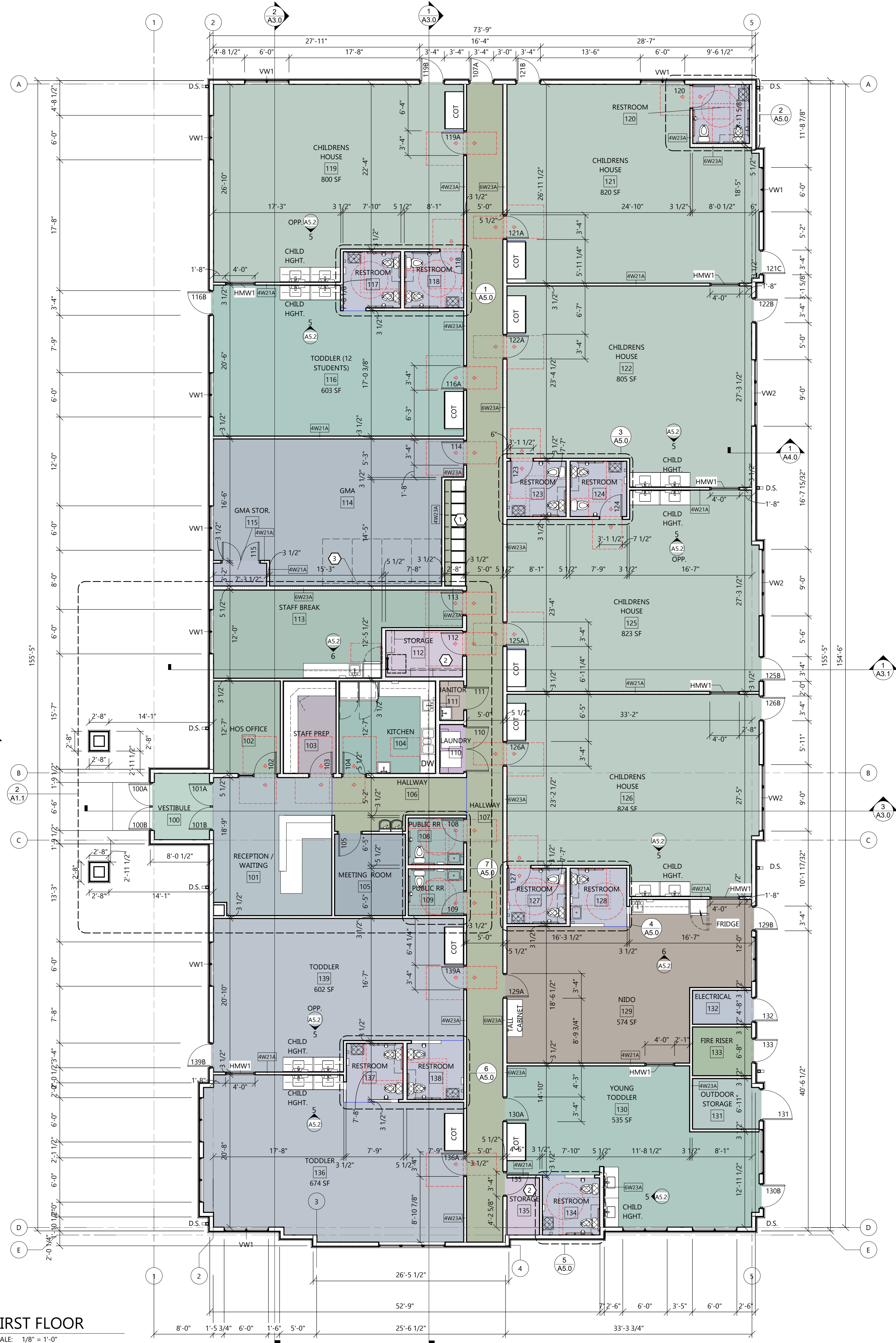
KEYED NOTES

- PROVIDE METAL LOCKERS PER HGE SPECIFICATIONS.
- PROVIDE METAL SHELVING PER HGE SPECIFICATIONS.
- CLIMBING WALL & MATS BY LL PER HGE OUTLINE SPECIFICATIONS



ENLARGED ENTRANCE PLAN
SCALE: 1/4" = 1'-0"

FIRST FLOOR
SCALE: 1/8" = 1'-0"



ARCHITECTURAL FIRST FLOOR PLAN



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PROJECT INFORMATION

PROPOSED EARLY CHILDHOOD SCHOOL FOR:
QUATTRO DEVELOPMENT
N35 W23824 CAPTIO DRIVE • PEWAUKEE, WI

PROFESSIONAL SEAL

PRELIMINARY DATES

NOV. 3, 2022
NOV. 11, 2022
NOV. 16, 2022
NOV. 17, 2022
DEC. 12, 2022
JAN. 24, 2023

JOB NUMBER

2245520

SHEET NUMBER

A1.1

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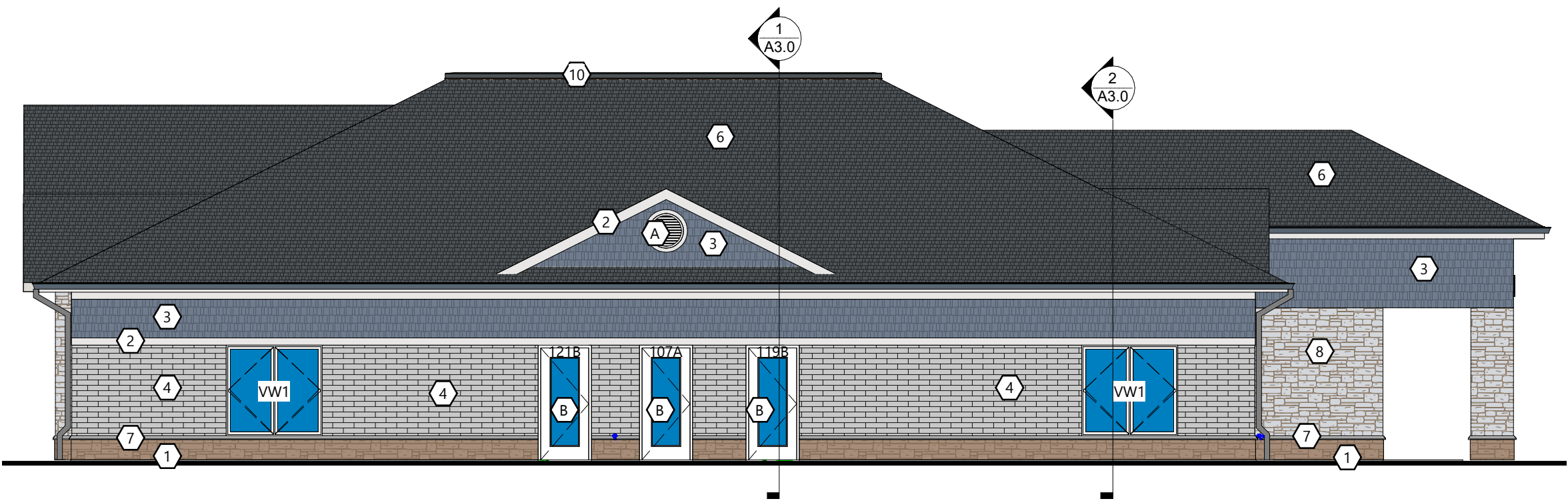


EXTERIOR FINISH KEY

- 10 PREFINISHED METAL COPING
MFR: PAC-CLAD
COLOR: MUSKET GRAY
- 9 VINYL CASEMENT WINDOW
MFR: MINNKOTA
PRODUCT: ASPEKT 1800
COLOR: WHITE
- 8 THIN STONE VENEER
MFR: LAFARGE FOX RIVER DECORATIVE STONE
PRODUCT: CHILTON COUNTRY SPIRE
- 7 PRECAST CONCRETE SILL
MFR: STONECAST
PRODUCT: LIMESTONE
- 6 ASPHALT SHINGLES
MFR: CERTANTEED LANDMARK
COLOR: GEORGETOWN GRAY
- 5 ALUM STOREFRONT WINDOW
GLASS: 1" INSUL. LOW-E GLASS
FRAMES: DARK BRONZE
- 4 THIN CONCRETE BRICK VENEER
MFR: COUNTY MATERIALS
PRODUCT: HERITAGE COLLECTION
COLOR: GRAYSTONE
- 3 SHAKE SIDING
MFR: JAMES HARDIE
PRODUCT: HARDIESHINGLE SIDING
COLOR: CADET SW9143
- 2 1x6 TRIM
MFR: JAMES HARDIE
COLOR: EXTRA WHITE SW7006
- 1 THIN STONE VENEER
MFR: LAFARGE FOX RIVER DECORATIVE STONE
PRODUCT: CUSTOM COUNTRY BLEND

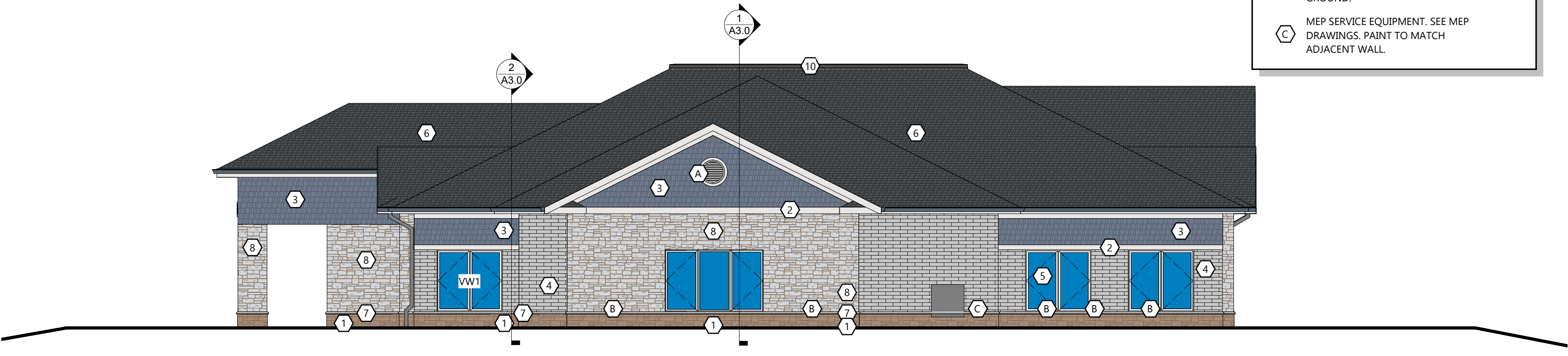
ELEVATION KEYNOTES

- A VINYL ATTIC VENT. COLOR TO MATCH
HARDIE TRIM, SW 7006 EXTRA WHITE
- B HOLLOW METAL DOOR TO BE PAINTED
PER HIGHER GROUND GUIDELINES.
COORDINATE COLOR WITH HIGHER
GROUND.
- C MEP SERVICE EQUIPMENT. SEE MEP
DRAWINGS. PAINT TO MATCH
ADJACENT WALL.



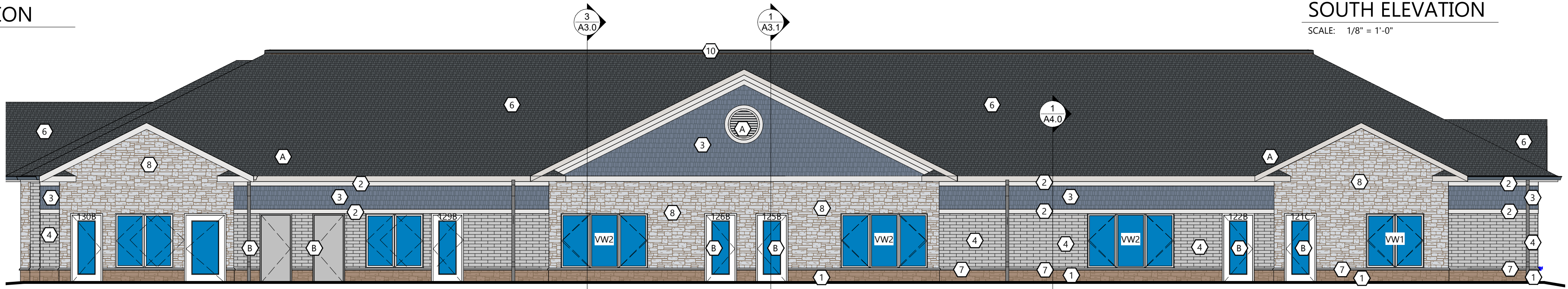
NORTH ELEVATION

SCALE: 1/8" = 1'-0"



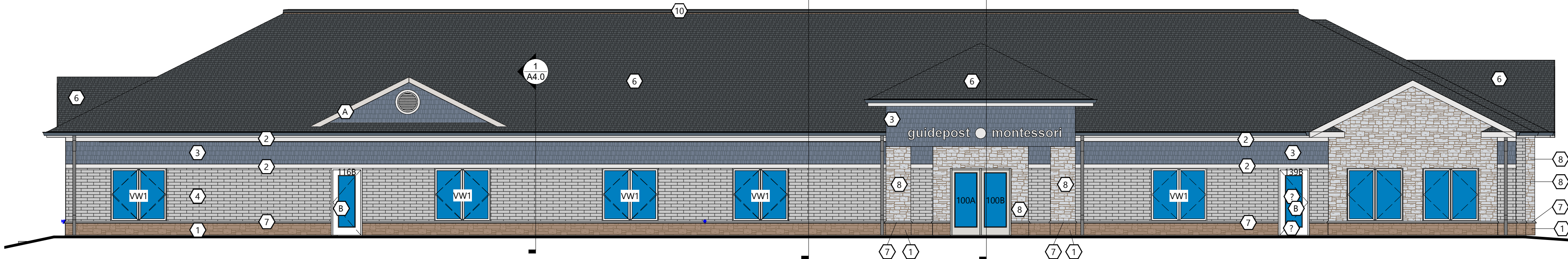
SOUTH ELEVATION

SCALE: 1/8" = 1'-0"



EAST ELEVATION

SCALE: 1/8" = 1'-0"



WEST ELEVATION

SCALE: 1/8" = 1'-0"

ARCHITECTURAL EXTERIOR ELEVATIONS



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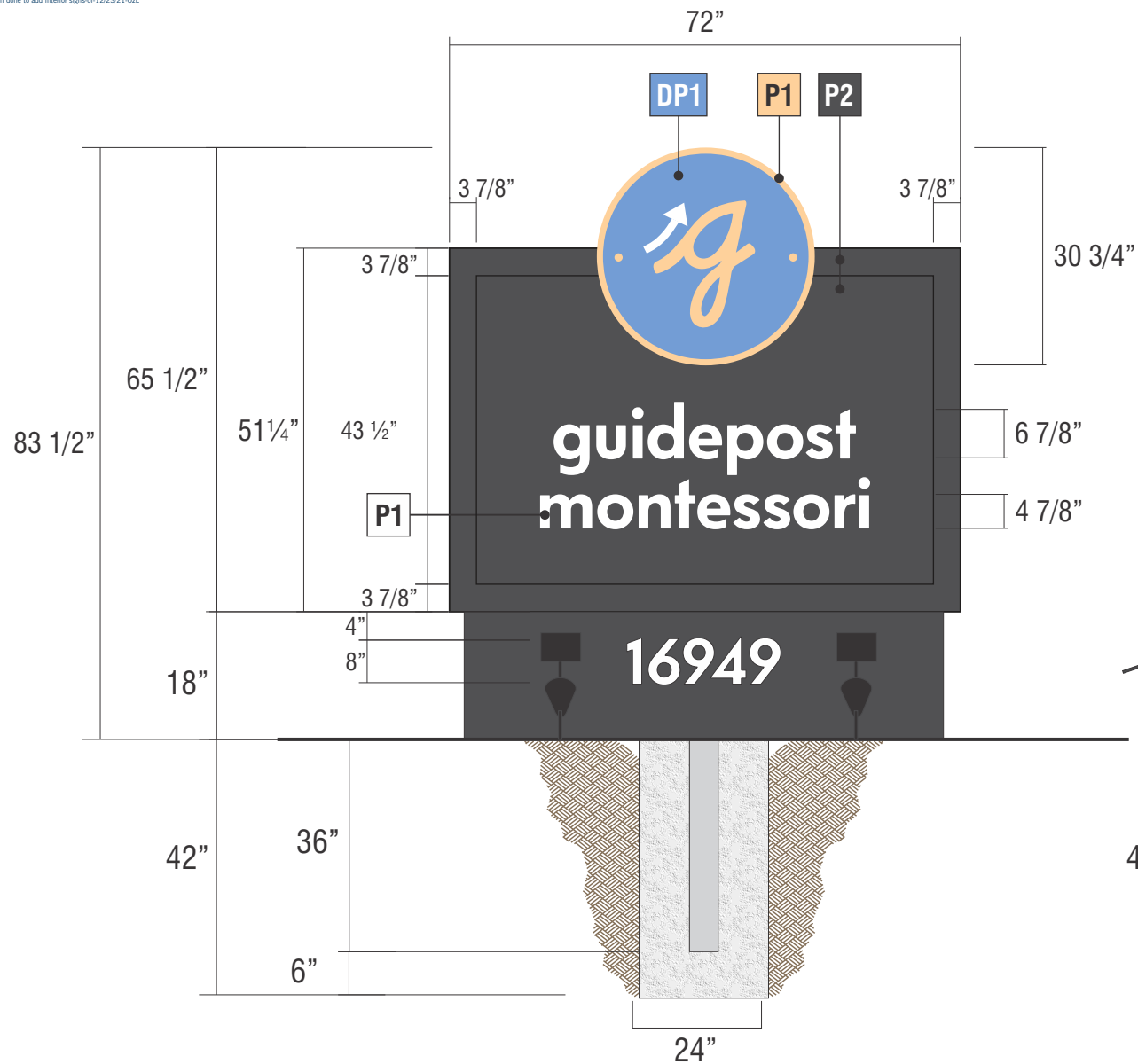
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SHEET NUMBER

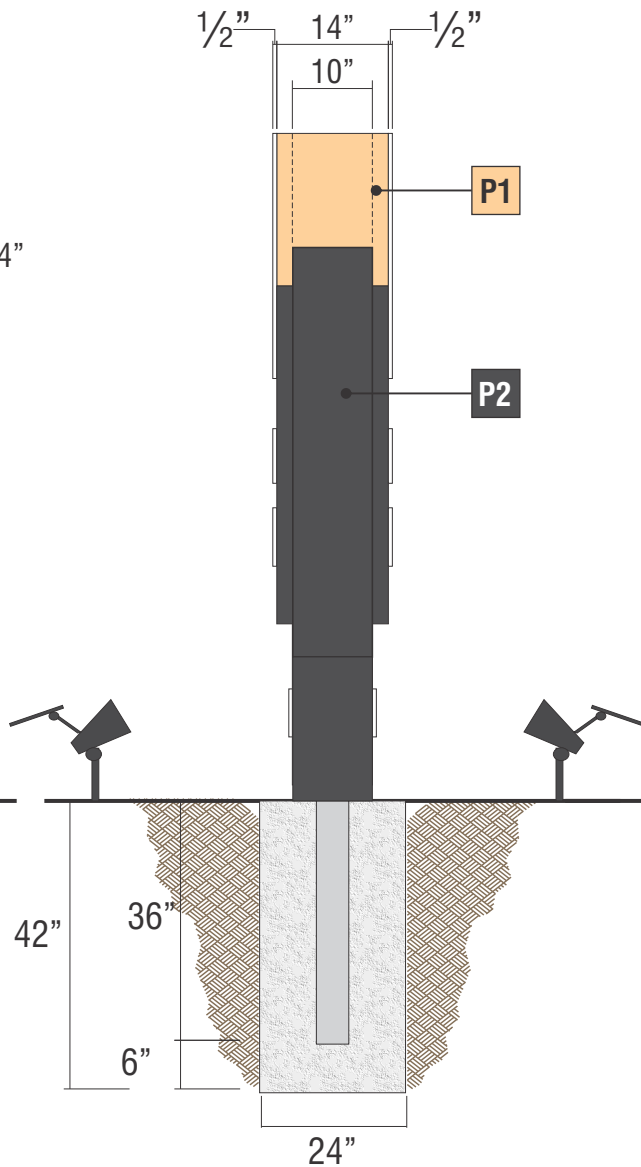
A2.0

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NOT FOR CONSTRUCTION



FRONT ELEVATION SCALE: 1/2" = 1'-0"



SIDE ELEVATION SCALE: 1/2" = 1'-0"



BACK ELEVATION SCALE: 1/2" = 1'-0"



A SIGNTYPE GPM-MON-65x72-83-DF

MANUFACTURE AND INSTALL ONE (1) DOUBLE SIDED NON-ILLUMINATED MONUMENT SIGN W/ EXTERNAL LIGHTS
CABINET: TO BE CLAD WITH .125" ALUMINUM PAINTED MATTHEWS MP07102 "DEEP RIVER", SATIN FINISH
PANEL: BREAK FORM ALUMINUM 2" DEEP PAINTED MATTHEWS MP07102 "DEEP RIVER", SATIN FINISH
LOGO: 1/2" PLEX WITH FIRST SURFACE DIGITAL PRINTED LOGO.
COPY: 1/2" PLEX PAINTED MATTHEWS SATIN WHITE.
ADDRESS NUMERALS (NON-ILLUMINATED) : 1/2" ACRYLIC, PAINTED MATTHEWS SATIN WHITE
POLE: TO BE CLAD WITH .125" ALUMINUM PAINTED DARK GREY
FOOTINGS: BY SIGNTECH ENGINEERING.

PAINT COLORS

- DP1** DIGITAL PRINT TO MATCH PMS 7453C BLUE, SATIN LAMINATE (REVERSED WEED DOTS TO SHOW THRU TAN BACKGROUND)
- P1** PAINT TO MATCH (C-0 M-20 Y-43 K-0), SATIN FINISH
- P2** PAINT MATTHEWS MP07102 "DEEP RIVER", SATIN FINISH
- P1** MATTHEWS SATIN WHITE



AMIR Solar Spotlights Outdoor = (4) Pack

TECHNICAL MEMORANDUM

Date: December 19, 2023

Prepared for: Brett Dahlman, P.E.
Quattro Development, LLC

Prepared by: Tammi Czewski, P.E., PTOE
Traffic Analysis & Design, Inc.

Subject: **Early Childhood Center – Pewaukee, WI**
Alternative Route & Recommendations for City of Pewaukee December 21,
2023 Planning Commission Meeting

INTRODUCTION

This technical memorandum was prepared to document the data sources and analysis methods for the exhibits to be presented at the December 21, 2023 City of Pewaukee, Wisconsin Planning Commission meeting. The traffic data and analysis in this memo is based on the Early Childhood School Traffic Impact Analysis (TIA) dated May 2, 2023, with additional queueing and delay studies collected in December 2023.

EXHIBIT 1 – PEAK HOUR TRAFFIC VOLUMES

This exhibit was prepared to demonstrate the existing, development, and total (existing plus development) traffic exiting from Highfield Court onto Highfield Road and exiting from Highfield Road onto eastbound Capitol Drive (STH 190). Currently, a low amount of traffic (five vehicles each in the AM and PM peak hours) turn left from Highfield Court onto Highfield Road, indicating that a low volume of existing traffic from commercial developments travel through the Broken Hill neighborhood.

EXHIBIT 2 – ALTERNATIVE ROUTES TRAVEL TIME

Early Childhood Center to Eastbound Capitol Drive (STH 190)

This exhibit was prepared to show the difference in travel distance and travel time to eastbound Capitol Drive (STH 190) via three different routes from the proposed early childhood center on Highfield Court. The three routes on this exhibit are:

- Path A (Green) – direct left-turn from Highfield Road to eastbound Capitol Drive (STH 190) (950 feet).
- Path B (Red) – neighborhood cut-through route for traffic to avoid the delays and queues at the Highfield Road/Capitol Drive (STH 190) intersection (5,885 feet).
- Path C (Blue) – Right-turn out from Highfield Drive to westbound Capitol Drive (STH 190), with a U-turn onto eastbound Capitol Drive (STH 190). U-turns are not allowed at the Capitol Drive (STH 190)/Pewaukee Road (STH 164) intersection, so westbound-eastbound U-turns would occur at Meadowcreek Drive (5,950 feet).

The total travel time for each route was estimated by adding the base travel time (distance times speed limit) plus turning movement delays calculated from the Synchro traffic analysis model, which considers queues at the stop controlled and signalized intersections. Peak hour traffic volumes from counts collected in 2022 and 2023 from both WisDOT and TADI and existing traffic control parameters were input into the model to represent existing field conditions. Additional peak hour traffic from the proposed early childhood center was then added to the various routes to represent future “build” conditions in the study area.

The comparison of route lengths and peak hour travel time is shown below in Table 1. Path A (green, direct route) is the shortest route that takes the least amount of time to reach eastbound Capitol Drive (STH 190). Path C (blue, U-turn route) is the longest route but takes less time to reach eastbound Capitol Drive (STH 190) than Path B (red, cut-through route).

Table 1. Travel Time: Development Site to Eastbound Capitol Drive (STH 190)

Route	Distance	Total Travel Time			
		AM		PM	
Path A (Green)	949 ft	1.2 min	--	1.9 min	--
Path B (Red)	5,883 ft	4.2 min	+ 3.0 min	4.2 min	+ 2.3 min
Path C (Blue)	5,949 ft	3.2 min	+ 2.0 min	3.6 min	+ 1.7 min

In summary, Path A (green) is the most direct route taking the least amount of time to reach eastbound Capitol Drive (STH 190). Cutting through the Broken Hill neighborhood, (Path B (red)), takes the longest amount of time.

EXHIBIT 3 – ALTERNATIVE ROUTES TRAVEL TIME

Site Traffic Exiting to North on Pewaukee Road (STH 164)

This exhibit was prepared to show the difference in travel distance and travel time to northbound Pewaukee Road (STH 164) via two different routes from the proposed early childhood center on Highfield Court. The two routes on this exhibit are:

- Path A – direct right turns from Highfield Road to Capitol Drive (STH 190) to Pewaukee Road (STH 164) (3,195 feet)
- Path B – neighborhood cut-through route for traffic avoiding the delays and queues at the Highfield Road/Capitol Drive (STH 190) intersection and Capitol Drive (STH 190)/Pewaukee Road (STH 164) intersection (3,330 feet).

The total travel time for each route was estimated using the same methods as described for site traffic exiting to eastbound Capitol Drive (STH 190). The comparison of route lengths and peak hour travel time for traffic exiting to northbound Pewaukee Road (STH 164) is shown below in Table 2. Path A (direct right turns) is a slightly shorter route than Path B (cut-through route) and has the shortest travel time.

Table 2. Travel Time: Development Site to Northbound STH 165

Route	Distance	Total Travel Time			
		AM		PM	
Path A (Green)	3,195 ft	1.7 min	--	2.0 min	--
Path B (Red)	3,329 ft	2.6 min	+ 0.9 min	2.5 min	+ 0.5 min

In summary, cutting through the Broken Hill neighborhood (Path B (Red)), is a longer route that takes more time to reach northbound Pewaukee Road (STH 164) than the direct route onto Capitol Drive (STH 190) (Path A (green)).

EXHIBIT 4 – HIGHFIELD COURT TURN RESTRICTION OPTION

This exhibit was prepared to show optional modifications to the Highfield Court intersection with Highfield Road to physically prevent site traffic and existing traffic exiting Highfield Court from turning left (north) to cut-through the neighborhood to reach northbound Pewaukee Road (STH 164) or eastbound Capitol Drive (STH 190). The modifications include constructing a raised concrete channelization island and installing right-turn only signs and no left-turn signs to channelize all eastbound Highfield Court traffic to southbound Highfield Road.

EXHIBIT 5 – HIGHFIELD ROAD TURN RESTRICTION OPTION

This exhibit was prepared to show the existing and future build queues and change in delays on southbound Highfield Road with the existing Highfield Road/Capitol Drive (STH 190) intersection configuration (full access) and an alternative intersection configuration that restricts southbound movements from Highfield Road to right-out only (no left-turns to Capitol Drive (STH 190)). From field videos collected in December 2023 (collected on a typical weekday from 6:00-9:00 a.m. and 3:00-6:00 p.m.), it was determined that the maximum AM or PM peak hour queues on southbound Highfield Road was four vehicles in the right-turn lane and three vehicles in the left-turn/through lane (shown in yellow).

Using the Synchro traffic model, the change in queues and delays between the existing and build conditions was noted. With the additional site traffic and the existing geometric condition, the maximum queues are expected to reach five vehicles in the right-turn lane (one additional vehicle, shown in cyan) and five vehicles in the left-turn/through lane (two additional vehicles, shown in cyan). Peak hour delays are expected to increase from 0.8 to 2.0 seconds in the right-turn lane (6-24%) and from 6.9 to 27.7 seconds in the left-turn/through lane (24-51%).

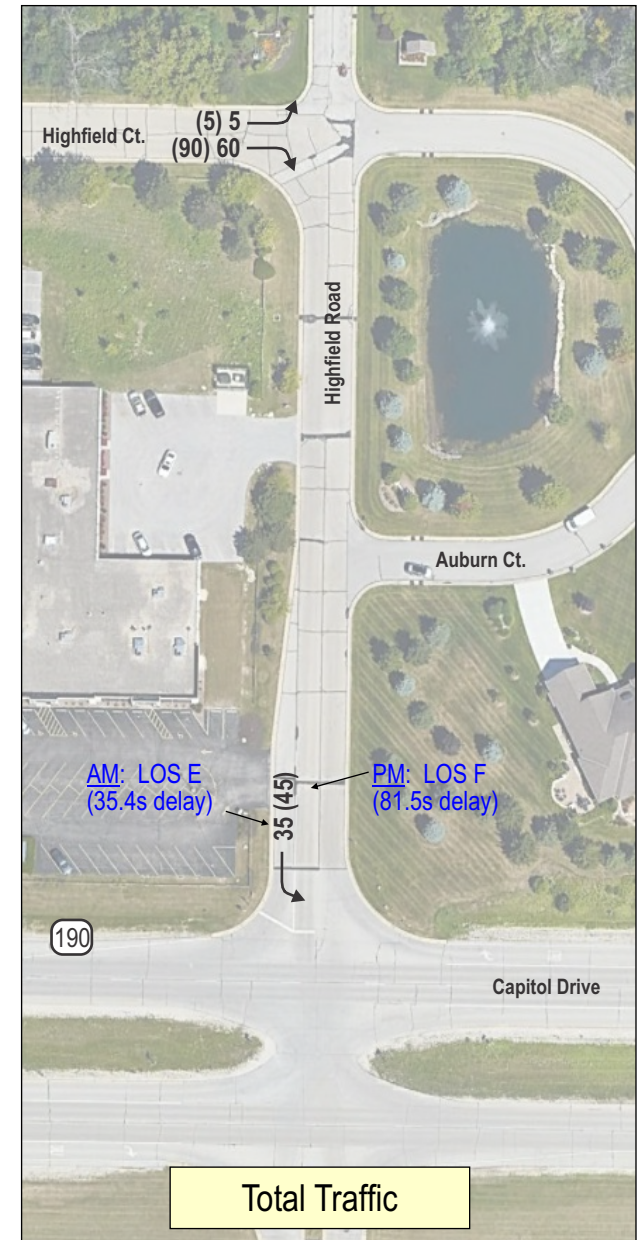
To mitigate southbound left-turn delays, the southbound approach could be configured with a raised median on Highfield Road to restrict traffic to turn right only onto westbound Capitol Drive (STH 190). Traffic heading eastbound on Capitol Drive (STH 190) would be able to U-turn at Meadowcreek Drive as shown on Exhibit 2. With this change, the southbound right-turn

delays increase by 1.7 to 22.4 seconds (13-32%) at LOS C acceptable conditions, but southbound left-turn/through delays are eliminated altogether.

EXHIBIT 6 – ALTERNATIVE LAND USES TRIP GENERATION COMPARISON

This exhibit was prepared to show trip generation estimates for a variety of commercial land use types allowed per the current zoning for the site. As shown, a typically-sized pharmacy or fast-food restaurant would generate considerably more trips (more than double) during a typical weekday than the proposed development. A strip retail plaza or high-turnover sit-down restaurant would generate a similar amount of weekday daily trips as the proposed development.

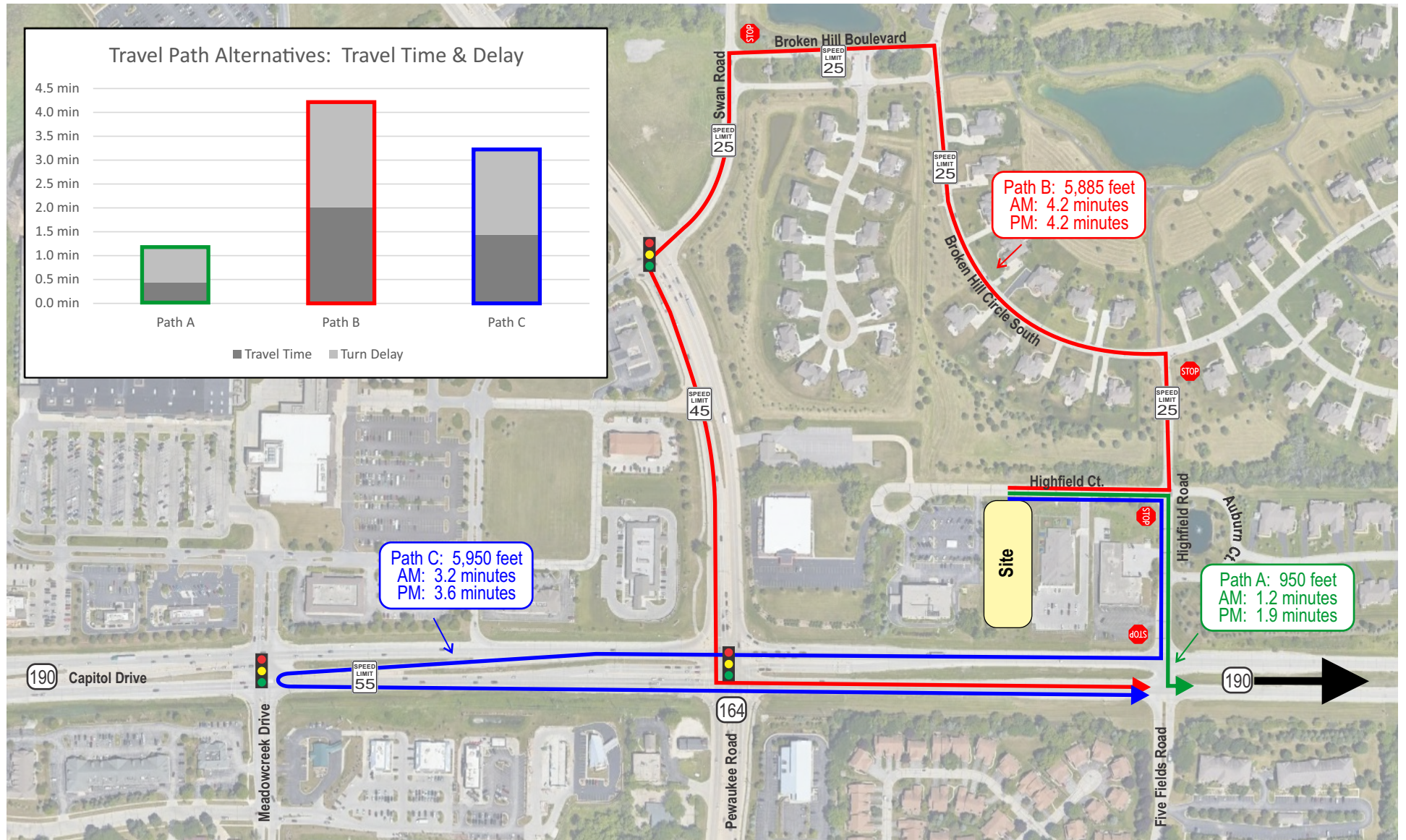
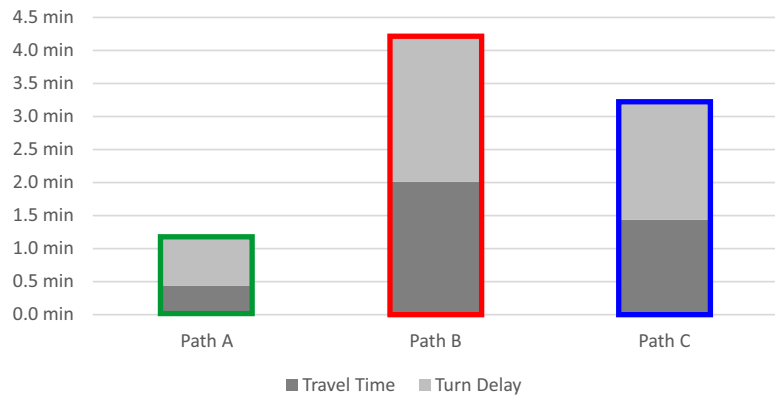
Any new use on the development site will increase trips in the study area. The mitigation measures noted in this report for Highfield Court would eliminate exiting traffic from cutting through the Broken Hill neighborhood to reach Pewaukee Road (STH 164) or Capitol Drive (STH 190).

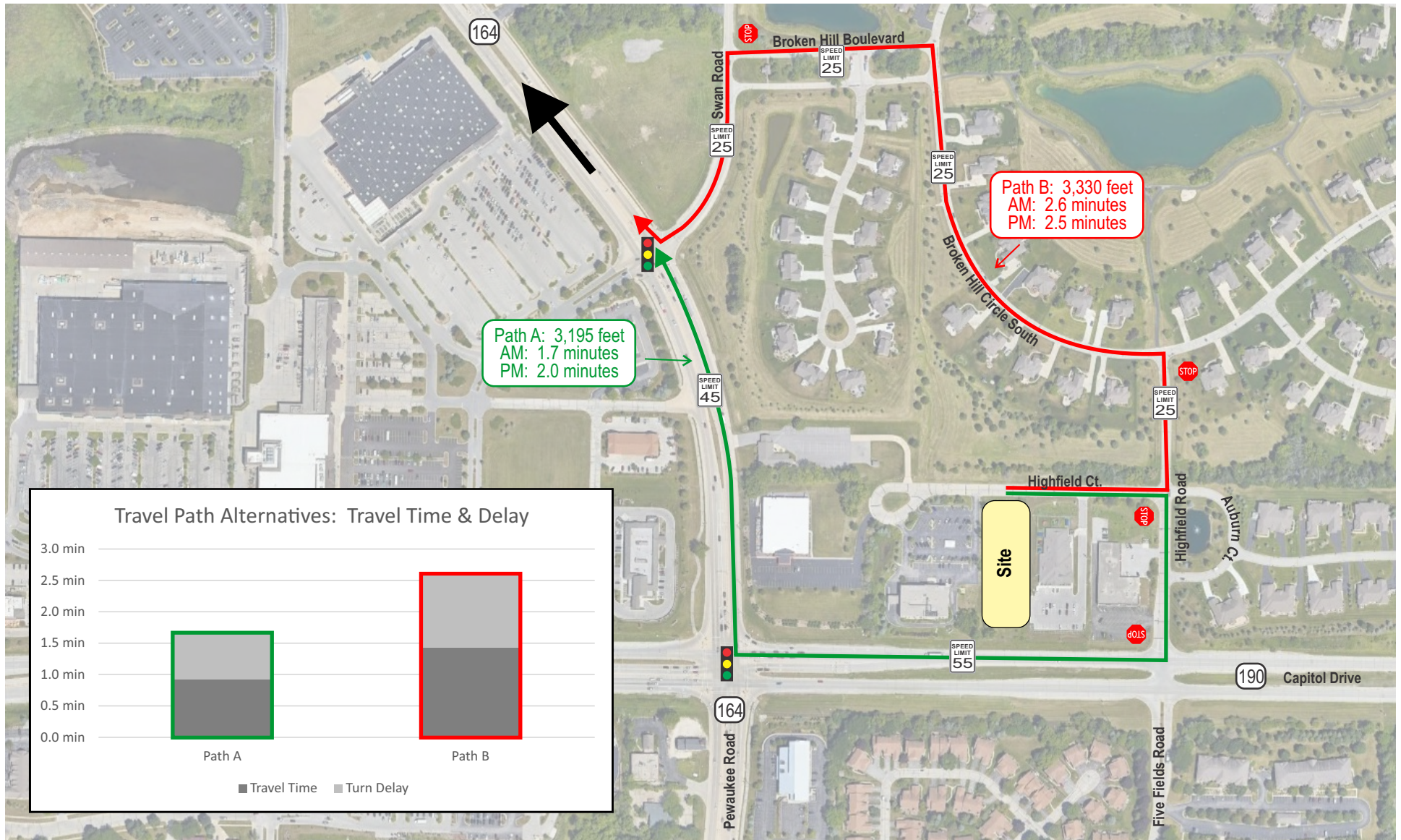


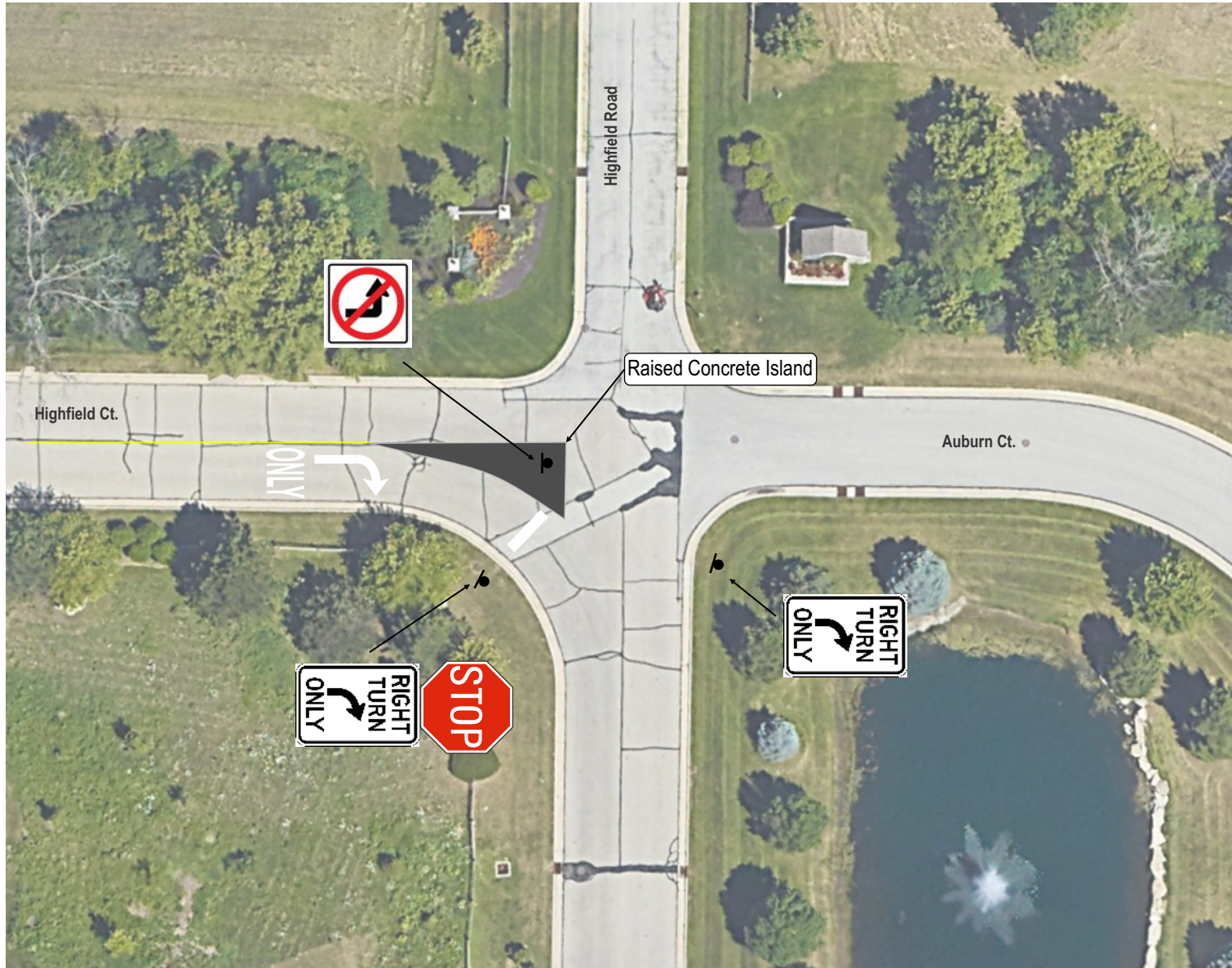
LEGEND

XX AM Peak Hour (7:30-8:30 a.m.)
(XX) PM Peak Hour (4:30-5:30 p.m.)

Travel Path Alternatives: Travel Time & Delay







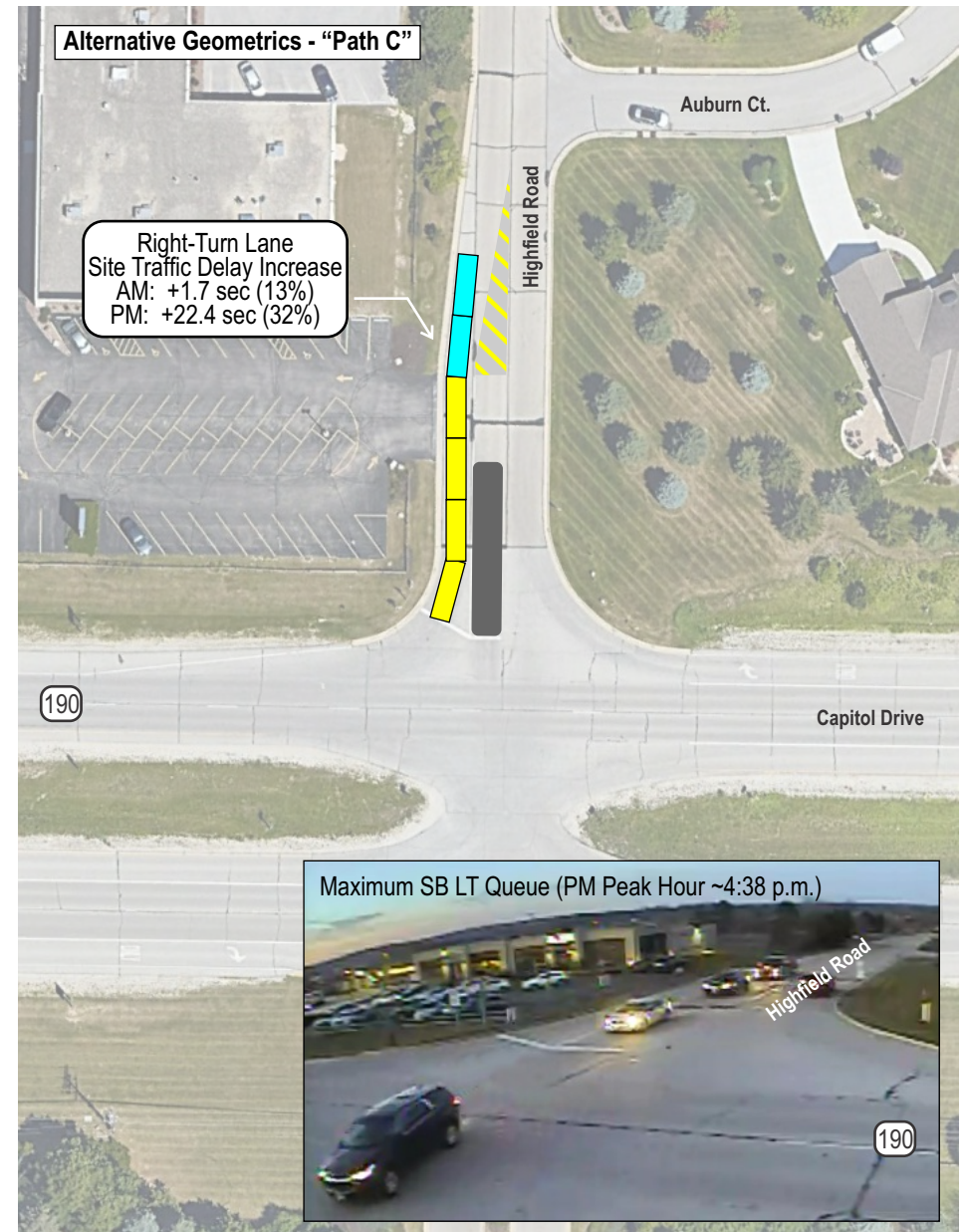
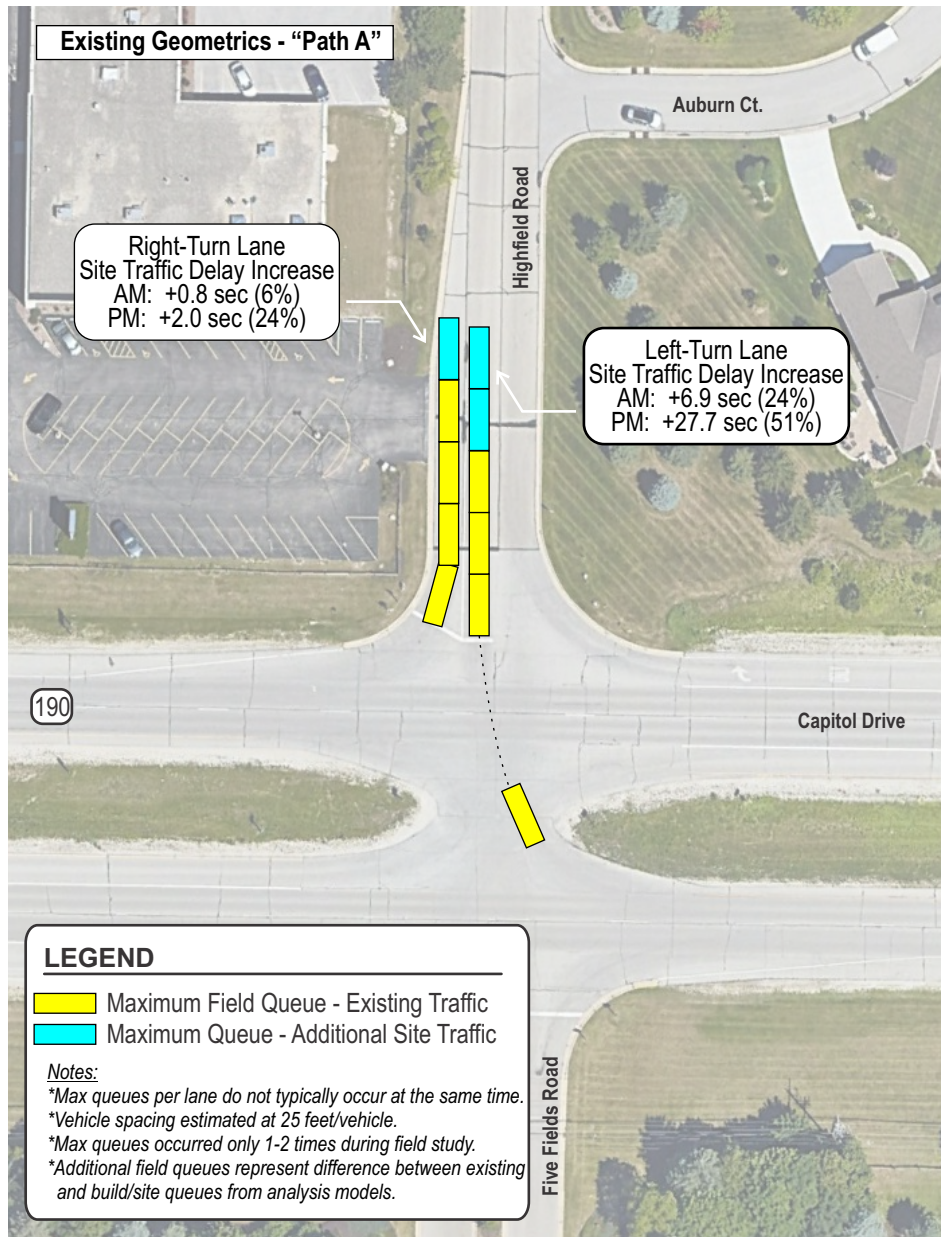


EXHIBIT 5
EXISTING AND FUTURE QUEUES
SOUTHBOUND APPROACH OF HIGHFIELD ROAD/STH 190 INTERSECTION

Comparison Trip Generation Table

Land Use	ITE Code	Size ^{2,3}	Weekday Daily	AM Peak			PM Peak		
				In	Out	Total	In	Out	Total
Early Childhood Center (Day Care Center)	565	152 Students	620 (4.09)	60 (53%)	50 (47%)	110 FCE	50 (47%)	55 (53%)	105 FCE
Medical-Dental Office Building (Stand-alone)	720	11,836 SF	430 (36.00)	30 (79%)	5 (21%)	35 FCE	15 (30%)	30 (70%)	45 FCE
Strip Retail Plaza (<40k)	822	11,836 SF	640 (54.45)	20 (60%)	10 (40%)	30 (2.36)	45 (50%)	45 (50%)	90 FCE
Pharmacy/Drugstore with Drive-Through Window	881	13,000 SF	1,410 (108.40)	25 (52%)	25 (48%)	50 (3.74)	70 (50%)	65 (50%)	135 (10.25)
General Office Building	710	11,836 SF	180 FCE	20 (88%)	5 (12%)	25 FCE	5 (17%)	25 (83%)	30 FCE
Drive-in Bank	912	3 Lanes	390 FCE	15 (61%)	10 (39%)	25 FCE	40 (49%)	40 (51%)	80 (27.07)
High-Turnover (Sit-Down) Restaurant	932	5,000 SF	540 (107.20)	30 (55%)	20 (45%)	50 (9.57)	25 (61%)	20 (39%)	45 (9.05)
Fast-Food Restaurant with Drive-Through Window	934	3,000 SF	1,400 (467.48)	70 (51%)	65 (49%)	135 (44.61)	50 (52%)	50 (48%)	100 (33.03)

¹ ITE Trip Rates (X.XX) and/or Fitted Curve Equations (FCE) are from the ITE Trip Generation Manual, 11th Edition.

² Office and retail uses were estimated to be the same size as the proposed day care use (11,836 sf).

³ The bank, pharmacy, and restaurant uses were estimated to be average typical sizes for similar existing uses in the area.

TIA TECHNICAL MEMORANDUM

Date: May 2, 2023

Prepared for: Brett Dahlman, P.E.
Quattro Development, LLC

Prepared by: Tammi Czewski, P.E., PTOE
Traffic Analysis & Design, Inc.

Subject: Early Childhood School – Pewaukee, WI
Traffic Impact Analysis

INTRODUCTION

This traffic impact analysis (TIA) technical memorandum was prepared to document the peak hour traffic impacts expected at the study intersections with buildout of the proposed early childhood school at 23877 Highfield Court in Pewaukee, Wisconsin. A map showing the development site and proposed access location is on [Exhibit 1](#).

PROPOSED DEVELOPMENT

The conceptual site plan for the proposed development is on [Exhibit 2](#). The site is zoned B-6 (Mixed-Use Business District). A conditional use is being sought for the proposed childcare development.

The early childhood school is proposed to be located between an existing office building and Capitol Drive East Kindercare on the south side of Highfield Court. Access to the early childhood school is proposed to be shared with the easternmost driveway for the existing office building. The early childhood school will provide Montessori childcare for up to 152 children (aged toddler through six years old) and 29 full-time staff in an 11,836-square foot building. The school will be open from 7:00 a.m. to 6:00 p.m. Monday through Friday. The site does not include a pick-up/drop-off lane – parents park first and then escort the children directly into the school. Due to variability in parent schedules, pick-ups and drop-off times are staggered over a two-to-three-hour period in the morning and afternoon.

STUDY AREA

Study Intersections

The study intersections are also identified on [Exhibit 1](#) and include the following:

- Pewaukee Road & Swan Road
- Highfield Road & Capitol Drive
- Highfield Road & Highfield Court
- Highfield Road & Broken Hill Circle South

The Pewaukee Road/Swan Road intersection operates with traffic signal control. All other study intersections operate with stop sign control. The existing geometrics, traffic control, posted speed limits, and distances between study intersections are shown on [Exhibit 3](#).

Study Area Roadways

Capitol Drive (STH 190) is classified as a Principal Arterial and is an east/west roadway with a four-lane cross-section (two through lanes in each direction) that is divided by a wide grass median. The 2022 annual average daily traffic (AADT) on Capitol Drive ranged from 22,800 to 26,200 vehicles per day (vpd) in the study area. The posted speed limit on Capitol Drive is 55 mph through the study area.

Pewaukee Road (STH 164) is classified as a Principal Arterial and is a north/south roadway with a four-lane cross-section (two through lanes in each direction) that is divided by a mix of concrete and grass median within the study area. There is a multi-use path along the west side of the roadway between Swan Road/Meadow Creek Market and Capitol Drive. The 2022 AADT on Pewaukee Road ranges from 17,300 vpd (north of Swan Road) to 22,600 vpd (south of Capitol Drive). The posted speed limit on Pewaukee Road is 45 mph through the study area.

Swan Road is classified as a Collector Roadway and is a north/south roadway with a two-lane cross-section (one through lane in each direction) and a 2018 AADT of 1,100 vpd north of Pewaukee Road. The posted speed limit on Swan Road is 35 mph through the study area. An advisory speed limit of 30 mph is posted at the curve just east of Pewaukee Road.

Broken Hill Circle South, Highfield Road, and Highfield Court are two-lane local roadways providing access for the Broken Hills neighborhood, the Auburn Ridge condominiums, and the commercial development along Highfield Court. The posted speed limit on these roadways is 25 mph.

EXISTING & FUTURE TRAFFIC VOLUMES

Existing Traffic Volumes

TADI collected peak hour turning movement counts at the study intersections on April 11 and 12, 2023. The collective peak hours for these intersections occur from 7:30-8:30 a.m. (AM peak hour) and from 4:30-5:30 p.m. (PM peak hour). The traffic counts were compiled for these peak hours as shown on [Exhibit 4](#). The complete traffic counts collected for this study are in [Appendix A](#).

Development Traffic Volumes

The trip generation and trip distribution for the proposed early childhood school development is on [Exhibit 5](#). Based on trip rates and fitted curve equations published in the Institute of

Transportation Engineers (ITE) *Trip Generation Manual, 11th Edition*, a daycare with up to 152 students generates 620 weekday daily trips, with 110 trips (60 in/50 out) in the AM peak hour and 105 trips (50 in/55 out) in the PM peak hour.

The distribution of these trips to the study area roadways was estimated based on the existing AADT volumes on the surrounding street system. The trip distribution was evaluated as:

- 30% to/from the west on Capitol Drive
- 25% to/from the east on Capitol Drive
- 20% to/from the north on Pewaukee Road
- 25% to/from the south on Pewaukee Road

The new trips for the proposed development were assigned to the study intersections based on this trip distribution. Trips were assigned for two scenarios:

- Scenario A: Without Cut-Through Traffic. Assumes that 100% of the site trips will access Pewaukee Road and Capitol Drive via at the Highfield Road/Capitol Drive intersection.

For Scenario A, the early childhood school traffic on Highfield Road, south of Highfield Circle is about 43% of the total traffic on that street segment. No additional site traffic is present on Highfield Road, north of Highfield Circle for Scenario A.

- Scenario B: With Cut-Through Traffic. Assumes that 17% of the site trips will access the Pewaukee Road and Capitol Drive via Swan Road (“cutting through” the neighborhood to the north). This percentage was estimated from the existing Highfield Court peak hour volumes turning to and from the north on Highfield Road.

For Scenario B, the early childhood school traffic on Highfield Road, south of Highfield Circle is about 38% of the total traffic on that street segment. The early childhood school traffic on Highfield Road, north of Highfield Circle is about 20% of the total traffic on that street segment.

The site traffic assignments are shown on [Exhibit 6](#) (Scenario A) and on [Exhibit 7](#) (Scenario B).

Future Build Traffic Volumes

The early childcare school new trips were added to the Existing traffic volumes to generate the Build traffic volumes. The Build traffic volumes are shown on [Exhibit 8](#) (Scenario A) and on [Exhibit 9](#) (Scenario B).

PEAK HOUR TRAFFIC OPERATIONS & QUEUES

The study intersections were analyzed using the Synchro 11 traffic analysis model (outputs based on the *Highway Capacity Manual, 6th Edition*) and the peak hour turning movement volumes estimated for each intersection. Intersection operation is defined by “level of service.” Level of Service (LOS) is a quantitative measure that refers to the overall quality of flow at an intersection ranging from very good, represented by LOS ‘A’, to very poor, represented by LOS ‘F’. For the purposes of this study, LOS D or better was used to define acceptable peak hour

operating conditions. The LOS definitions for signalized and unsignalized intersections are in [Appendix B](#).

Existing Traffic Operations

The Existing traffic volumes were evaluated with the existing geometrics and traffic control at each study intersection. The Existing traffic weekday AM and PM peak hour LOS, delays, and queues for each study intersection are shown on [Exhibit 10](#). The Synchro capacity analysis worksheets for the Existing traffic analyses are in [Appendix B](#). The Synchro analysis shows that the southbound left-turn movement from Highfield Road onto Capitol Drive operates at LOS F with 53 seconds of delay (less than one minute per vehicle) during the weekday PM peak hour. Traffic queues for this movement are relatively low, however, at 1-2 vehicles per lane. All other study intersections operate acceptably at LOS D or better during the peak hours. All traffic movements at the Highfield Road intersections with Highfield Court and Broken Hills Circle South operate at LOS A.

Build Traffic Operations

The Build traffic operations were evaluated with the additional trips generated from the proposed early childhood school on Highfield Court. Build traffic volumes for Scenario A (no cut-through traffic) results in increased AM and PM delays (LOS E and/or F) at both northbound and southbound movements from Highfield Road and Five Fields Road. Delays for these movements are approximately 0.5-1.5 minutes or less per vehicle, and queues are 1-3 vehicles per lane. With the additional site traffic, the Highfield Road intersection with Highfield Ct. operates at LOS B or better during the peak hours.

To avoid these peak hour delays, some development traffic may choose to travel northward and use Swan Road to access Pewaukee Road and Capitol Drive. Build traffic volumes for Scenario B (with cut-through traffic), the traffic impact to the Highfield Road/Capitol Drive intersection is less, with northbound delays from Five Fields Road remaining at LOS D or better for both the AM and PM peak hours, and southbound delays from Highfield Road remaining at LOS D for the AM peak hour. Southbound delays from Highfield Road are expected to remain at LOS F during the weekday PM peak hours, but with only 17 seconds of additional delay and approximately one additional vehicle in queue. Even with the additional traffic traveling northward for Scenario B, all movements at the Highfield Road/Broken Circle South intersection remain at LOS A during the peak hours, and no significant increase in delays or queues are expected at the Swan Road/Pewaukee Road intersection.

The Build traffic weekday AM and PM peak hour LOS, delays, and queues for each study intersection are shown on [Exhibit 11](#) (Scenario A) and [Exhibit 12](#) (Scenario B). The Synchro capacity analysis worksheets for the Build traffic analyses are in [Appendix C](#) (Scenario A) and [Appendix D](#) (Scenario B).

TRAFFIC GAP STUDY

A traffic gap study was completed for the Highfield Road/Capitol Drive intersection to field-verify the traffic operations evaluated in Synchro. The intersection is located just east of the

Capitol Drive/Pewaukee Road traffic signal which creates one-way/eastbound traffic gaps on Capitol Drive through the Highfield Road intersection.

The length of gaps, in time, between vehicles traveling along eastbound and westbound Capitol Drive at the median opening for Highfield Road were collected during the weekday AM and PM peak hours. Critical gap and follow-up times for vehicle turning movements were then calculated based on equations in the Highway Capacity Manual (HCM). These equations include adjustment factors for heavy vehicles, grade, divided roadways, and intersection geometry. The gap analysis worksheet calculations and compiled gap counts are in [Appendix E](#).

Equivalent gaps (or capacity of gaps) are calculated by adding the number of gaps available for only one vehicle to turn, gaps long enough for two vehicles to turn, and gaps long enough for three vehicles to turn (without counting the same gaps twice). The equivalent gaps represent the number of additional turning movements that could be made above and beyond the existing turning movements already occurring. [Table 1](#) summarizes the findings of the gap study. The table shows the equivalent gaps calculated from the field counts, the new site trips assigned to the turning movements at Highfield Road, and the excess gaps available for additional turning movements above those numbers.

Table 1. Capitol Drive at Highfield Road Traffic Gaps

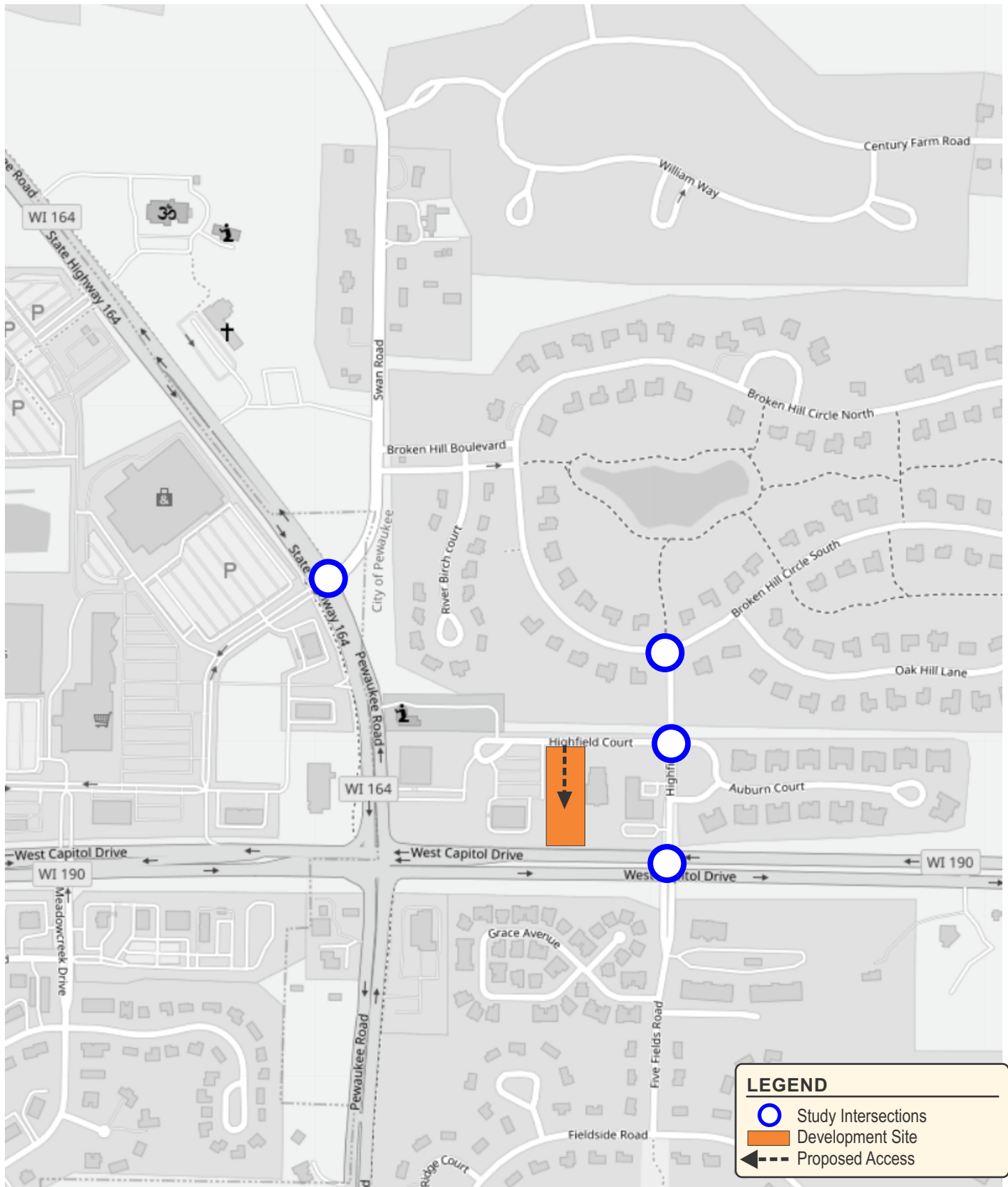
Intersection Direction	Peak Hour	Equivalent Gaps	Net New Site Trips	Excess Gaps
Scenario A: Without Cut-Through Traffic to Swan Road				
SB Left Turn from Highfield Road ¹	AM	160	10	150
	PM	171	15	156
SB Right Turn from Highfield Road	AM	214	40	174
	PM	160	40	120
EB Left Turn from Capitol Drive	AM	404	45	359
	PM	278	40	238
Scenario B: With Cut-Through Traffic to Swan Road				
SB Left Turn from Highfield Road ¹	AM	160	5	155
	PM	171	10	161
SB Right Turn from Highfield Road	AM	214	35	179
	PM	160	35	125
EB Left Turn from Capitol Drive	AM	404	35	369
	PM	278	30	248

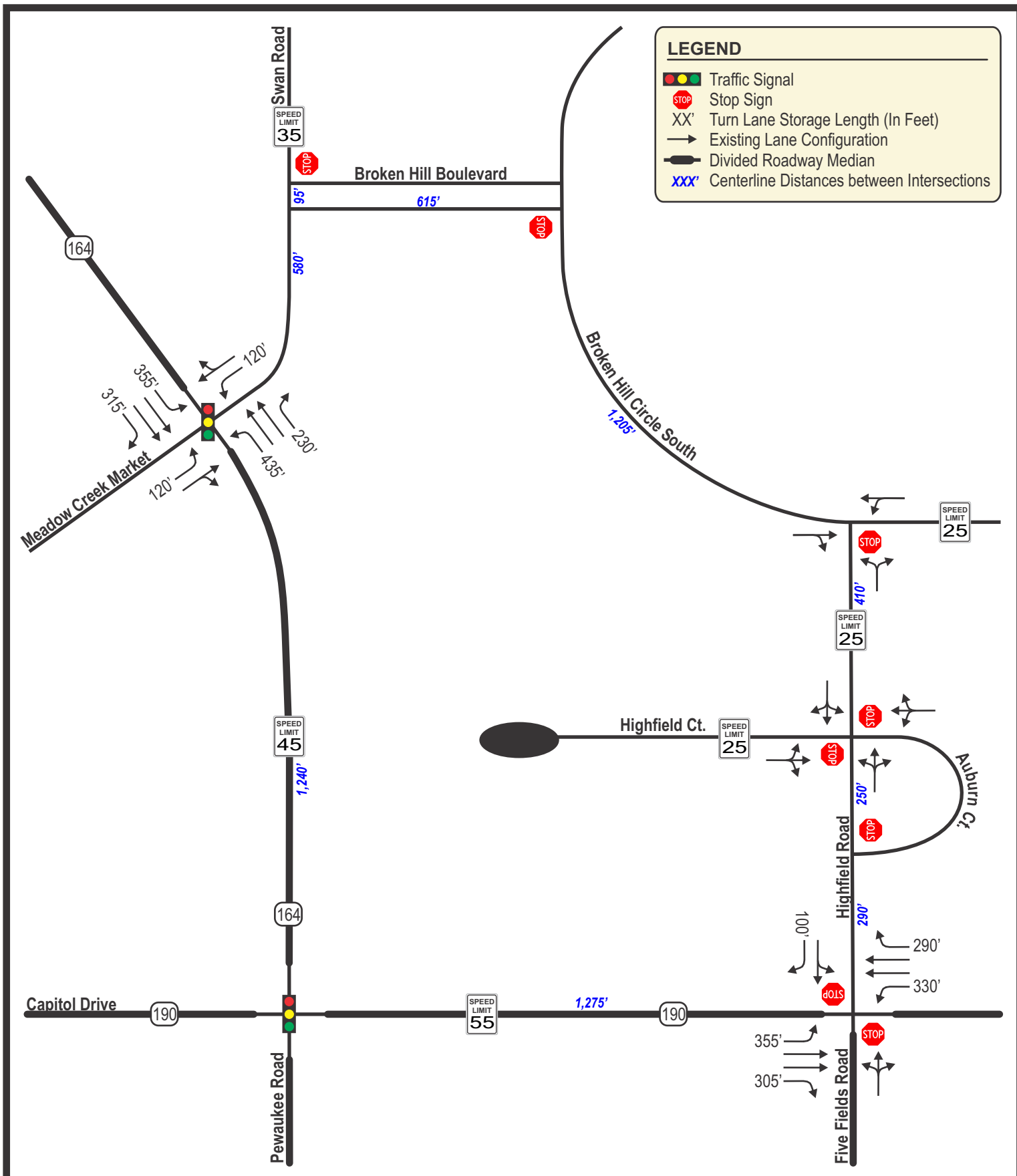
¹ SB left turn is completed in a two-stage gap maneuver (1 stage: SB Through to Median, 2nd stage, SB left turn from Median). The lower number of gaps between the stage 1 and stage 2 movements is reported in the table.

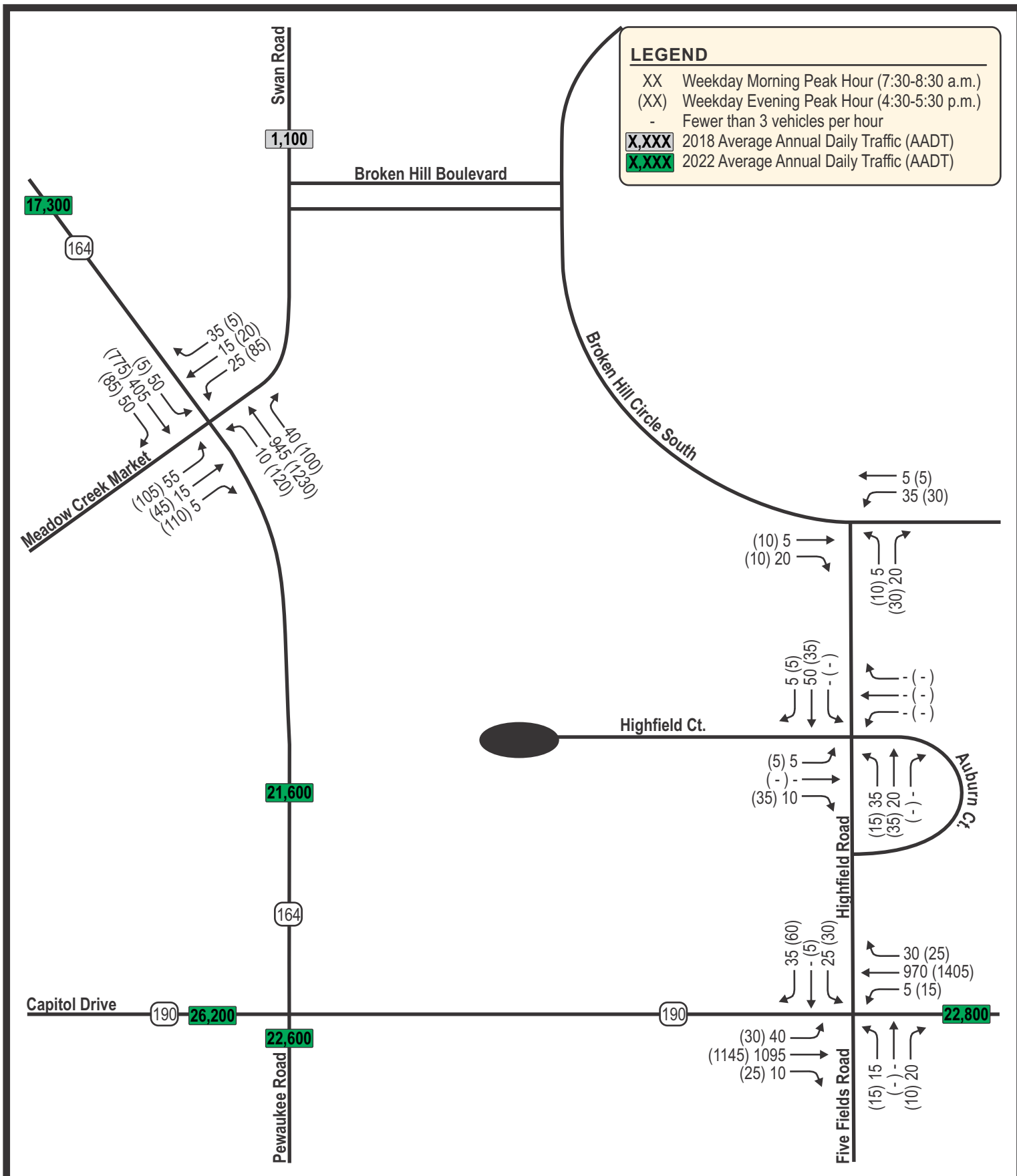
As shown, there are anywhere from 120-369 excess gaps for additional turning movements beyond the existing and site trip traffic assigned to the Capitol Drive/Highfield Road intersection (Scenarios A & B). These excess gaps affirm that the adjacent signal provides one-way gaps in traffic along Capitol Drive that are not reflected in the Synchro traffic model. With a significant amount of excess gaps, traffic movements from Highfield Road may operate with less delays than shown in the LOS tables.

RECOMMENDATIONS & CONCLUSIONS

No modifications to the study intersections are recommended to accommodate the additional traffic from the proposed early childhood school on Highfield Court in Pewaukee, Wisconsin. Although the traffic signal at the Capitol Drive/Pewaukee Road intersection creates gaps for traffic to make southbound left-turn movements from Highfield Court with reasonable delays (per the gap study), some traffic may “cut-through” the neighborhood roads to the north of the site and use Swan Road to access Pewaukee Road and then Capitol Drive. The additional “cut-through” traffic is not expected to significantly increase delays at the Swan Road/Pewaukee Road intersection, and traffic operations at the Highfield Road/Broken Hills Circle South intersection is expected to remain at LOS A during the peak hours for all turning movements.







Day Care Trip Generation Table¹

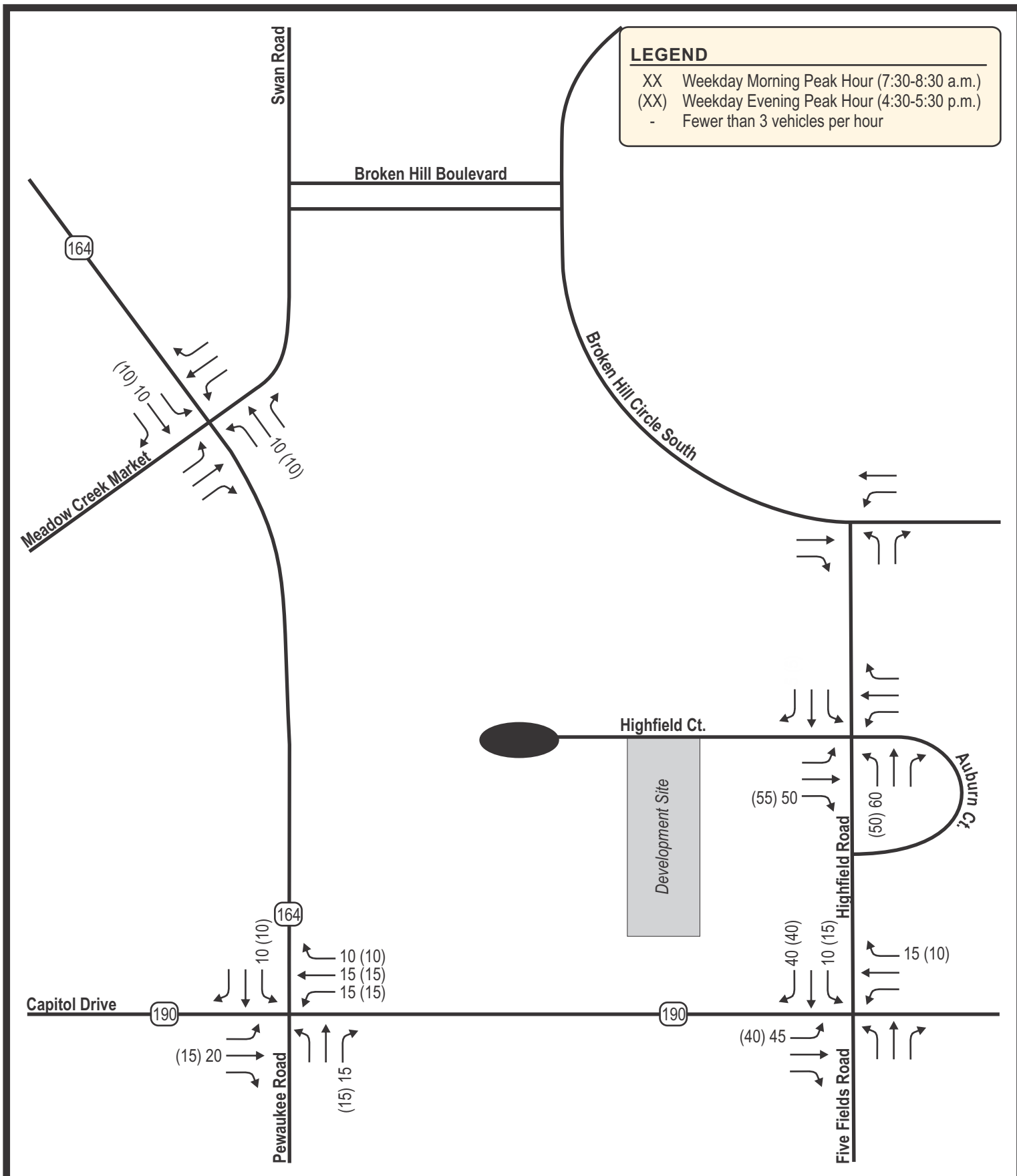
Land Use	ITE Code	Proposed Size	Weekday Daily	AM Peak			PM Peak		
				In	Out	Total	In	Out	Total
Day Care Center	565	152 Students	620 (4.09)	60 (53%)	50 (47%)	110 FCE	50 (47%)	55 (53%)	105 FCE
Total New Trips			620	60	50	110	50	55	105

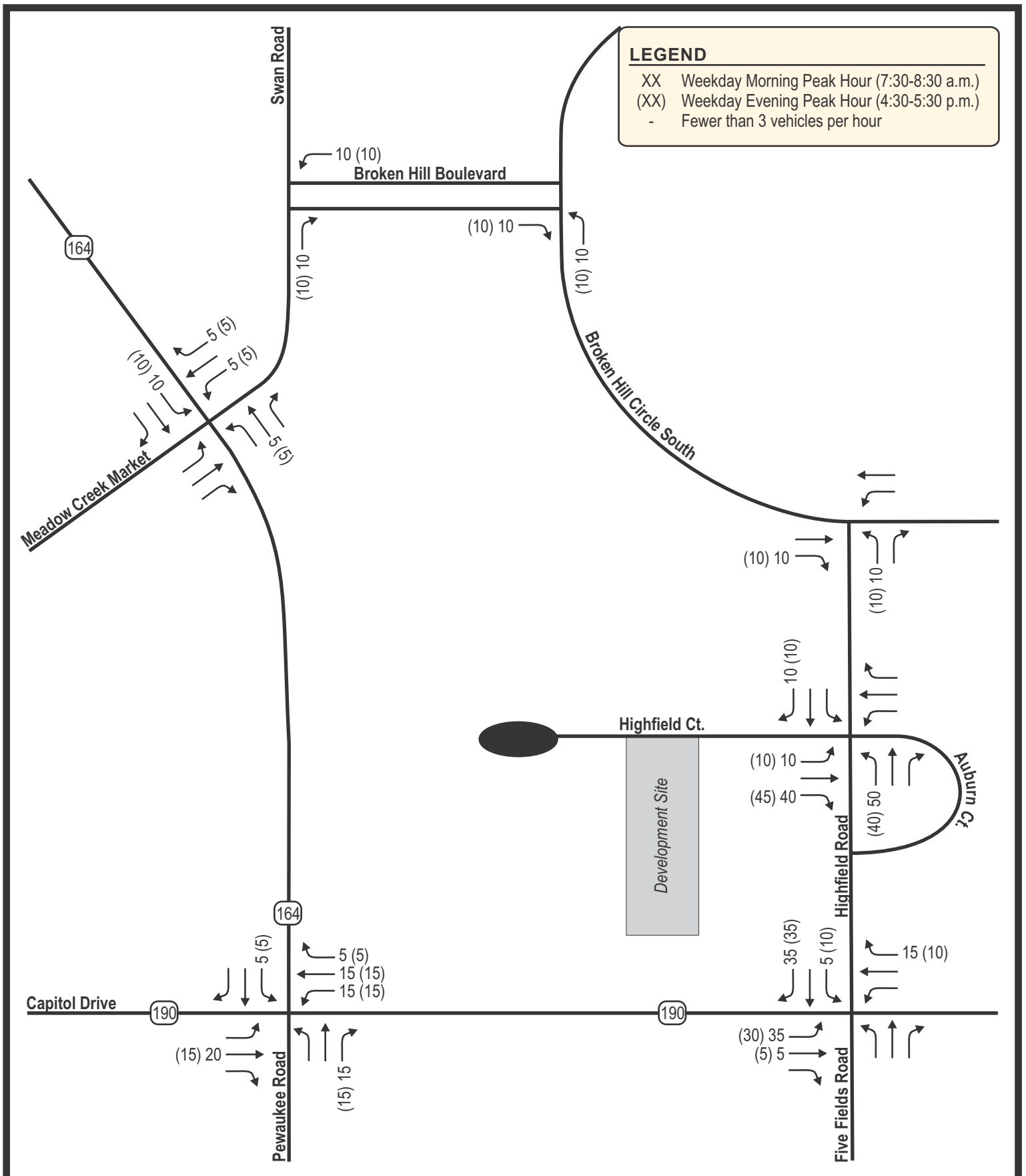
¹ ITE Trip Rates (X.XX) and/or Fitted Curve Equations (FCE) are from the ITE Trip Generation Manual, 11th Edition.

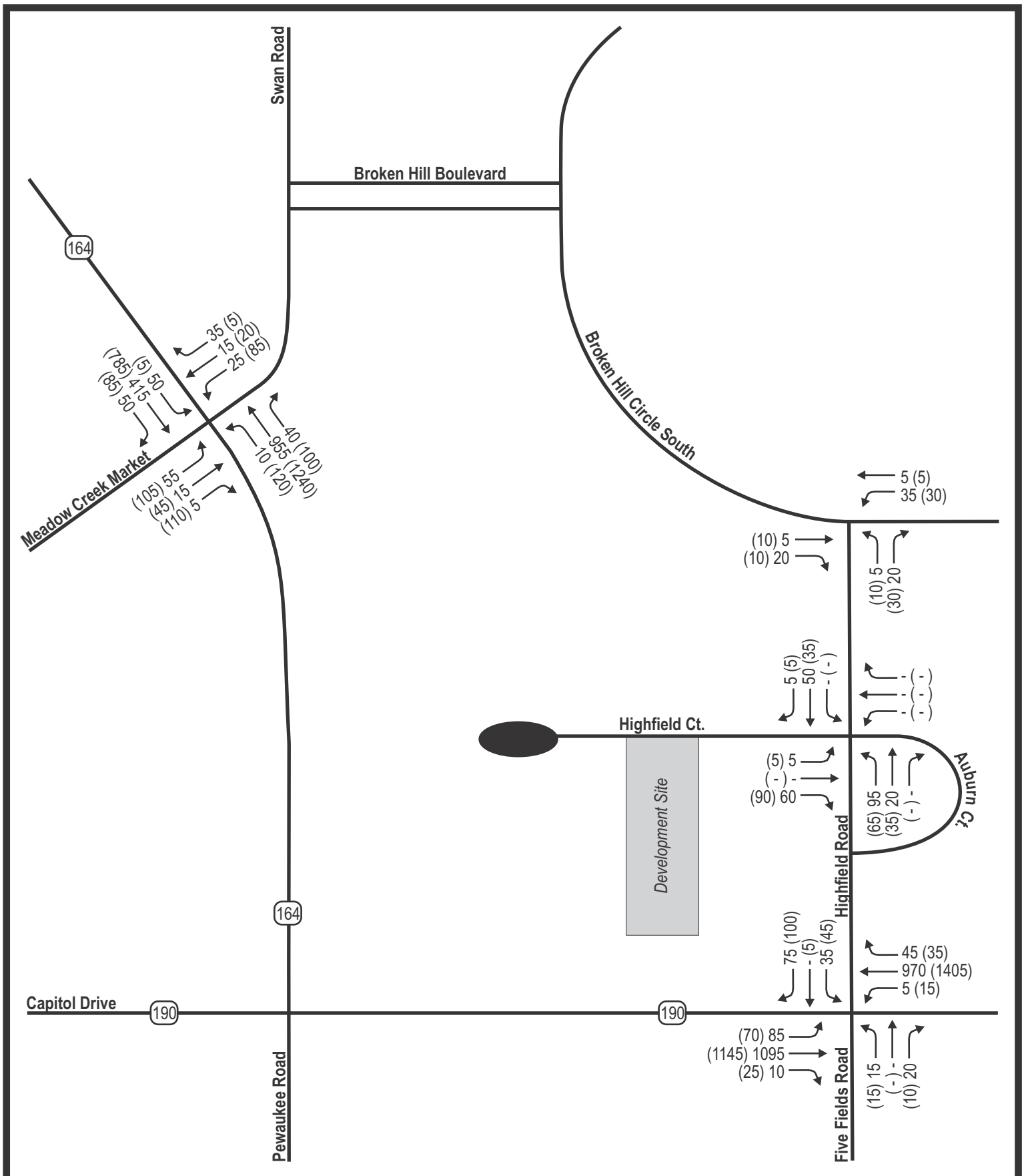
TRIP DISTRIBUTION (New Trips)

W. on Capitol Drive	30%	20	15	15	15
E. on Capitol Drive	25%	15	10	10	15
N. on STH 164	20%	10	10	10	10
S. on STH 164	25%	15	15	15	15
	100%	60	50	50	55







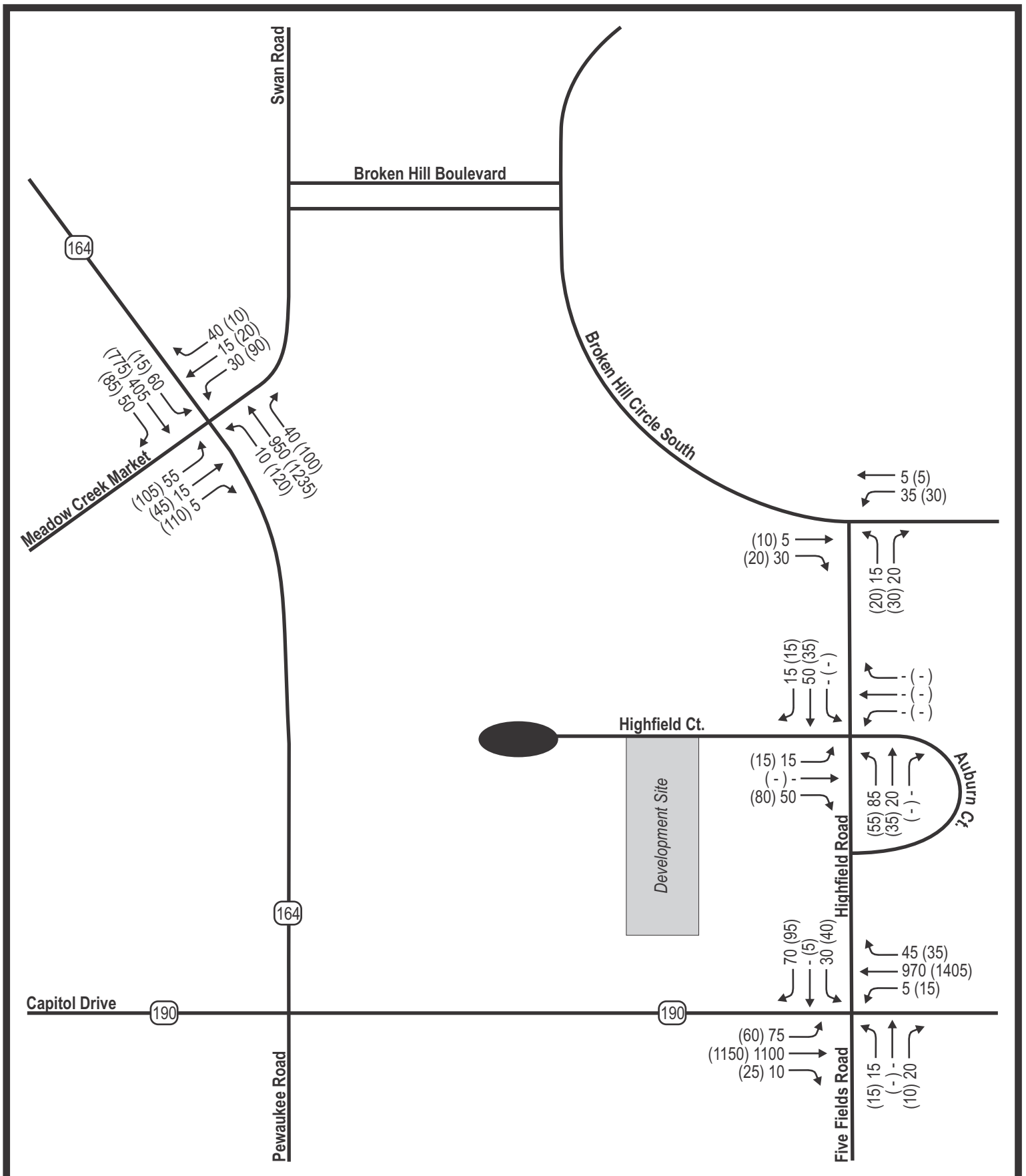


3072: 05-02-2023



NOT TO SCALE

EXHIBIT 8 **SCENARIO A BUILD PEAK HOUR TRAFFIC VOLUMES** **WITHOUT CUT-THROUGH TRAFFIC**



**Year 2023 Existing Traffic Peak Hour Operating Conditions
With Existing Geometrics and Traffic Control**

Intersection	Peak Hour	Metric	Level of Service (LOS) per Movement by Approach												I/S LOS & Delay
			Eastbound			Westbound			Northbound			Southbound			
			↗	→	↘	↙	←	↖	↖	↑	↗	↘	↓	↙	
Pewaukee Road & Swan Road Traffic Signal Control	AM	Lanes->	1	1>		1	1>		1	2	1	1	2	1	
		LOS	D	D		D	D		A	A	A	A	A	A	A
		Delay	39	35		36	36		4	4	3	7	6	5	8
		Queue	65'	30'		35'	60'		5'	140'	10'	40'	90'	20'	
	PM	LOS	C	C		D	C		A	A	A	B	B	B	B
		Delay	30	31		37	27		8	8	5	11	14	11	13
		Queue	100'	135'		85'	30'		35'	205'	25'	5'	180'	35'	
Highfield Road/Five Fields Road & Capitol Drive Stop Sign Control (NB/SB)	AM	Lanes->	1	2	1	1	2	1	<1>			1	1>		
		LOS	B	*	*	B	*	*	C			D	B	A	
		Delay	11	*	*	11	*	*	23			28	12	1	
		Queue	5'	*	*	0'	*	*	15'			15'	5'		
	PM	LOS	B	*	*	B	*	*	D			F	C	A	
		Delay	13	*	*	11	*	*	28			53	17	1	
		Queue	5'	*	*	5'	*	*	15'			35'	15'		
Highfield Road & Highfield Ct./Auburn Ct. Stop Sign Control (EB/WB)	AM	Lanes->	<1>			<1>			<1>			<1>			
		LOS	A			A			A			A			A
		Delay	9			9			7			7			3
		Queue	5'			0'			5'			0'			
	PM	LOS	A			A			A			A			A
		Delay	8			9			7			7			3
		Queue	5'			0'			0'			0'			
Highfield Road & Broken Hills Circle South Stop Sign Control (NB)	AM	Lanes->	-	1>		<1>		-	<1>			-			
		LOS	-	*		A	-		A			-			A
		Delay	-	*		7	-		8			-			5
		Queue	-	*		5'	-		5'			-			
	PM	LOS	-	*		A	-		A			-			A
		Delay	-	*		7	-		8			-			6
		Queue	-	*		5'	-		5'			-			

(-) indicates a movement that is prohibited or does not exist; (*) indicates a freeflow movement; (< or >) indicates a shared turn lane movement.
Delay is reported in seconds. Queue is the maximum of the 50th & 95th percentile queue, measured in feet.
U-Turns, if any, are included in the left-turn volume.



3072: 05-02-2023



**EXHIBIT 10
EXISTING TRAFFIC LOS & QUEUES**

**Year 2023 Build Scenario A Traffic Peak Hour Operating Conditions
With Existing Geometrics and Traffic Control**

Intersection	Peak Hour	Metric	Level of Service (LOS) per Movement by Approach												I/S LOS & Delay
			Eastbound			Westbound			Northbound			Southbound			
			↗	→	↘	↙	←	↖	↖	↑	↗	↘	↓	↙	
Pewaukee Road & Swan Road Traffic Signal Control	AM	Lanes->	1	1>		1	1>		1	2	1	1	2	1	
		LOS	D	D		D	D		A	A	A	A	A	A	A
		Delay	39	35		36	36		4	4	3	7	6	5	8
	PM	Queue	65'	30'		35'	60'		5'	145'	10'	40'	95'	20'	
		LOS	C	C		D	C		A	A	A	B	B	B	B
		Delay	30	31		37	27		8	8	5	12	14	11	13
	Queue	100'	135'		85'	30'		35'	210'	25'	5'	185'	35'		
Highfield Road/Five Fields Road & Capitol Drive Stop Sign Control (NB/SB)	AM	Lanes->	1	2	1	1	2	1	<1>			1	1>		
		LOS	B	*	*	B	*	*	D			E	B	A	
		Delay	11	*	*	11	*	*	28			35	13	1	
	PM	Queue	15'	*	*	0'	*	*	20'			25'	15'		
		LOS	B	*	*	B	*	*	E			F	C	A	
		Delay	14	*	*	11	*	*	36			81	19	2	
	Queue	15'	*	*	5'	*	*	20'			65'	30'			
Highfield Road & Highfield Ct./Auburn Ct. Stop Sign Control (EB/WB)	AM	Lanes->	<1>			<1>			<1>			<1>			
		LOS	A			B			A			A			A
		Delay	9			10			7			7			5
	PM	Queue	10'			0'			5'			0'			
		LOS	A			B			A			A			A
		Delay	9			10			7			7			5
	Queue	10'			0'			5'			0'				
Highfield Road & Broken Hills Circle South Stop Sign Control (NB)	AM	Lanes->	-	1>		<1		-	<1>			-			
		LOS	-	*		A	-		A			-			A
		Delay	-	*		7	-		8			-			5
	PM	Queue	-	*		5'	-		5'			-			
		LOS	-	*		A	-		A			-			A
		Delay	-	*		7	-		8			-			6
	Queue	-	*		5'	-		5'			-				

(-) indicates a movement that is prohibited or does not exist; (*) indicates a freeflow movement; (< or >) indicates a shared turn lane movement.
Delay is reported in seconds. Queue is the maximum of the 50th & 95th percentile queue, measured in feet.
U-Turns, if any, are included in the left-turn volume.



3072: 05-02-2023



**EXHIBIT 11
SCENARIO A BUILD TRAFFIC LOS & QUEUES
WITHOUT CUT-THROUGH TRAFFIC**

**Year 2023 Build Scenario B Traffic Peak Hour Operating Conditions
With Existing Geometrics and Traffic Control**

Intersection	Peak Hour	Metric	Level of Service (LOS) per Movement by Approach												I/S LOS & Delay
			Eastbound			Westbound			Northbound			Southbound			
			↗	→	↘	↙	←	↖	↖	↑	↗	↘	↓	↙	
Pewaukee Road & Swan Road Traffic Signal Control	AM	Lanes->	1	1>		1	1>		1	2	1	1	2	1	
		LOS	D	D		D	D		A	A	A	A	A	A	A
		Delay	39	35		36	36		4	5	3	8	6	5	8
	PM	Queue	65'	30'		45'	65'		5'	145'	10'	40'	90'	20'	
		LOS	C	C		D	C		A	A	A	B	B	B	B
		Delay	31	31		37	28		8	8	5	12	14	11	13
	Queue	100'	135'		90'	35'		35'	210'	25'	15'	180'	35'		
Highfield Road/Five Fields Road & Capitol Drive Stop Sign Control (NB/SB)	AM	Lanes->	1	2	1	1	2	1	<1>			1	1>		
		LOS	B	*	*	B	*	*	D			D	B	A	
		Delay	11	*	*	11	*	*	27			33	13	1	
	PM	Queue	15'	*	*	0'	*	*	20'			20'	15'		
		LOS	B	*	*	B	*	*	D			F	C	A	
		Delay	14	*	*	11	*	*	34			70	18	2	
	Queue	15'	*	*	5'	*	*	15'			55'	30'			
Highfield Road & Highfield Ct./Auburn Ct. Stop Sign Control (EB/WB)	AM	Lanes->	<1>			<1>			<1>			<1>			
		LOS	A			B			A			A			A
		Delay	9			10			7			7			5
	PM	Queue	10'			0'			5'			0'			
		LOS	A			B			A			A			A
		Delay	9			10			7			7			5
	Queue	10'			0'			5'			0'				
Highfield Road & Broken Hills Circle South Stop Sign Control (NB)	AM	Lanes->	-	1>		<1>		-	<1>			-			
		LOS	-	*		A	-		A			-			A
		Delay	-	*		7	-		9			-			5
	PM	Queue	-	*		5'	-		5'			-			
		LOS	-	*		A	-		A			-			A
		Delay	-	*		7	-		8			-			5
	Queue	-	*		5'	-		5'			-				

(-) indicates a movement that is prohibited or does not exist; (*) indicates a freeflow movement; (< or >) indicates a shared turn lane movement.
Delay is reported in seconds. Queue is the maximum of the 50th & 95th percentile queue, measured in feet.
U-Turns, if any, are included in the left-turn volume.



3072: 05-02-2023



**EXHIBIT 12
SCENARIO B BUILD TRAFFIC LOS & QUEUES
WITH CUT-THROUGH TRAFFIC**

APPENDIX A

TRAFFIC COUNTS

Intersection Traffic Volume Report

Count Basics		Version 2022.11.2	Page 1 of 13
Start Date:	Tuesday, April 11, 2023	Weekday	Schools in Session
Total Number of Hours Counted:	5.5	Non-Holiday	No Special Events

Base Information, Observed (5.5) Hour and Estimated (24) Hour Volume Summaries

Major St: STH 164
 Minor St: SWAN RD
 Intersection of: STH 164 & SWAN RD

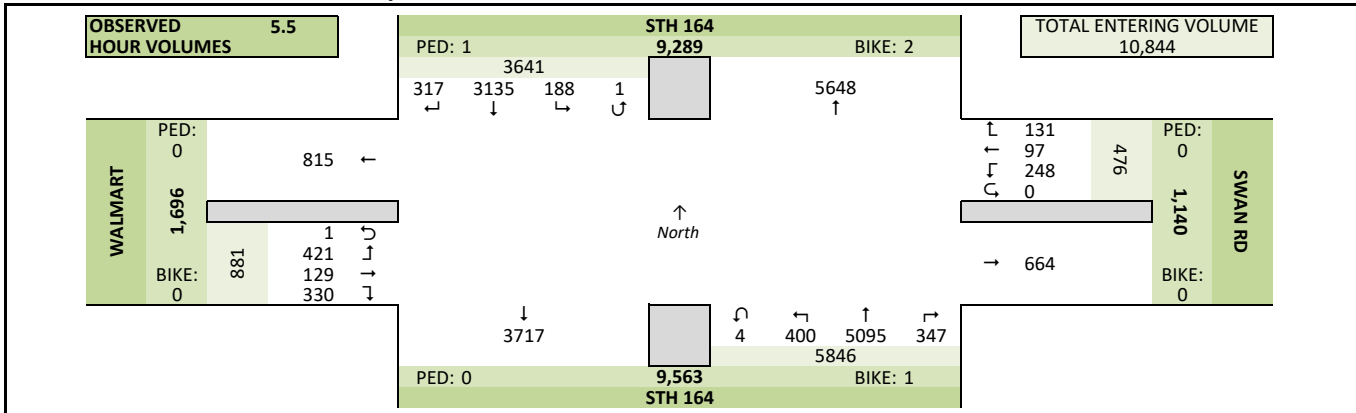
Site Information

Municipality	City of Pewaukee
County	67 - Waukesha
Traffic Control	Traffic Signal
Roadway Names	North Direction
North Leg	STH 164
East Leg	SWAN RD
South Leg	STH 164
West Leg	WALMART
Special Considerations	
Schools	In Session
Holidays	None
Special Events	None
Special Pedestrians Observed	
Pre-school children	None
Elementary school age children	None
Visually impaired (white cane/helper dog)	None
Elderly/disabled (except wheelchairs)	None
Wheelchairs/electric scooters	None
Other (describe)	None

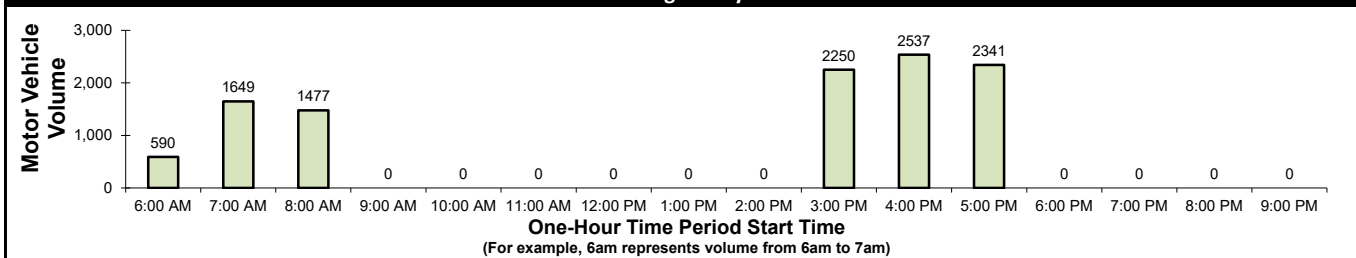
Count Information

Hrs Counted:	06:30 AM-09:00 AM and 03:00 PM-06:00 PM
1st Day of Count	Tuesday, April 11, 2023
AM Peak Period	Tuesday, April 11, 2023
Midday Peak Period	Tuesday, April 11, 2023
PM Peak Period	Tuesday, April 11, 2023
Calculated Peak Hours	
AM	7:15-8:15am
MD	
PM	4:15-5:15pm
Peak Hours Selected for Analysis	
AM	7:30-8:30am
MD	
PM	4:30-5:30pm
Daily/Seasonal Adjustment Group	(2) Urban Arterials & Collectors
Count Expansion Group	(2) Urban Arterials & Collectors
Daily/Seasonal Adjustment Factor	0.956
Count Expansion Factor	2.414
Company Name	TADI, Inc.
Manual Adj.	1.000
Observers	AM Peak Period: LuAnn Gaertner Midday Peak Period: None PM Peak Period: Lori Atwell
Comments	2021 DOT Daily & Seasonal Factors

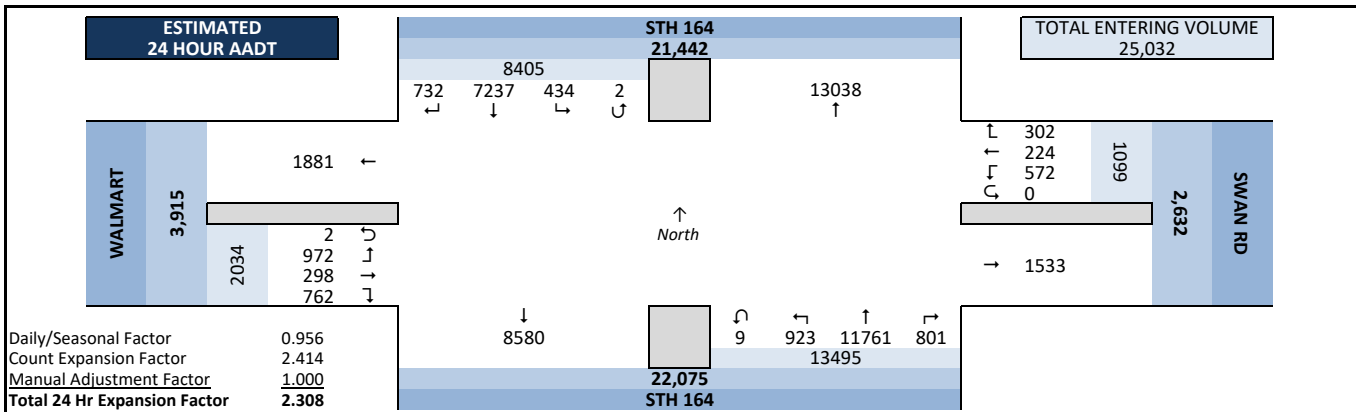
Observed 5.5 Hour Volume Summary



Total Entering Hourly Volume



Estimated 24 Hour AADT



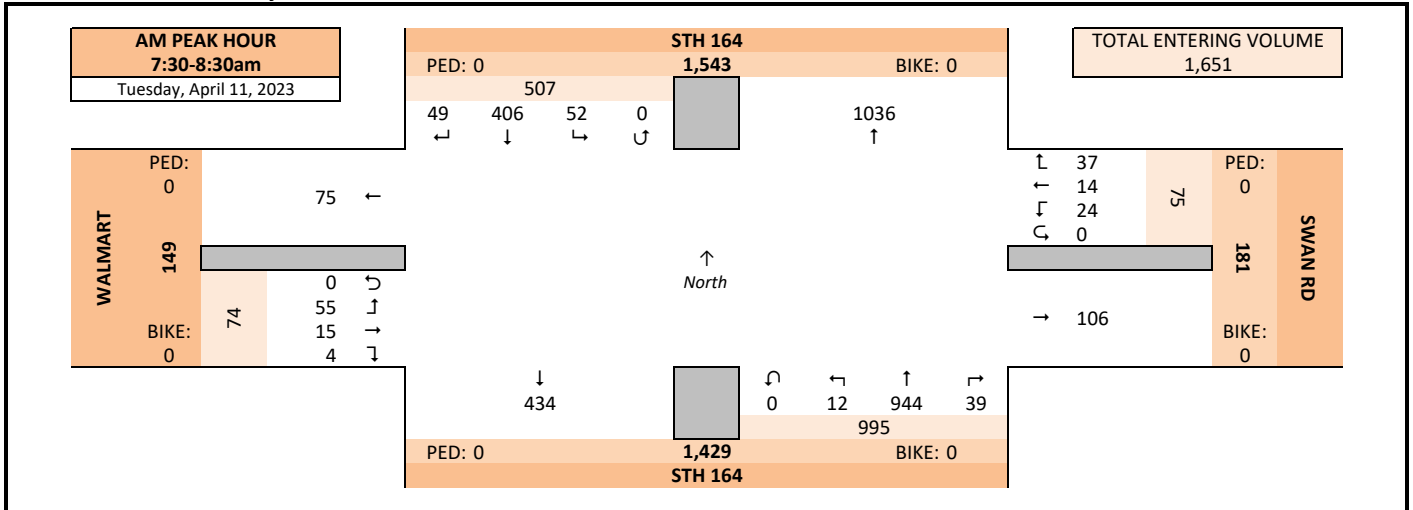
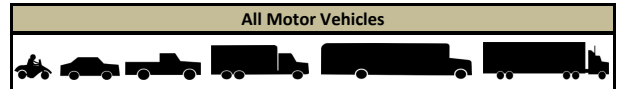
Intersection Traffic Volume Report

Count Basics		Page 2 of 13	
Start Date:	Tuesday, April 11, 2023	Weekday	Schools in Session
Total Number of Hours Counted: 5.5		Non-Holiday	No Special Events

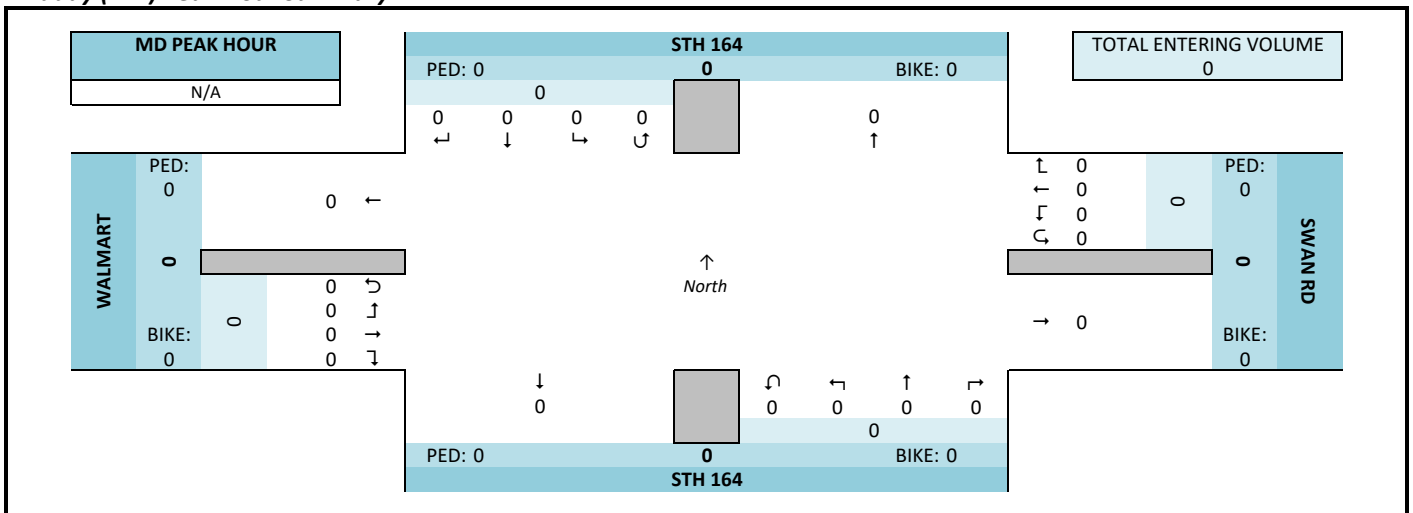
Peak Hour Volume Graphical Summary

STH 164 & SWAN RD

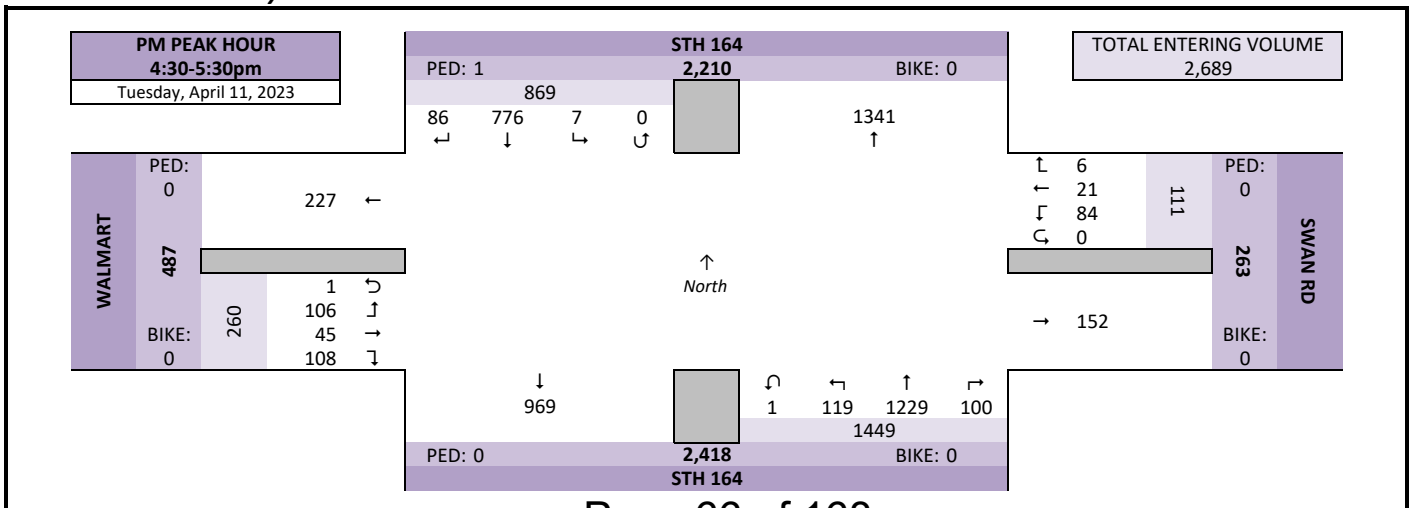
AM Peak Hour Summary



Midday (MD) Peak Hour Summary



PM Peak Hour Summary

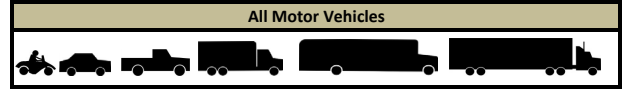


Intersection Traffic Volume Report

Count Basics			Page 3 of 13	
Start Date:	Tuesday, April 11, 2023	Weekday	Schools in Session	
Total Number of Hours Counted:	5.5	Non-Holiday	No Special Events	

Peak Hour Volume Summary

STH 164 & SWAN RD








Peak Hour Volumes, Truck Percentages, and PHFs

Tuesday, April 11, 2023		↓					↶					↑					↷					
		From North					From East					From South					From West					
AM Peak Hour	AM Peak Hour	STH 164					SWAN RD					STH 164					WALMART					
	Start Time	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Totals
	7:30 AM	7	90	10	0	107	13	3	5	0	21	7	284	1	0	292	2	2	15	0	19	439
	7:45 AM	20	115	15	0	150	9	6	3	0	18	8	258	2	0	268	1	5	14	0	20	456
	8:00 AM	13	102	11	0	126	6	3	5	0	14	13	202	4	0	219	0	2	14	0	16	375
	8:15 AM	9	99	16	0	124	9	2	11	0	22	11	200	5	0	216	1	6	12	0	19	381
	Peak Hour Volume	49	406	52	0	507	37	14	24	0	75	39	944	12	0	995	4	15	55	0	74	1651
	Rounded Hourly Volume	50	405	50	0	505	35	15	25	0	75	40	945	10	0	995	5	15	55	0	75	1650
	% Single Unit Trucks	16.3	8.1	3.8	0.0	8.5	5.4	14.3	0.0	0.0	5.3	5.1	4.2	25.0	0.0	4.5	0.0	13.3	5.5	0.0	6.8	5.9
	% Heavy Trucks	2.0	2.2	3.8	0.0	2.4	8.1	0.0	0.0	0.0	4.0	0.0	1.3	0.0	0.0	1.2	25.0	0.0	1.8	0.0	2.7	1.8
% Trucks (Total)	18.4	10.3	7.7	0.0	10.8	13.5	14.3	0.0	0.0	9.3	5.1	5.5	25.0	0.0	5.7	25.0	13.3	7.3	0.0	9.5	7.6	
Peak Hour Factor (PHF)	0.61	0.88	0.81	0.00	0.84	0.71	0.58	0.55	0.00	0.85	0.75	0.83	0.60	0.00	0.85	0.50	0.62	0.92	0.00	0.92	0.91	

N/A		↓ From North					← From East					↑ From South					→ From West					Totals
MD Peak Hour		STH 164					SWAN RD					STH 164					WALMART					
Start Time	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total		
12:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
12:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
12:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
12:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
Peak Hour Volume	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
Rounded Hourly Volume	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
% Single Unit Trucks	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		
% Heavy Trucks	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		
% Trucks (Total)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		
Peak Hour Factor (PHF)	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00		

Tuesday, April 11, 2023		↓ From North					← From East					↑ From South					→ From West					Totals
PM Peak Hour	PM Peak Hour	STH 164					SWAN RD					STH 164					WALMART					
	Start Time	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	
	4:30 PM	20	218	1	0	239	3	2	26	0	31	28	321	36	0	385	19	6	22	0	47	702
	4:45 PM	19	182	2	0	203	0	9	17	0	26	22	322	23	0	367	33	12	19	1	65	661
	5:00 PM	25	201	2	0	228	2	6	19	0	27	29	286	35	0	350	28	19	43	0	90	695
	5:15 PM	22	175	2	0	199	1	4	22	0	27	21	300	25	1	347	28	8	22	0	58	631
	Peak Hour Volume	86	776	7	0	869	6	21	84	0	111	100	1229	119	1	1449	108	45	106	1	260	2689
	Rounded Hourly Volume	85	775	5	0	865	5	20	85	0	110	100	1230	120	0	1450	110	45	105	0	260	2685
	% Single Unit Trucks	1.2	2.7	0.0	0.0	2.5	0.0	0.0	4.8	0.0	3.6	0.0	1.5	0.0	0.0	1.2	0.0	0.0	0.0	0.0	0.0	1.6
	% Heavy Trucks	0.0	0.8	0.0	0.0	0.7	0.0	0.0	0.0	0.0	0.0	0.0	0.2	0.0	0.0	0.1	0.0	0.0	0.0	0.0	0.0	0.3
	% Trucks (Total)	1.2	3.5	0.0	0.0	3.2	0.0	0.0	4.8	0.0	3.6	0.0	1.6	0.0	0.0	1.4	0.0	0.0	0.0	0.0	0.0	1.9
Peak Hour Factor (PHF)	0.86	0.89	0.87	0.00	0.91	0.50	0.58	0.81	0.00	0.90	0.86	0.95	0.83	0.25	0.94	0.82	0.59	0.62	0.25	0.72	0.96	

Peak Hour Pedestrian and Bicyclist Volumes

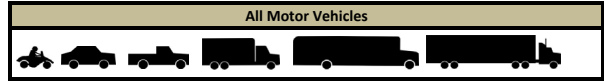
Pedestrians and Bicyclists		Crossing 			Crossing 			Crossing 			Crossing 			Total Ped & Bike Volume
		North Approach			East Approach			South Approach			West Approach			
15-Minute Start Time		STH 164			SWAN RD			STH 164			WALMART			
		Pedestrian	Bicyclist	Total	Pedestrian	Bicyclist	Total	Pedestrian	Bicyclist	Total	Pedestrian	Bicyclist	Total	
AM	7:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
	7:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
	8:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
	8:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
	Total	0	0	0	0	0	0	0	0	0	0	0	0	0
MD	12:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
	12:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
	12:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
	12:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
	Total	0	0	0	0	0	0	0	0	0	0	0	0	0
PM	4:30 PM	1	0	1	0	0	0	0	0	0	0	0	0	1
	4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
	5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
	5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
	Total	1	0	1	0	0	0	0	0	0	0	0	0	1

Intersection Traffic Volume Report

Count Basics			Page 5 of 13
Start Date:	Tuesday, April 11, 2023	Weekday	Schools in Session
Total Number of Hours Counted:	5.5	Non-Holiday	No Special Events

15-Minute Motor Vehicle Data

STH 164 & SWAN RD



15-Minute Motor Vehicle Data

15-Minute Time Period Start Time	From North					From East					From South					From West					15-Min Totals	Hourly Sum	PHF	
	STH 164					SWAN RD					STH 164					WALMART								
	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total				
AM Peak Period	6:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	6:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	6:30 AM	6	63	15	0	84	7	1	2	0	10	9	158	1	0	168	1	2	6	0	9	271	1344	0.84
	6:45 AM	9	82	14	0	105	8	3	2	0	13	11	172	0	0	183	2	3	13	0	18	319	1512	0.86
	7:00 AM	7	71	13	0	91	21	1	5	0	27	10	198	4	0	212	1	1	20	0	22	352	1649	0.90
	7:15 AM	14	103	13	0	130	11	2	7	0	20	8	229	4	0	241	1	2	8	0	11	402	1672	0.92
	7:30 AM	7	90	10	0	107	13	3	5	0	21	7	284	1	0	292	2	2	15	0	19	439	1651	0.91
	7:45 AM	20	115	15	0	150	9	6	3	0	18	8	258	2	0	268	1	5	14	0	20	456	1562	0.86
	8:00 AM	13	102	11	0	126	6	3	5	0	14	13	202	4	0	219	0	2	14	0	16	375	1477	0.97
	8:15 AM	9	99	16	0	124	9	2	11	0	22	11	200	5	0	216	1	6	12	0	19	381		
	8:30 AM	14	98	34	1	147	5	7	6	0	18	7	160	2	0	169	1	1	14	0	16	350		
	8:45 AM	9	104	20	0	133	16	3	14	0	33	25	165	2	0	192	0	3	10	0	13	371		
9:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
9:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
9:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
9:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
Midday Peak Period	10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	10:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	10:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	10:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	11:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	11:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	11:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	11:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	12:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	12:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	12:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	12:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
1:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
1:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
1:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
1:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
PM Peak Period	2:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	2:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	2:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	2:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	3:00 PM	14	172	2	0	188	4	4	14	0	22	21	210	29	1	261	22	8	33	0	63	534	2250	0.95
	3:15 PM	19	167	2	0	188	3	6	17	0	26	9	233	28	0	270	30	8	22	0	60	544	2246	0.95
	3:30 PM	17	188	3	0	208	1	3	7	0	11	22	259	41	1	323	16	6	16	0	38	580	2346	0.91
	3:45 PM	18	197	5	0	220	3	4	9	0	16	24	241	32	0	297	30	3	26	0	59	592	2468	0.88
	4:00 PM	12	153	1	0	166	4	9	14	0	27	11	229	38	1	279	31	5	22	0	58	530	2537	0.90
	4:15 PM	12	208	2	0	222	3	4	19	0	26	14	268	35	0	317	34	13	32	0	79	644	2702	0.96
	4:30 PM	20	218	1	0	239	3	2	26	0	31	28	321	36	0	385	19	6	22	0	47	702	2689	0.96
	4:45 PM	19	182	2	0	203	0	9	17	0	26	22	322	23	0	367	33	12	19	1	65	661	2526	0.91
	5:00 PM	25	201	2	0	228	2	6	19	0	27	29	286	35	0	350	28	19	43	0	90	695	2341	0.84
	5:15 PM	22	175	2	0	199	1	4	22	0	27	21	300	25	1	347	28	8	22	0	58	631		
	5:30 PM	15	173	4	0	192	1	6	8	0	15	23	223	35	0	281	25	9	17	0	51	539		
	5:45 PM	16	174	1	0	191	1	9	16	0	26	14	177	18	0	209	24	5	21	0	50	476		
	6:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	6:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	6:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	6:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	7:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	7:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	7:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	7:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
8:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
8:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
8:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
8:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
9:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
9:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
9:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
9:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
Totals		317	3135	188	1	3641	131	97	248	0	476	347	5095	400	4	5846	330	129	421	1	881	10844		

Peak Hour All Vehicle Volume Summary

	↓					←					↑					→					Total Hourly Volume	PHF
Hourly Time Period	From North					From East					From South					From West						
Start Time	STH 164					SWAN RD					STH 164					WALMART						
	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total		
AM 7:30 AM	49	406	52	0	507	37	14	24	0	75	39	944	12	0	995	4	15	55	0	74	1651	0.91
MD 12:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
PM 4:30 PM	86	776	7	0	869	6	21	84	0	111	106	229	0	0	335	108	45	106	1	260	2689	0.96

Intersection Traffic Volume Report

Count Basics			Page 9 of 13
Start Date:	Tuesday, April 11, 2023	Weekday	Schools in Session
Total Number of Hours Counted:	5.5	Non-Holiday	No Special Events

15-Minute Heavy Vehicle Data

STH 164 & SWAN RD



15-Minute Heavy Vehicle Data

15-Minute Time Period	Start Time	From North STH 164					From East SWAN RD					From South STH 164					From West WALMART					15-Min Totals	Hourly Sum
		Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total		
AM Peak Period	6:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	6:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	6:30 AM	1	1	2	0	4	0	0	0	0	0	1	14	0	0	15	0	0	0	0	0	19	67
	6:45 AM	0	4	0	0	4	0	0	0	0	0	0	6	0	0	6	0	0	1	0	1	11	78
	7:00 AM	0	7	2	0	9	1	0	0	0	1	1	12	0	0	13	0	0	1	0	1	24	95
	7:15 AM	2	3	2	0	7	1	0	0	0	1	0	4	1	0	5	0	0	0	0	0	13	102
	7:30 AM	1	7	3	0	11	1	0	0	0	1	0	17	1	0	18	0	0	0	0	0	30	126
	7:45 AM	4	9	1	0	14	1	0	0	0	1	0	12	0	0	12	0	1	0	0	1	28	121
	8:00 AM	2	11	0	0	13	2	1	0	0	3	1	10	2	0	13	0	0	2	0	2	31	126
	8:15 AM	2	15	0	0	17	1	1	0	0	2	1	13	0	0	14	1	1	2	0	4	37	
	8:30 AM	2	8	3	0	13	0	0	2	0	2	1	9	0	0	10	0	0	0	0	0	25	
	8:45 AM	0	15	2	0	17	1	0	0	0	1	0	14	0	0	14	0	1	0	0	1	33	
	9:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	9:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	9:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	9:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Midday Peak Period	10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	10:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	10:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	10:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	11:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	11:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	11:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	11:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	12:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	12:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	12:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	12:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	1:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	1:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	1:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	1:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
PM Peak Period	2:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	2:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	2:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	2:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	3:00 PM	2	8	0	0	10	0	1	0	0	1	2	10	0	0	12	1	0	0	0	1	24	81
	3:15 PM	0	12	0	0	12	0	0	2	0	2	0	4	0	0	4	0	0	0	0	0	18	68
	3:30 PM	1	6	0	0	7	0	0	0	0	0	0	7	0	0	7	0	0	0	0	0	14	71
	3:45 PM	0	10	1	0	11	0	0	1	0	1	1	10	1	0	12	0	1	0	0	1	25	73
	4:00 PM	0	5	0	0	5	0	0	1	0	1	1	4	0	0	5	0	0	0	0	0	11	58
	4:15 PM	0	9	0	0	9	1	0	3	0	4	0	7	1	0	8	0	0	0	0	0	21	60
	4:30 PM	0	9	0	0	9	0	0	0	0	0	0	7	0	0	7	0	0	0	0	0	16	52
	4:45 PM	0	4	0	0	4	0	0	1	0	1	0	5	0	0	5	0	0	0	0	0	10	50
	5:00 PM	0	9	0	0	9	0	0	0	0	0	0	4	0	0	4	0	0	0	0	0	13	48
	5:15 PM	1	5	0	0	6	0	0	3	0	3	0	4	0	0	4	0	0	0	0	0	13	
	5:30 PM	1	4	0	0	5	0	0	0	0	0	0	7	2	0	9	0	0	0	0	0	14	
	5:45 PM	0	6	0	0	6	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	8	
	6:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	6:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	6:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	6:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	7:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	7:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	7:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	7:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	8:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	8:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	8:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	8:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	9:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	9:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	9:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	9:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Totals		19	167	16	0	202	9	3	13	0	25	9	182	8	0	199	2	4	6	0	12	438	

Peak Hour Heavy Vehicle Volume Summary

Hourly Time Period Start Time	From North					From East					From South					From West					Total Hourly Volume
	STH 164					SWAN RD					STH 164					WALMART					
	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	
AM 7:30 AM	9	42	4	0	55	5	2	0	0	7	2	52	3	0	57	1	2	4	0	7	126
MD 12:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
PM 4:30 PM	1	27	0	0	28	0	0	4	0	4	0	26	0	0	20	0	0	0	0	0	52

Intersection Traffic Volume Report

Count Basics		Version 2022.11.2	Page 1 of 13
Start Date:	Tuesday, April 11, 2023	Weekday	Schools in Session
Total Number of Hours Counted:	5.5	Non-Holiday	No Special Events

Base Information, Observed (5.5) Hour and Estimated (24) Hour Volume Summaries

Major St: BROKEN HILL CIR S

Minor St: HIGHFIELD RD

Intersection of: BROKEN HILL CIR S & HIGHFIELD RD

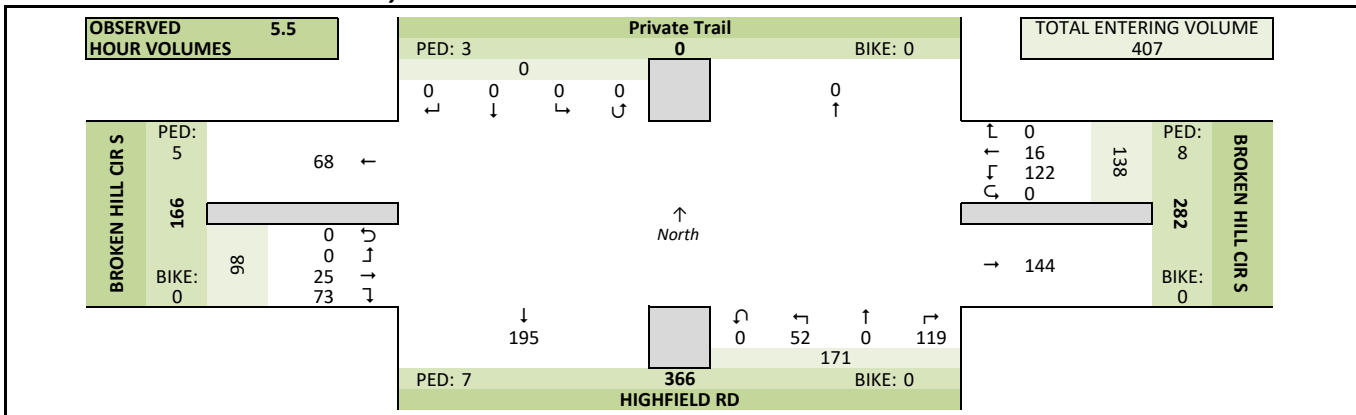
Site Information

Municipality	City of Pewaukee
County	67 - Waukesha
WisDOT Region	SE
Traffic Control	Partial Stop Control
Roadway Names	North Direction
North Leg	Private Trail
East Leg	BROKEN HILL CIR S
South Leg	HIGHFIELD RD
West Leg	BROKEN HILL CIR S
Special Considerations	
Schools	In Session
Holidays	None
Special Events	None
Special Pedestrians Observed	
Pre-school children	None
Elementary school age children	None
Visually impaired (white cane/helper dog)	None
Elderly/disabled (except wheelchairs)	None
Wheelchairs/electric scooters	None
Other (describe)	None

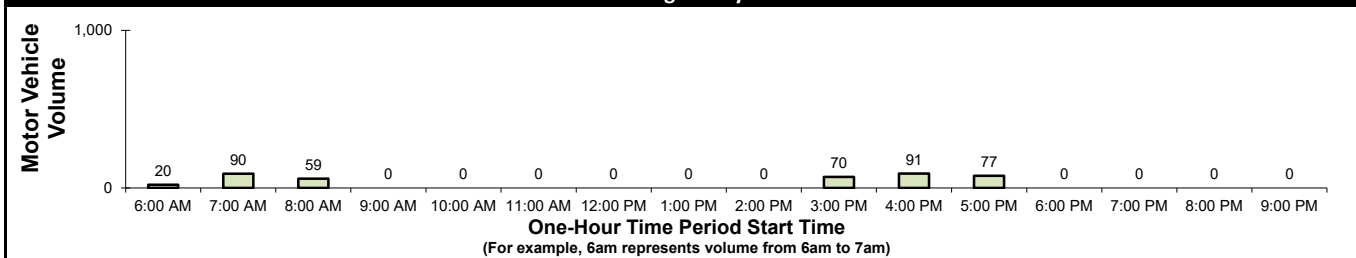
Count Information

Hrs Counted:	06:30 AM-09:00 AM and 03:00 PM-06:00 PM
1st Day of Count	Tuesday, April 11, 2023
AM Peak Period	Wednesday, April 12, 2023
Midday Peak Period	Tuesday, April 11, 2023
PM Peak Period	Tuesday, April 11, 2023
Calculated Peak Hours	
AM	7:30-8:30am
MD	
PM	4:30-5:30pm
Peak Hours Selected for Analysis	
AM	7:30-8:30am
MD	
PM	4:30-5:30pm
Daily/Seasonal Adjustment Group	(2) Urban Arterials & Collectors
Count Expansion Group	(2) Urban Arterials & Collectors
Daily/Seasonal Adjustment Factor	0.955
Count Expansion Factor	2.414
Company Name	TADI, Inc.
Manual Adj.	1.000
Observers	AM Peak Period: Wendy Picard Midday Peak Period: None PM Peak Period: Dani Ruffalo
Comments	2021 DOT Daily & Seasonal Factors

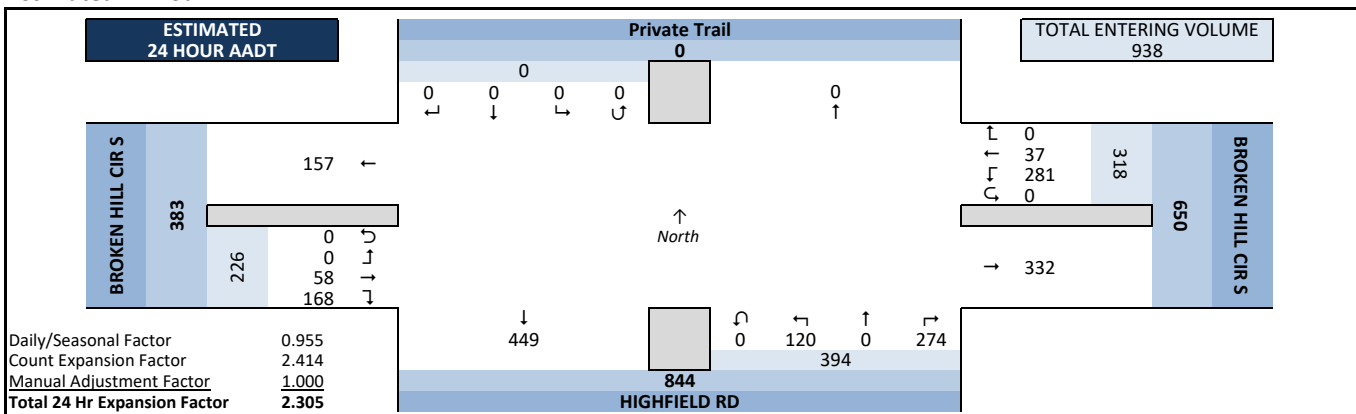
Observed 5.5 Hour Volume Summary



Total Entering Hourly Volume



Estimated 24 Hour AADT



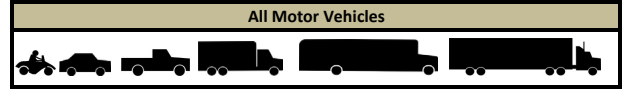
Count Basics			Page 2 of 13
Start Date:	Tuesday, April 11, 2023	Weekday	Schools in Session
Total Number of Hours Counted: 5.5		Non-Holiday	No Special Events

Intersection Traffic Volume Report

Count Basics			Page 3 of 13	
Start Date:	Tuesday, April 11, 2023	Weekday	Schools in Session	
Total Number of Hours Counted:	5.5	Non-Holiday	No Special Events	

Peak Hour Volume Summary

BROKEN HILL CIR S & HIGHFIELD RD








Peak Hour Volumes, Truck Percentages, and PHFs

Wednesday, April 12, 2023		↓					←					↑					→						
		From North					From East					From South					From West						
AM Peak Hour	AM Peak Hour	Private Trail					BROKEN HILL CIR S					HIGHFIELD RD					BROKEN HILL CIR S						
	Start Time	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Totals	
	7:30 AM	0	0	0	0	0	0	2	6	0	8	5	0	3	0	8	4	1	0	0	5	21	
	7:45 AM	0	0	0	0	0	0	1	13	0	14	7	0	1	0	8	7	1	0	0	8	30	
	8:00 AM	0	0	0	0	0	0	1	6	0	7	4	0	0	0	4	6	1	0	0	7	18	
	8:15 AM	0	0	0	0	0	0	2	12	0	14	3	0	3	0	6	3	1	0	0	4	24	
	Peak Hour Volume	0	0	0	0	0	0	6	37	0	43	19	0	7	0	26	20	4	0	0	24	93	
	Rounded Hourly Volume	0	0	0	0	0	0	5	35	0	40	20	0	5	0	25	20	5	0	0	25	90	
	% Single Unit Trucks	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	10.5	0.0	14.3	0.0	11.5	10.0	50.0	0.0	0.0	16.7	7.5	
	% Heavy Trucks	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
% Trucks (Total)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	10.5	0.0	14.3	0.0	11.5	10.0	50.0	0.0	0.0	16.7	7.5		
Peak Hour Factor (PHF)	0.00	0.00	0.00	0.00	0.00	0.00	0.75	0.71	0.00	0.77	0.68	0.00	0.58	0.00	0.81	0.71	1.00	0.00	0.00	0.75	0.77		

N/A		↓ From North					← From East					↑ From South					→ From West					Totals
Midday (MD) Peak Hour	MD Peak Hour	Private Trail					BROKEN HILL CIR S					HIGHFIELD RD					BROKEN HILL CIR S					
	Start Time	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	
	12:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	12:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	12:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	12:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	Peak Hour Volume	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	Rounded Hourly Volume	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	% Single Unit Trucks	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
	% Heavy Trucks	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
% Trucks (Total)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		
Peak Hour Factor (PHF)	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00		

Tuesday, April 11, 2023		↓					←					↑					→					
PM Peak Hour	PM Peak Hour	Private Trail					BROKEN HILL CIR S					HIGHFIELD RD					BROKEN HILL CIR S					
	Start Time	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Totals
	4:30 PM	0	0	0	0	0	0	0	8	0	8	9	0	2	0	11	5	3	0	0	8	27
	4:45 PM	0	0	0	0	0	0	3	11	0	14	6	0	3	0	9	2	2	0	0	4	27
	5:00 PM	0	0	0	0	0	0	0	9	0	9	8	0	3	0	11	3	2	0	0	5	25
	5:15 PM	0	0	0	0	0	0	1	3	0	4	8	0	2	0	10	1	1	0	0	2	16
	Peak Hour Volume	0	0	0	0	0	0	4	31	0	35	31	0	10	0	41	11	8	0	0	19	95
	Rounded Hourly Volume	0	0	0	0	0	0	5	30	0	35	30	0	10	0	40	10	10	0	0	20	95
	% Single Unit Trucks	0.0	0.0	0.0	0.0	0.0	0.0	0.0	9.7	0.0	8.6	3.2	0.0	0.0	0.0	2.4	0.0	0.0	0.0	0.0	0.0	4.2
	% Heavy Trucks	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
% Trucks (Total)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	9.7	0.0	8.6	3.2	0.0	0.0	0.0	2.4	0.0	0.0	0.0	0.0	0.0	4.2	
Peak Hour Factor (PHF)	0.00	0.00	0.00	0.00	0.00	0.00	0.33	0.70	0.00	0.62	0.86	0.00	0.83	0.00	0.93	0.55	0.67	0.00	0.00	0.59	0.88	

Peak Hour Pedestrian and Bicyclist Volumes

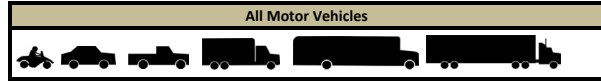
Pedestrians and Bicyclists		Crossing 			Crossing 			Crossing 			Crossing 			Total Ped & Bike Volume
		North Approach			East Approach			South Approach			West Approach			
15-Minute Start Time		Private Trail			BROKEN HILL CIR S			HIGHFIELD RD			BROKEN HILL CIR S			
		Pedestrian	Bicyclist	Total	Pedestrian	Bicyclist	Total	Pedestrian	Bicyclist	Total	Pedestrian	Bicyclist	Total	
AM	7:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
	7:45 AM	0	0	0	0	0	0	1	0	1	0	0	0	1
	8:00 AM	2	0	2	0	0	0	1	0	1	0	0	0	3
	8:15 AM	1	0	1	0	0	0	0	0	0	0	0	0	1
	Total	3	0	3	0	0	0	2	0	2	0	0	0	5
MD	12:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
	12:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
	12:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
	12:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
	Total	0	0	0	0	0	0	0	0	0	0	0	0	0
PM	4:30 PM	0	0	0	4	0	4	0	0	0	0	0	0	4
	4:45 PM	0	0	0	4	0	4	0	0	0	0	0	0	4
	5:00 PM	0	0	0	0	0	0	0	0	0	2	0	2	2
	5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
	Total	0	0	0	8	0	8	0	0	0	2	0	2	10

Intersection Traffic Volume Report

Count Basics			Page 5 of 13
Start Date:	Tuesday, April 11, 2023	Weekday	Schools in Session
Total Number of Hours Counted:	5.5	Non-Holiday	No Special Events

15-Minute Motor Vehicle Data

BROKEN HILL CIR S & HIGHFIELD RD



15-Minute Motor Vehicle Data

15-Minute Time Period Start Time	From North					From East					From South					From West					15-Min Totals	Hourly Sum	PHF	
	Private Trail					BROKEN HILL CIR S					HIGHFIELD RD					BROKEN HILL CIR S								
	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total				
AM Peak Period	6:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	6:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	6:30 AM	0	0	0	0	0	0	0	0	1	0	1	2	0	1	0	3	2	0	0	0	2	59	0.64
	6:45 AM	0	0	0	0	0	0	0	0	5	0	5	3	0	0	0	3	6	0	0	0	6	74	0.80
	7:00 AM	0	0	0	0	0	0	0	0	14	0	14	2	0	0	0	2	7	0	0	0	7	90	0.75
	7:15 AM	0	0	0	0	0	0	0	0	4	0	4	4	0	3	0	7	3	2	0	0	5	85	0.71
	7:30 AM	0	0	0	0	0	0	0	2	6	0	8	5	0	3	0	8	4	1	0	0	5	93	0.78
	7:45 AM	0	0	0	0	0	0	0	1	13	0	14	7	0	1	0	8	7	1	0	0	8	88	0.73
	8:00 AM	0	0	0	0	0	0	0	1	6	0	7	4	0	0	0	4	6	1	0	0	7	59	0.61
	8:15 AM	0	0	0	0	0	0	0	2	12	0	14	3	0	3	0	6	3	1	0	0	4		
	8:30 AM	0	0	0	0	0	0	0	1	7	0	8	4	0	1	0	5	2	1	0	0	3		
	8:45 AM	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1	0	0	0	0	0		
9:00 AM	0	0	Private Trail	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
9:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
9:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
9:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
Midday Peak Period	10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	10:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	10:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	10:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	11:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	11:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	11:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	11:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	12:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	12:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	12:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	12:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
1:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
1:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
1:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
1:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
PM Peak Period	2:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	2:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	2:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	2:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	3:00 PM	0	0	0	0	0	0	0	0	1	0	1	4	0	4	0	8	3	1	0	0	4	70	0.83
	3:15 PM	0	0	0	0	0	0	0	2	1	0	3	9	0	4	0	13	2	2	0	0	4	80	0.87
	3:30 PM	0	0	0	0	0	0	0	1	3	0	4	4	0	5	0	9	3	0	0	0	3	74	0.80
	3:45 PM	0	0	0	0	0	0	0	0	4	0	4	5	0	3	0	8	5	4	0	0	9	85	0.79
	4:00 PM	0	0	0	0	0	0	0	1	6	0	7	7	0	6	0	13	3	0	0	0	3	91	0.84
	4:15 PM	0	0	0	0	0	0	0	0	3	0	3	5	0	2	0	7	2	2	0	0	4	93	0.86
	4:30 PM	0	0	0	0	0	0	0	0	8	0	8	9	0	2	0	11	5	3	0	0	8	95	0.88
	4:45 PM	0	0	0	0	0	0	0	3	11	0	14	6	0	3	0	9	2	2	0	0	4	90	0.83
	5:00 PM	0	0	0	0	0	0	0	0	9	0	9	8	0	3	0	11	3	2	0	0	5	77	0.77
	5:15 PM	0	0	0	0	0	0	0	1	3	0	4	8	0	2	0	10	1	1	0	0	2		
	5:30 PM	0	0	0	0	0	0	0	1	2	0	3	14	0	2	0	16	2	1	0	0	3		
	5:45 PM	0	0	0	0	0	0	0	0	3	0	3	5	0	4	0	9	2	0	0	0	2		
	6:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	6:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	6:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	6:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	7:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	7:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	7:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
	7:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
8:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
8:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
8:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
8:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
9:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
9:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
9:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
9:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0			
Totals	0	0	0	0	0	0	0	16	122	0	138	119	0	52	0	171	73	25	0	0	98	407		

Peak Hour All Vehicle Volume Summary

	↓					←					↑					→						
Hourly	From North					From East					From South					From West					Total	
Time Period	Private Trail					BROKEN HILL CIR S					HIGHFIELD RD					BROKEN HILL CIR S					Hourly	
Start Time	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Volume	
AM 7:30 AM	0	0	0	0	0	0	0	6	37	0	43	19	0	7	0	26	20	4	0	0	24	93
MD 12:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
PM 4:30 PM	0	0	0	0	0	0	0	4	31	0	35	32	0	20	0	11	8	0	0	0	19	95

Intersection Traffic Volume Report

Count Basics			Page 9 of 13
Start Date:	Tuesday, April 11, 2023	Weekday	Schools in Session
Total Number of Hours Counted:	5.5	Non-Holiday	No Special Events

15-Minute Heavy Vehicle Data

BROKEN HILL CIR S & HIGHFIELD RD



15-Minute Heavy Vehicle Data

15-Minute Time Period Start Time	From North					From East					From South					From West					15-Min Totals	Hourly Sum	
	Private Trail					BROKEN HILL CIR S					HIGHFIELD RD					BROKEN HILL CIR S							
	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total			
AM Peak Period	6:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	6:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	6:30 AM	0	0	0	0	0	0	0	0	0	0	2	0	0	0	2	1	0	0	0	1	3	5
	6:45 AM	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1	1	0	0	0	1	2	3
	7:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	7:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6
	7:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	1	7
	7:45 AM	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1	1	0	0	0	1	2	7
	8:00 AM	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1	1	1	0	0	2	3	5
	8:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	1	
	8:30 AM	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1	0	0	0	0	0	1	
	8:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	9:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	9:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	9:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	9:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Midday Peak Period	10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	10:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	10:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	10:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	11:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	11:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	11:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	11:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	12:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	12:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	12:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	12:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	1:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	1:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	1:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	1:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
PM Peak Period	2:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	2:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	2:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	2:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	3:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2
	3:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3
	3:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3
	3:45 PM	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1	1	0	0	0	1	2	3
	4:00 PM	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1	0	0	0	0	0	1	3
	4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4
	4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4
	4:45 PM	0	0	0	0	0	0	0	2	0	2	0	0	0	0	0	0	0	0	0	0	2	4
	5:00 PM	0	0	0	0	0	0	0	1	0	1	1	0	0	0	1	0	0	0	0	0	2	3
	5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	0	0	0	0	0	1	
	6:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	6:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	6:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	6:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	7:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	7:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	7:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	7:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
8:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
8:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
8:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
8:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
9:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
9:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
9:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
9:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
Totals	0	0	0	0	0	0	0	3	0	3	9	0	2	0	11	5	2	0	0	7	21		

Peak Hour Heavy Vehicle Volume Summary

Hourly Time Period Start Time	From North					From East					From South					From West					Total Hourly Volume
	Private Trail					BROKEN HILL CIR S					HIGHFIELD RD					BROKEN HILL CIR S					
	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	
AM 7:30 AM	0	0	0	0	0	0	0	0	0	0	2	0	1	0	3	2	2	0	0	4	
MD 12:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
PM 4:30 PM	0	0	0	0	0	0	0	3	0	3	1	0	0	0	1	0	0	0	0	0	

Intersection Traffic Volume Report

Count Basics		Version 2022.11.2	Page 1 of 13
Start Date:	Tuesday, April 11, 2023	Weekday	Schools in Session
Total Number of Hours Counted:	5.5	Non-Holiday	No Special Events

Base Information, Observed (5.5) Hour and Estimated (24) Hour Volume Summaries

Major St: HIGHFIELD RD
 Minor St: HIGHFIELD COURT
 Intersection of: HIGHFIELD RD & HIGHFIELD COURT

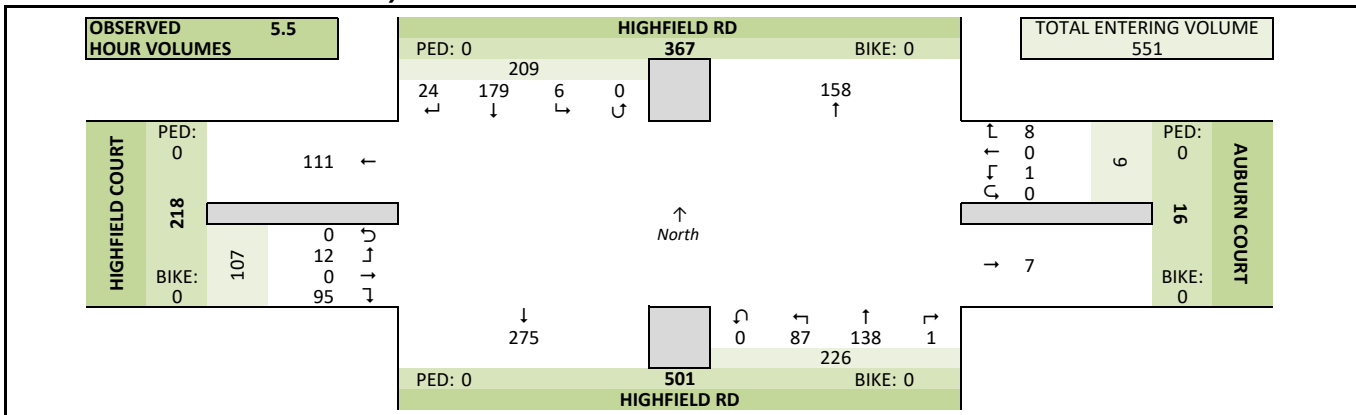
Site Information

Municipality	City of Pewaukee
County	67 - Waukesha
WisDOT Region	SE
Traffic Control	Partial Stop Control
Roadway Names	North Direction
North Leg	HIGHFIELD RD
East Leg	AUBURN COURT
South Leg	HIGHFIELD RD
West Leg	HIGHFIELD COURT
Special Considerations	
Schools	In Session
Holidays	None
Special Events	None
Special Pedestrians Observed	
Pre-school children	None
Elementary school age children	None
Visually impaired (white cane/helper dog)	None
Elderly/disabled (except wheelchairs)	None
Wheelchairs/electric scooters	None
Other (describe)	None

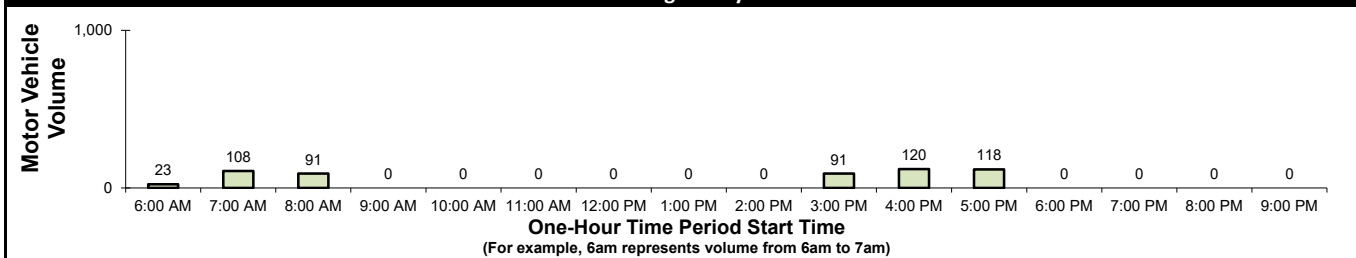
Count Information

Hrs Counted:	06:30 AM-09:00 AM and 03:00 PM-06:00 PM
1st Day of Count	Tuesday, April 11, 2023
AM Peak Period	Wednesday, April 12, 2023
Midday Peak Period	Wednesday, April 12, 2023
PM Peak Period	Tuesday, April 11, 2023
Calculated Peak Hours	
AM	7:45-8:45am
MD	
PM	4:45-5:45pm
Peak Hours Selected for Analysis	
AM	7:30-8:30am
MD	
PM	4:30-5:30pm
Daily/Seasonal Adjustment Group	(2) Urban Arterials & Collectors
Count Expansion Group	(2) Urban Arterials & Collectors
Daily/Seasonal Adjustment Factor	0.955
Count Expansion Factor	2.414
Company Name	TADI, Inc.
Manual Adj.	1.000
Observers	AM Peak Period: LuAnn Gaertner Midday Peak Period: None PM Peak Period: Jane Fait
Comments	2021 DOT Daily & Seasonal Factors

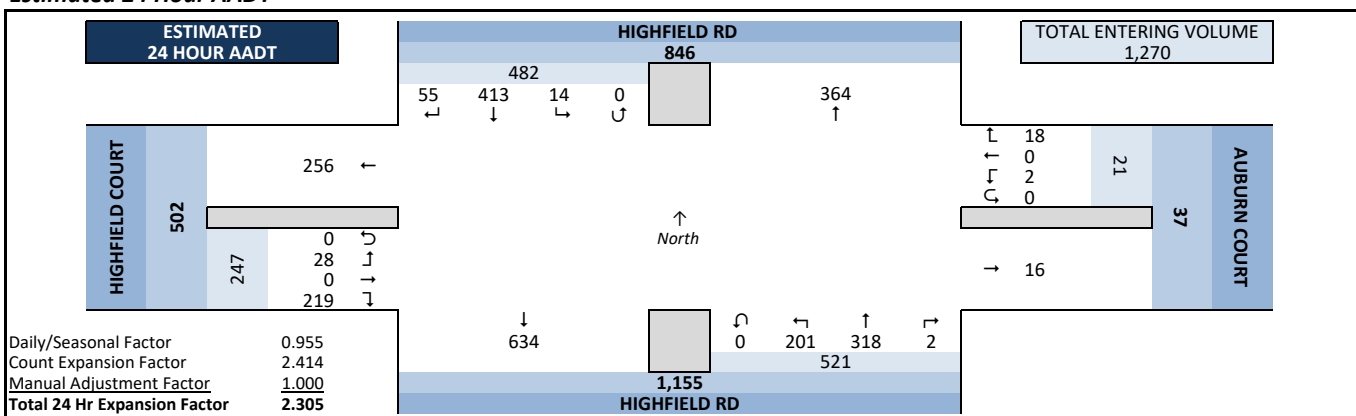
Observed 5.5 Hour Volume Summary



Total Entering Hourly Volume



Estimated 24 Hour AADT



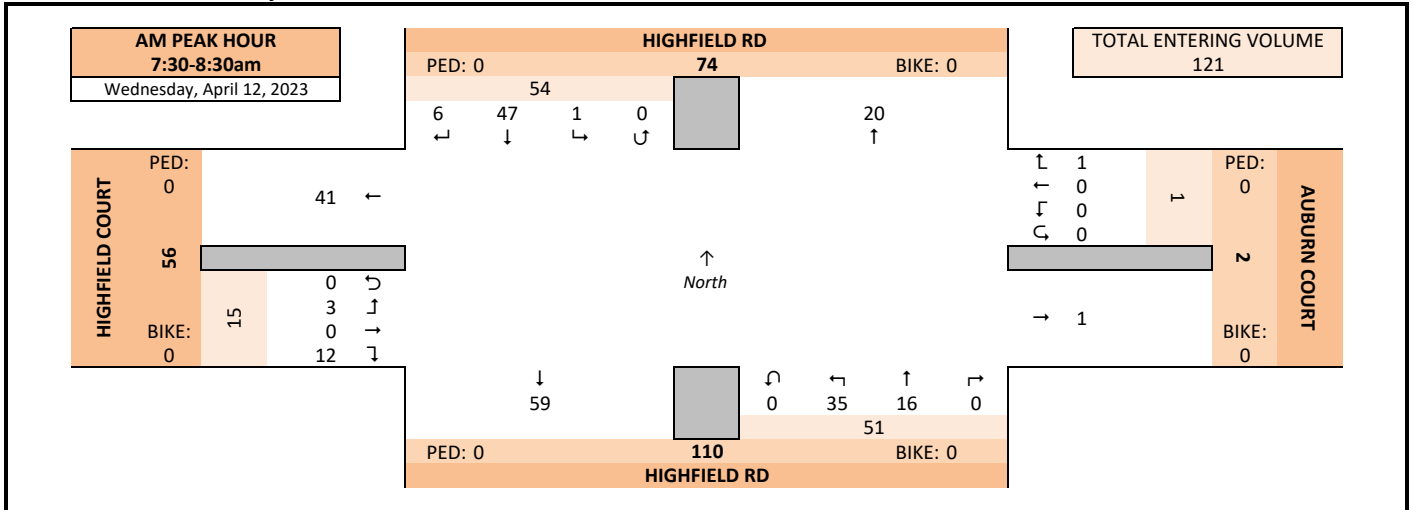
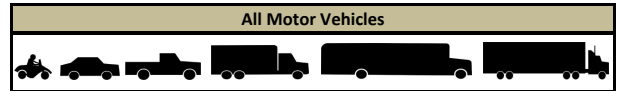
Intersection Traffic Volume Report

Count Basics		Page 2 of 13	
Start Date:	Tuesday, April 11, 2023	Weekday	Schools in Session
Total Number of Hours Counted: 5.5		Non-Holiday	No Special Events

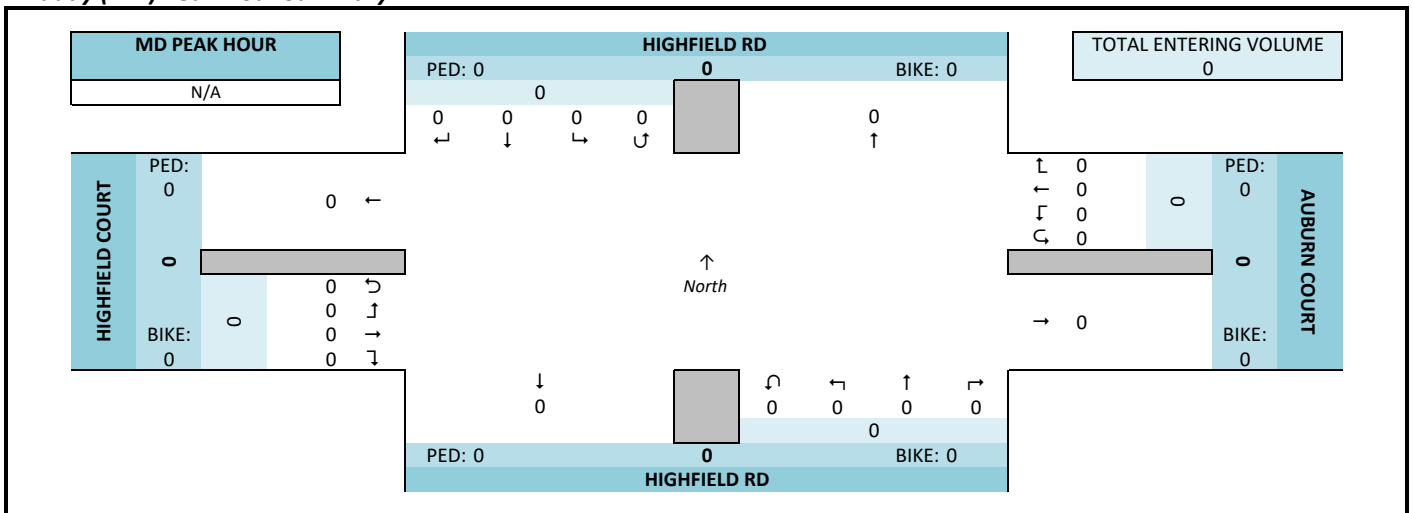
Peak Hour Volume Graphical Summary

HIGHFIELD RD & HIGHFIELD COURT

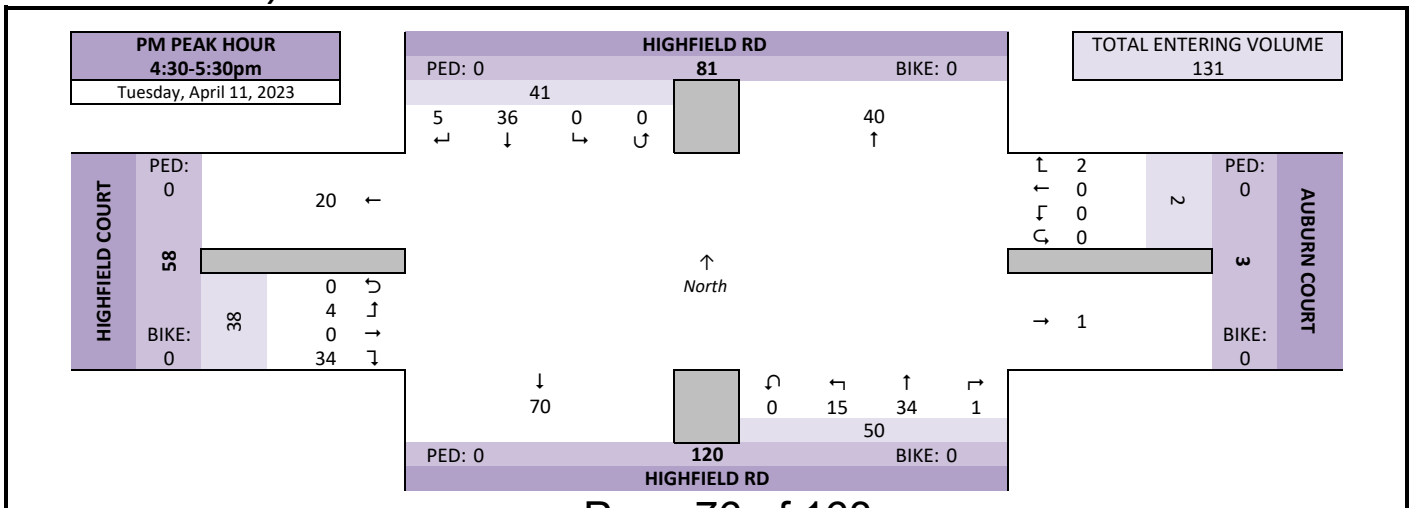
AM Peak Hour Summary



Midday (MD) Peak Hour Summary



PM Peak Hour Summary

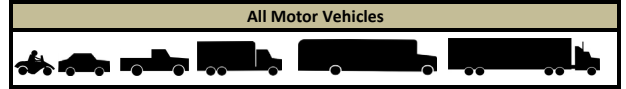


Intersection Traffic Volume Report

Count Basics			Page 3 of 13	
Start Date:	Tuesday, April 11, 2023	Weekday	Schools in Session	
Total Number of Hours Counted:	5.5	Non-Holiday	No Special Events	

Peak Hour Volume Summary

HIGHFIELD RD & HIGHFIELD COURT








Peak Hour Volumes, Truck Percentages, and PHFs

Wednesday, April 12, 2023		↓					←					↑					→					
AM Peak Hour		HIGHFIELD RD					AUBURN COURT					HIGHFIELD RD					HIGHFIELD COURT					
Start Time	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Totals	
7:30 AM	1	5	1	0	7	0	0	0	0	0	0	5	9	0	14	4	0	0	0	4	25	
7:45 AM	2	18	0	0	20	0	0	0	0	0	0	3	8	0	11	3	0	1	0	4	35	
8:00 AM	1	11	0	0	12	1	0	0	0	1	0	3	9	0	12	2	0	1	0	3	28	
8:15 AM	2	13	0	0	15	0	0	0	0	0	0	5	9	0	14	3	0	1	0	4	33	
Peak Hour Volume	6	47	1	0	54	1	0	0	0	1	0	16	35	0	51	12	0	3	0	15	121	
Rounded Hourly Volume	5	45	0	0	50	0	0	0	0	0	0	15	35	0	50	10	0	5	0	15	115	
% Single Unit Trucks	16.7	2.1	0.0	0.0	3.7	0.0	0.0	0.0	0.0	0.0	0.0	12.5	2.9	0.0	5.9	8.3	0.0	33.3	0.0	13.3	5.8	
% Heavy Trucks	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
% Trucks (Total)	16.7	2.1	0.0	0.0	3.7	0.0	0.0	0.0	0.0	0.0	0.0	12.5	2.9	0.0	5.9	8.3	0.0	33.3	0.0	13.3	5.8	
Peak Hour Factor (PHF)	0.75	0.65	0.25	0.00	0.67	0.25	0.00	0.00	0.00	0.25	0.00	0.80	0.97	0.00	0.91	0.75	0.00	0.75	0.00	0.94	0.86	

N/A		↓ From North					← From East					↑ From South					→ From West					
Midday (MD) Peak Hour	MD Peak Hour	HIGHFIELD RD					AUBURN COURT					HIGHFIELD RD					HIGHFIELD COURT					
	Start Time	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Totals
	12:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	12:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	12:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	12:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	Peak Hour Volume	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	Rounded Hourly Volume	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	% Single Unit Trucks	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
	% Heavy Trucks	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
% Trucks (Total)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Peak Hour Factor (PHF)	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	

Tuesday, April 11, 2023		↓					←					↑					→					
PM Peak Hour		From North					From East					From South					From West					
		HIGHFIELD RD					AUBURN COURT					HIGHFIELD RD					HIGHFIELD COURT					
Start Time		Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Totals
4:30 PM		1	12	0	0	13	2	0	0	0	2	1	8	2	0	11	5	0	1	0	6	32
4:45 PM		1	11	0	0	12	0	0	0	0	0	0	7	7	0	14	10	0	1	0	11	37
5:00 PM		2	10	0	0	12	0	0	0	0	0	0	9	3	0	12	12	0	2	0	14	38
5:15 PM		1	3	0	0	4	0	0	0	0	0	0	10	3	0	13	7	0	0	0	7	24
Peak Hour Volume		5	36	0	0	41	2	0	0	0	2	1	34	15	0	50	34	0	4	0	38	131
Rounded Hourly Volume		5	35	0	0	40	0	0	0	0	0	0	35	15	0	50	35	0	5	0	40	130
% Single Unit Trucks		0.0	8.3	0.0	0.0	7.3	0.0	0.0	0.0	0.0	0.0	0.0	2.9	0.0	0.0	2.0	0.0	0.0	0.0	0.0	3.1	3.1
% Heavy Trucks		0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
% Trucks (Total)		0.0	8.3	0.0	0.0	7.3	0.0	0.0	0.0	0.0	0.0	0.0	2.9	0.0	0.0	2.0	0.0	0.0	0.0	0.0	3.1	3.1
Peak Hour Factor (PHF)		0.62	0.75	0.00	0.00	0.79	0.25	0.00	0.00	0.00	0.25	0.25	0.85	0.54	0.00	0.89	0.71	0.00	0.50	0.00	0.68	0.86

Peak Hour Pedestrian and Bicyclist Volumes

Pedestrians and Bicyclists		Crossing 			Crossing 			Crossing 			Crossing 			Total Ped & Bike Volume
		North Approach			East Approach			South Approach			West Approach			
15-Minute Start Time		HIGHFIELD RD			AUBURN COURT			HIGHFIELD RD			HIGHFIELD COURT			
		Pedestrian	Bicyclist	Total	Pedestrian	Bicyclist	Total	Pedestrian	Bicyclist	Total	Pedestrian	Bicyclist	Total	
AM	7:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	
	7:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	
	8:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	
	8:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	
	Total	0	0	0	0	0	0	0	0	0	0	0	0	
MD	12:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	
	12:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	
	12:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	
	12:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	
	Total	0	0	0	0	0	0	0	0	0	0	0	0	
PM	4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	
	4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	
	5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	
	5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	
	Total	0	0	0	0	0	0	0	0	0	0	0	0	

Intersection Traffic Volume Report

Count Basics			Page 9 of 13
Start Date:	Tuesday, April 11, 2023	Weekday	Schools in Session
Total Number of Hours Counted:	5.5	Non-Holiday	No Special Events

15-Minute Heavy Vehicle Data

HIGHFIELD RD & HIGHFIELD COURT



15-Minute Heavy Vehicle Data

15-Minute Time Period	Start Time	From North HIGHFIELD RD					From East AUBURN COURT					From South HIGHFIELD RD					From West HIGHFIELD COURT					15-Min Totals	Hourly Sum
		Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total		
AM Peak Period	6:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	6:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	6:30 AM	1	0	0	0	1	0	0	0	0	0	0	2	0	0	2	0	0	1	0	1	4	5
	6:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3
	7:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	6
	7:15 AM	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	1	7
	7:30 AM	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	2	7
	7:45 AM	1	0	0	0	1	0	0	0	0	0	0	1	0	1	1	0	0	1	0	1	3	6
	8:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	0	0	1	1	3
	8:15 AM	0	0	0	0	0	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	1	
	8:30 AM	0	1	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	
	8:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	9:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	9:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	9:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	9:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Midday Peak Period	10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	10:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	10:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	10:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	11:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	11:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	11:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	11:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	12:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	12:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	12:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	12:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	1:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	1:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	1:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	1:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
PM Peak Period	2:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	2:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	2:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	2:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	3:00 PM	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	2	4
	3:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3
	3:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	3
	3:45 PM	1	0	0	0	1	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	2	3
	4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	1	0	1	1	3
	4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4
	4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	4
	4:45 PM	0	2	0	0	2	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	2	4
	5:00 PM	0	1	0	0	1	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	2	3
	5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	5:45 PM	0	0	0	0	0	1	0	0	0	1	0	0	0	0	0	0	0	0	0	0	1	
	6:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	6:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	6:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	6:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	7:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	7:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	7:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	7:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	8:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	8:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	8:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	8:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	9:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	9:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	9:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	9:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Totals		4	5	0	0	9	1	0	0	0	1	0	7	1	0	8	1	0	4	0	5	23	

Peak Hour Heavy Vehicle Volume Summary

Hourly Time Period Start Time	From North HIGHFIELD RD					From East AUBURN COURT					From South HIGHFIELD RD					From West HIGHFIELD COURT					Total Hourly Volume
	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	
AM 7:30 AM	1	1	0	0	2	0	0	0	0	0	0	2	1	0	3	1	0	1	0	2	
MD 12:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
PM 4:30 PM	0	3	0	0	3	0	0	0	0	0	0	1	0	0	1	0	0	0	0	0	

Intersection Traffic Volume Report

Count Basics		Version 2022.11.2	Page 1 of 13
Start Date:	Tuesday, April 11, 2023	Weekday	Schools in Session
Total Number of Hours Counted:	5.5	Non-Holiday	No Special Events

Base Information, Observed (5.5) Hour and Estimated (24) Hour Volume Summaries

Major St: STH 190 - Capitol Drive

Minor St: Highfield Road

Intersection of: STH 190 - Capitol Drive & Highfield Road

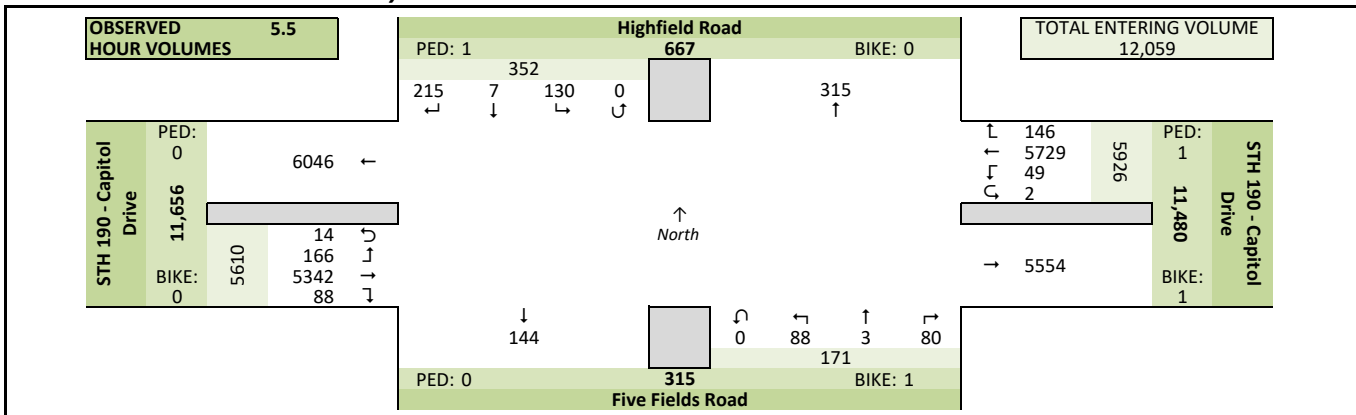
Site Information

Municipality	City of Pewaukee
County	67 - Waukesha
WisDOT Region	SE
Traffic Control	Partial Stop Control
Roadway Names	North Direction
North Leg	Highfield Road
East Leg	STH 190 - Capitol Drive
South Leg	Five Fields Road
West Leg	STH 190 - Capitol Drive
Special Considerations	
Schools	In Session
Holidays	None
Special Events	None
Special Pedestrians Observed	
Pre-school children	None
Elementary school age children	None
Visually impaired (white cane/helper dog)	None
Elderly/disabled (except wheelchairs)	None
Wheelchairs/electric scooters	None
Other (describe)	None

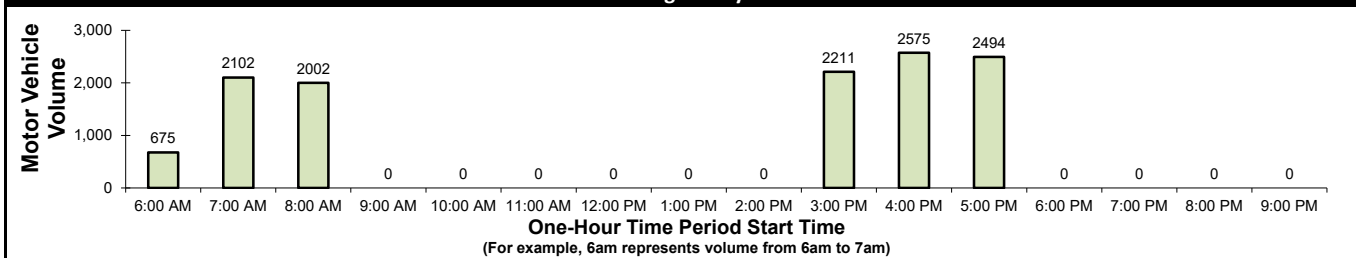
Count Information

Hrs Counted:	06:30 AM-09:00 AM and 03:00 PM-06:00 PM
1st Day of Count	Tuesday, April 11, 2023
AM Peak Period	Wednesday, April 12, 2023
Midday Peak Period	Tuesday, April 11, 2023
PM Peak Period	Tuesday, April 11, 2023
Calculated Peak Hours	
AM	7:30-8:30am
MD	
PM	4:30-5:30pm
Peak Hours Selected for Analysis	
AM	7:30-8:30am
MD	
PM	4:30-5:30pm
Daily/Seasonal Adjustment Group	(2) Urban Arterials & Collectors
Count Expansion Group	(2) Urban Arterials & Collectors
Daily/Seasonal Adjustment Factor	0.955
Count Expansion Factor	2.414
Company Name	TADI, Inc.
Manual Adj.	1.000
Observers	AM Peak Period: Amy Scheuerlein Midday Peak Period: None PM Peak Period: Amy Scheuerlein
Comments	2021 DOT Daily & Seasonal Factors

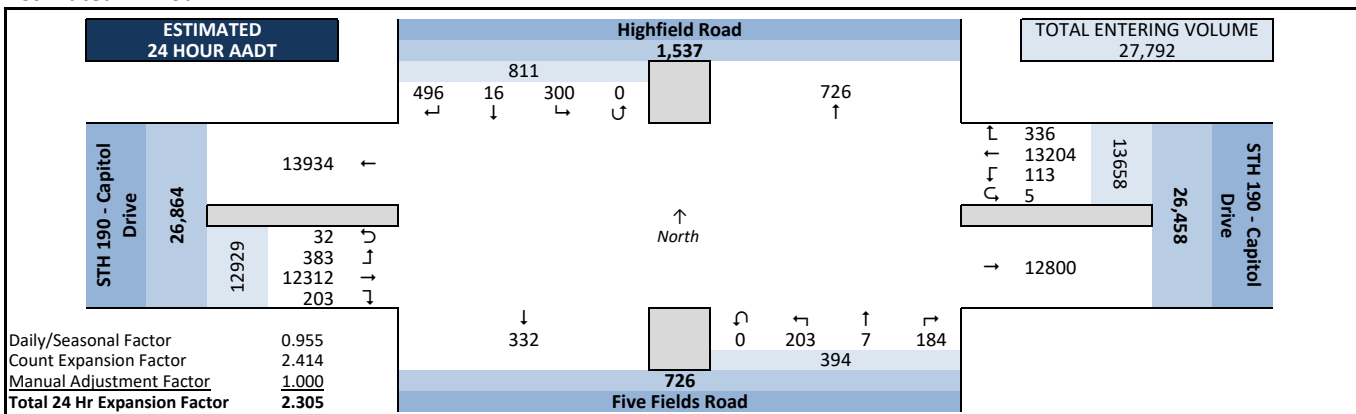
Observed 5.5 Hour Volume Summary



Total Entering Hourly Volume



Estimated 24 Hour AADT

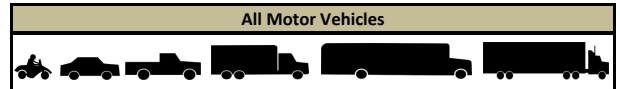


Intersection Traffic Volume Report

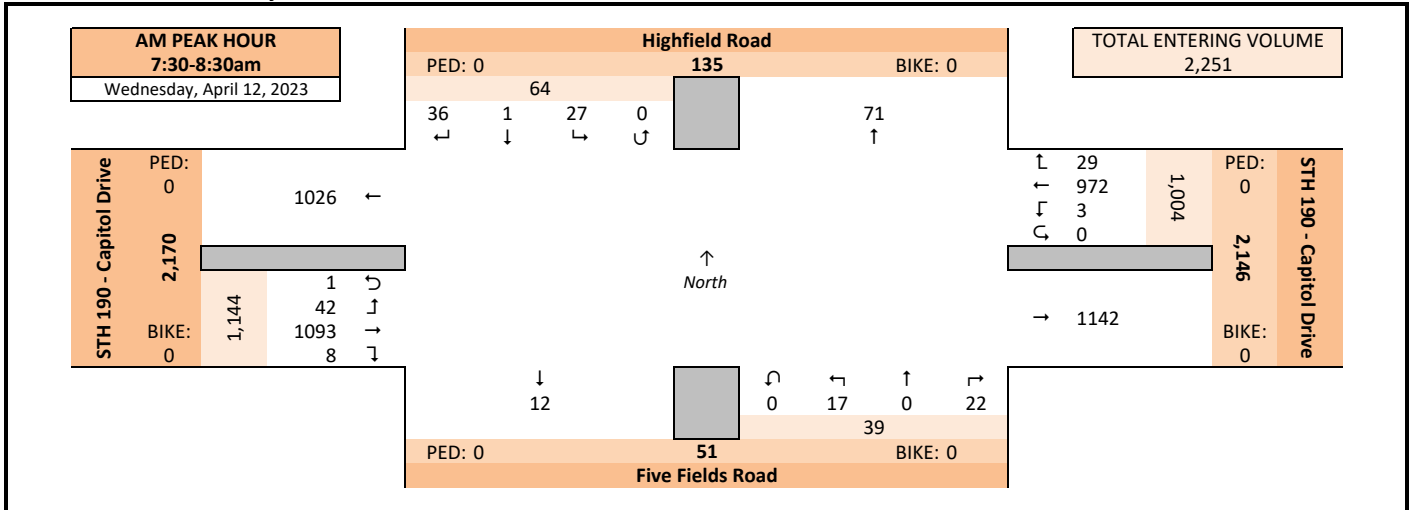
Count Basics		Page 2 of 13	
Start Date:	Tuesday, April 11, 2023	Weekday	Schools in Session
Total Number of Hours Counted: 5.5		Non-Holiday	No Special Events

Peak Hour Volume Graphical Summary

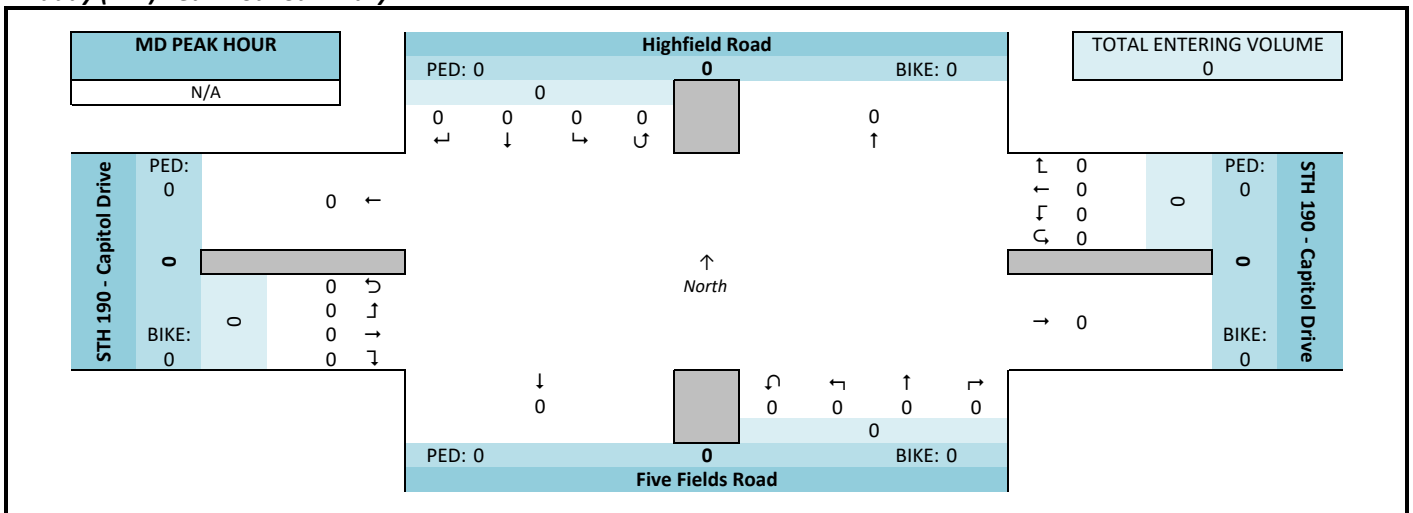
STH 190 - Capitol Drive & Highfield Road



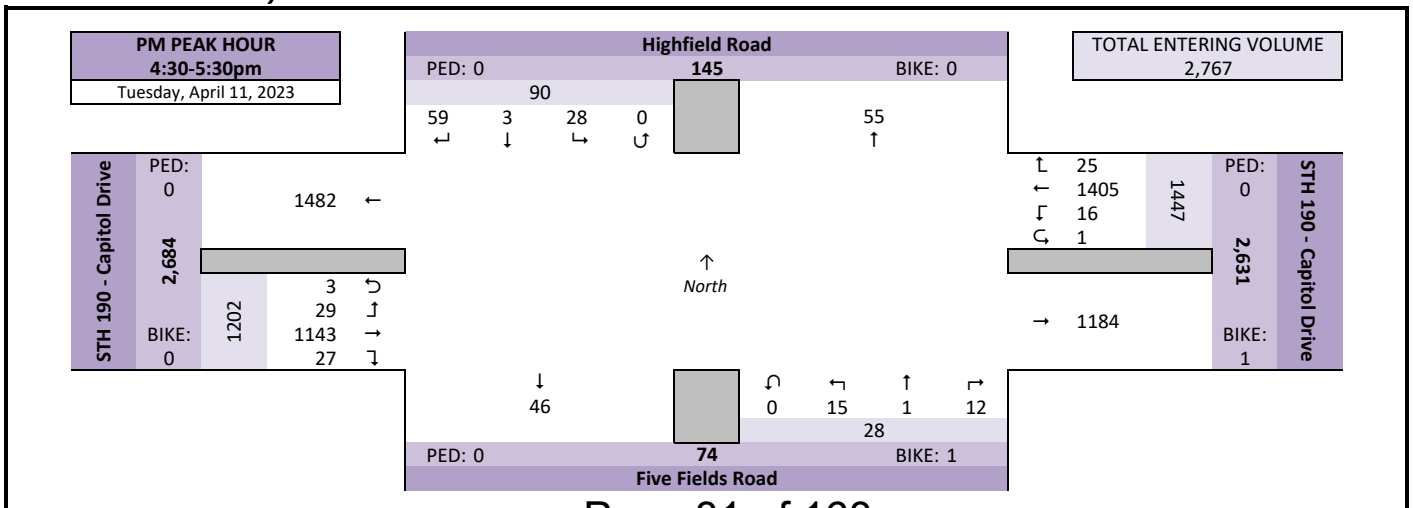
AM Peak Hour Summary



Midday (MD) Peak Hour Summary



PM Peak Hour Summary

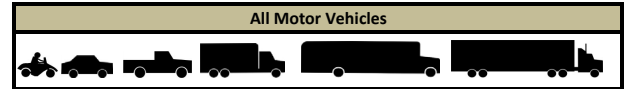


Intersection Traffic Volume Report

Count Basics			Page 3 of 13	
Start Date:	Tuesday, April 11, 2023	Weekday	Schools in Session	
Total Number of Hours Counted:	5.5	Non-Holiday	No Special Events	

Peak Hour Volume Summary

STH 190 - Capitol Drive & Highfield Road








Peak Hour Volumes, Truck Percentages, and PHFs

Wednesday, April 12, 2023		↓					←					↑					→					
		From North					From East					From South					From West					
AM Peak Hour		Highfield Road					STH 190 - Capitol Drive					Five Fields Road					STH 190 - Capitol Drive					
Start Time	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Totals	
7:30 AM	3	0	4	0	7	9	248	0	0	257	7	0	5	0	12	1	321	10	1	333	609	
7:45 AM	10	1	13	0	24	10	259	0	0	269	8	0	5	0	13	2	273	17	0	292	598	
8:00 AM	11	0	1	0	12	4	242	2	0	248	5	0	3	0	8	2	255	10	0	267	535	
8:15 AM	12	0	9	0	21	6	223	1	0	230	2	0	4	0	6	3	244	5	0	252	509	
Peak Hour Volume	36	1	27	0	64	29	972	3	0	1004	22	0	17	0	39	8	1093	42	1	1144	2251	
Rounded Hourly Volume	35	0	25	0	60	30	970	5	0	1005	20	0	15	0	35	10	1095	40	0	1145	2245	
% Single Unit Trucks	2.8	0.0	0.0	0.0	1.6	6.9	3.7	0.0	0.0	3.8	4.5	0.0	5.9	0.0	5.1	0.0	3.8	2.4	0.0	3.7	3.7	
% Heavy Trucks	0.0	0.0	0.0	0.0	0.0	0.0	0.8	0.0	0.0	0.8	0.0	0.0	0.0	0.0	0.0	0.0	1.1	0.0	0.0	1.0	0.9	
% Trucks (Total)	2.8	0.0	0.0	0.0	1.6	6.9	4.5	0.0	0.0	4.6	4.5	0.0	5.9	0.0	5.1	0.0	4.8	2.4	0.0	4.7	4.6	
Peak Hour Factor (PHF)	0.75	0.25	0.52	0.00	0.67	0.72	0.94	0.37	0.00	0.93	0.69	0.00	0.85	0.00	0.75	0.67	0.85	0.62	0.25	0.86	0.92	

N/A		↓ From North					← From East					↑ From South					→ From West					
Midday (MD) Peak Hour	MD Peak Hour	Highfield Road					STH 190 - Capitol Drive					Five Fields Road					STH 190 - Capitol Drive					
	Start Time	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Totals
	12:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	12:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	12:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	12:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	Peak Hour Volume	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	Rounded Hourly Volume	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
	% Single Unit Trucks	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
	% Heavy Trucks	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
% Trucks (Total)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Peak Hour Factor (PHF)	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	0.00	

Tuesday, April 11, 2023		↓					←					↑					→						
PM Peak Hour	PM Peak Hour	From North					From East					From South					From West						
		Highfield Road					STH 190 - Capitol Drive					Five Fields Road					STH 190 - Capitol Drive						
	Start Time	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Totals	
	4:30 PM	10		1	6	0	17	6	326	5	0	337	5	0	7	0	12	7	286	8	2	303	669
	4:45 PM	19		2	12	0	33	6	344	4	0	354	3	1	1	0	5	4	269	8	1	282	674
	5:00 PM	16	0		4	0	20	6	365	3	0	374	4	0	4	0	8	10	302	8	0	320	722
	5:15 PM	14	0		6	0	20	7	370	4	1	382	0	0	3	0	3	6	286	5	0	297	702
	Peak Hour Volume	59	3		28	0	90	25	1405	16	1	1447	12	1	15	0	28	27	1143	29	3	1202	2767
	Rounded Hourly Volume	60	5		30	0	95	25	1405	15	0	1445	10	0	15	0	25	25	1145	30	5	1205	2770
	% Single Unit Trucks	3.4	0.0		3.6	0.0	3.3	0.0	1.3	6.2	0.0	1.3	0.0	0.0	0.0	0.0	0.0	0.0	1.7	3.4	0.0	1.7	1.5
% Heavy Trucks	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.5	0.0	0.0	0.5	0.0	0.0	0.0	0.0	0.0	0.0	0.2	0.0	0.0	0.2	0.3	
% Trucks (Total)	3.4	0.0		3.6	0.0	3.3	0.0	1.8	6.2	0.0	1.8	0.0	0.0	0.0	0.0	0.0	0.0	1.8	3.4	0.0	1.8	1.8	
Peak Hour Factor (PHF)	0.78	0.37	0.58	0.00	0.68	0.89	0.95	0.80	0.25	0.95	0.60	0.25	0.54	0.00	0.58	0.67	0.95	0.91	0.37	0.94	0.96		

Peak Hour Pedestrian and Bicyclist Volumes

Pedestrians and Bicyclists		Crossing 			Crossing 			Crossing 			Crossing 			Total Ped & Bike Volume
		North Approach			East Approach			South Approach			West Approach			
Highfield Road		STH 190 - Capitol Drive			Five Fields Road			STH 190 - Capitol Drive						
15-Minute Start Time		Pedestrian	Bicyclist	Total	Pedestrian	Bicyclist	Total	Pedestrian	Bicyclist	Total	Pedestrian	Bicyclist	Total	
AM	7:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	
	7:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	
	8:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	
	8:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	
	Total	0	0	0	0	0	0	0	0	0	0	0	0	
MD	12:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	
	12:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	
	12:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	
	12:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	
	Total	0	0	0	0	0	0	0	0	0	0	0	0	
PM	4:30 PM	0	0	0	0	0	0	0	1	1	0	0	0	
	4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	
	5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	
	5:15 PM	0	0	0	0	1	1	0	0	0	0	0	1	
	Total	0	0	0	0	1	1	0	1	1	0	0	2	

Intersection Traffic Volume Report

Count Basics			Page 9 of 13
Start Date:	Tuesday, April 11, 2023	Weekday	Schools in Session
Total Number of Hours Counted:	5.5	Non-Holiday	No Special Events

15-Minute Heavy Vehicle Data

STH 190 - Capitol Drive & Highfield Road



15-Minute Heavy Vehicle Data

15-Minute Time Period Start Time	From North					From East					From South					From West					15-Min Totals	Hourly Sum	
	Highfield Road					STH 190 - Capitol Drive					Five Fields Road					STH 190 - Capitol Drive							
	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total			
AM Peak Period	6:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	6:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	6:30 AM	0	0	0	0	0	1	9	0	0	10	0	0	0	0	0	0	10	0	0	10	20	66
	6:45 AM	0	0	1	0	1	1	4	0	0	5	0	0	0	0	0	0	9	0	0	9	15	66
	7:00 AM	0	0	0	0	0	0	8	0	0	8	0	0	0	0	0	0	7	0	0	7	15	79
	7:15 AM	0	0	0	0	0	0	11	0	0	11	0	0	0	0	0	0	5	0	0	5	16	94
	7:30 AM	0	0	0	0	0	1	6	0	0	7	0	0	0	0	0	0	13	0	0	13	20	103
	7:45 AM	0	0	0	0	0	0	11	0	0	11	1	0	0	0	1	0	16	0	0	16	28	112
	8:00 AM	1	0	0	0	1	1	14	0	0	15	0	0	1	0	1	0	12	1	0	13	30	110
	8:15 AM	0	0	0	0	0	0	13	0	0	13	0	0	0	0	0	0	12	0	0	12	25	
	8:30 AM	0	0	0	0	0	1	10	0	0	11	1	0	0	0	1	0	17	0	0	17	29	
	8:45 AM	0	0	1	0	1	0	8	0	0	8	0	0	1	0	1	0	16	0	0	16	26	
	9:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	9:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	9:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	9:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
Midday Peak Period	10:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	10:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	10:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	10:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	11:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	11:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	11:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	11:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	12:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	12:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	12:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	12:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	1:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	1:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	1:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	1:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
PM Peak Period	2:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	2:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	2:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	2:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	3:00 PM	0	0	0	0	0	0	7	0	0	7	0	0	1	0	1	0	12	0	0	12	20	72
	3:15 PM	0	0	0	0	0	0	9	1	0	10	0	0	0	0	0	0	4	0	0	4	14	66
	3:30 PM	0	0	0	0	0	0	12	0	0	12	0	0	1	0	1	1	6	0	0	7	20	68
	3:45 PM	0	0	0	0	0	0	9	0	0	9	1	0	0	0	1	0	7	1	0	8	18	60
	4:00 PM	0	0	0	0	0	0	9	0	0	9	0	0	0	0	0	0	5	0	0	5	14	59
	4:15 PM	0	0	0	0	0	0	9	0	0	9	0	0	1	0	1	0	6	0	0	6	16	61
	4:30 PM	0	0	0	0	0	0	8	1	0	9	0	0	0	0	0	0	3	0	0	3	12	51
	4:45 PM	1	0	1	0	2	0	5	0	0	5	0	0	0	0	0	0	10	0	0	10	17	44
	5:00 PM	1	0	0	0	1	0	6	0	0	6	0	0	0	0	0	0	8	1	0	9	16	36
	5:15 PM	0	0	0	0	0	0	6	0	0	6	0	0	0	0	0	0	0	0	0	0	6	
	5:30 PM	0	0	0	0	0	0	1	0	0	1	1	0	0	0	1	0	3	0	0	3	5	
	5:45 PM	0	0	0	0	0	0	7	0	0	7	0	1	0	0	1	0	1	0	0	1	9	
	6:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	6:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	6:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	6:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	7:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	7:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	7:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
	7:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	
8:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
8:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
8:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
8:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
9:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
9:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
9:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
9:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0		
Totals	3	0	3	0	6	5	182	2	0	189	4	1	5	0	10	1	182	3	0	186	391		

Peak Hour Heavy Vehicle Volume Summary

Hourly Time Period Start Time	From North					From East					From South					From West					Total Hourly Volume
	Highfield Road					STH 190 - Capitol Drive					Five Fields Road					STH 190 - Capitol Drive					
	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	Right	Thru	Left	U-Tn	Total	
AM 7:30 AM	1	0	0	0	1	2	44	0	0	46	1	0	1	0	2	0	53	1	0	54	103
MD 12:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0
PM 4:30 PM	2	0	1	0	3	0	25	1	0	26	0	0	0	0	0	0	21	1	0	22	53

APPENDIX B

LOS DEFINITIONS & SYNCHRO TRAFFIC ANALYSIS

Existing Traffic

Level of Service Definitions

LOS	Signalized Intersections Control Delay/Vehicle (sec/veh)	Unsignalized Intersections Avg. Control Delay (sec/veh)	Relative Delay
A	≤10	≤10	Short Delays
	Free-flow traffic operations at average travel speeds. Vehicles completely unimpeded in ability to maneuver. Minimal delay at signalized intersections.		
B	> 10 - 20	> 10 - 15	
	Reasonably unimpeded traffic operations at average travel speeds. Vehicle maneuverability slightly restricted. Low traffic delays.		
C	> 20 - 35	> 15 - 25	
	Stable traffic operations. Lane changes becoming more restricted. Travel speeds reduced to half of average free flow travel speeds. Longer intersection delays.		
D	> 35 - 55	> 25 - 35	Moderate Delays
	Small increases in traffic flow can cause increased delays. Delays likely attributable to increased traffic, reduced signal progression, and adverse timing.		
E	> 55 - 80	> 35 - 50	
	Significant delays. Travel speeds reduced to one-third of average free flow travel speed.		
F	> 80	> 50	Long Delays
	Extremely low speeds. Intersection congestion. Long delays. Extensive traffic queues at intersections.		























Source: Highway Capacity Manual, Transportation Research Board, Washington, D.C., 2010

Lanes, Volumes, Timings

100: Pewaukee Road & Meadow Creek Market/Swan Road

Existing

AM Peak Hour













												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	55	15	5	25	15	35	10	945	40	50	405	50
Future Volume (vph)	55	15	5	25	15	35	10	945	40	50	405	50
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1972	1972	1900	1972	1972	1900
Lane Width (ft)	12	12	12	12	12	12	12	12	12	12	12	12
Grade (%)		0%			0%			0%			0%	
Storage Length (ft)	120		0	210		0	435		230	355		315
Storage Lanes	1		0	1		0	1		1	1		1
Taper Length (ft)	100			100			100			100		
Right Turn on Red			No			No			No			No
Link Speed (mph)		35			35			45			45	
Link Distance (ft)		420			477			663			577	
Travel Time (s)		8.2			9.3			10.0			8.7	
Confl. Peds. (#/hr)												
Confl. Bikes (#/hr)												
Peak Hour Factor	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91
Growth Factor	100%	100%	100%	100%	100%	100%	100%	100%	62%	100%	100%	62%
Heavy Vehicles (%)	10%	10%	10%	9%	9%	9%	6%	6%	6%	11%	11%	11%
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	0	0	0	0
Parking (#/hr)												
Mid-Block Traffic (%)		0%			0%			0%			0%	
Shared Lane Traffic (%)												
Lane Group Flow (vph)	60	21	0	27	54	0	11	1038	27	55	445	34
Turn Type	Perm	NA		Perm	NA		pm+pt	NA	Perm	Perm	NA	Perm
Protected Phases		4			8		1	6			2	
Permitted Phases	4			8			6		6	2		2
Detector Phase	4	4		8	8		1	6	6	2	2	2
Switch Phase												
Minimum Initial (s)	8.0	8.0		8.0	8.0		8.0	12.0	12.0	12.0	12.0	12.0
Minimum Split (s)	14.9	14.9		15.3	15.3		13.5	19.3	19.3	19.3	19.3	19.3
Total Split (s)	19.0	19.0		19.0	19.0		14.0	67.0	67.0	53.0	53.0	53.0
Total Split (%)	22.1%	22.1%		22.1%	22.1%		16.3%	77.9%	77.9%	61.6%	61.6%	61.6%
Maximum Green (s)	12.1	12.1		11.7	11.7		8.5	59.7	59.7	45.7	45.7	45.7
Yellow Time (s)	3.9	3.9		4.3	4.3		3.5	4.3	4.3	4.3	4.3	4.3
All-Red Time (s)	3.0	3.0		3.0	3.0		2.0	3.0	3.0	3.0	3.0	3.0
Lost Time Adjust (s)	0.0	0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	6.9	6.9		7.3	7.3		5.5	7.3	7.3	7.3	7.3	7.3
Lead/Lag							Lead			Lag	Lag	Lag
Lead-Lag Optimize?												
Vehicle Extension (s)	1.5	1.5		1.5	1.5		1.5	3.0	3.0	4.8	4.8	4.8
Minimum Gap (s)	1.5	1.5		1.5	1.5		1.5	2.0	2.0	3.8	3.8	3.8
Time Before Reduce (s)	30.0	30.0		30.0	30.0		25.0	19.0	19.0	29.0	29.0	29.0
Time To Reduce (s)	0.0	0.0		0.0	0.0		0.0	13.0	13.0	8.0	8.0	8.0
Recall Mode	None	None		None	None		None	C-Max	C-Max	C-Max	C-Max	C-Max
Walk Time (s)												
Flash Dont Walk (s)												
Pedestrian Calls (#/hr)												
v/c Ratio	0.44	0.12		0.20	0.33		0.01	0.38	0.02	0.15	0.18	0.03
Control Delay	46.1	35.3		38.1	40.9		3.2	4.6	3.7	8.0	5.5	6.2

Lanes, Volumes, Timings

100: Pewaukee Road & Meadow Creek Market/Swan Road

Existing

AM Peak Hour

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Queue Delay	0.0	0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	46.1	35.3		38.1	40.9		3.2	4.6	3.7	8.0	5.5	6.2
90th %ile Green (s)	12.1	12.1		11.7	11.7		8.0	59.7	59.7	46.2	46.2	46.2
90th %ile Term Code	Max	Max		Hold	Hold		Min	Coord	Coord	Coord	Coord	Coord
70th %ile Green (s)	9.8	9.8		9.4	9.4		0.0	62.0	62.0	62.0	62.0	62.0
70th %ile Term Code	Gap	Gap		Hold	Hold		Skip	Coord	Coord	Coord	Coord	Coord
50th %ile Green (s)	8.4	8.4		8.0	8.0		0.0	63.4	63.4	63.4	63.4	63.4
50th %ile Term Code	Hold	Hold		Min	Min		Skip	Coord	Coord	Coord	Coord	Coord
30th %ile Green (s)	8.4	8.4		8.0	8.0		0.0	63.4	63.4	63.4	63.4	63.4
30th %ile Term Code	Hold	Hold		Min	Min		Skip	Coord	Coord	Coord	Coord	Coord
10th %ile Green (s)	0.0	0.0		0.0	0.0		0.0	78.7	78.7	78.7	78.7	78.7
10th %ile Term Code	Skip	Skip		Skip	Skip		Skip	Coord	Coord	Coord	Coord	Coord
Queue Length 50th (ft)	31	11		14	28		1	89	3	7	31	4
Queue Length 95th (ft)	67	31		37	61		5	142	11	38	90	21
Internal Link Dist (ft)		340			397			583			497	
Turn Bay Length (ft)	120			210			435		230	355		315
Base Capacity (vph)	175	234		176	211		747	2750	1185	366	2519	1086
Starvation Cap Reductn	0	0		0	0		0	0	0	0	0	0
Spillback Cap Reductn	0	0		0	0		0	0	0	0	0	0
Storage Cap Reductn	0	0		0	0		0	0	0	0	0	0
Reduced v/c Ratio	0.34	0.09		0.15	0.26		0.01	0.38	0.02	0.15	0.18	0.03

Intersection Summary

Area Type: Other

Cycle Length: 86

Actuated Cycle Length: 86

Offset: 4 (5%), Referenced to phase 2:SBTL and 6:NBTL, Start of Yellow

Natural Cycle: 50























Control Type: Actuated-Coordinated

Splits and Phases: 100: Pewaukee Road & Meadow Creek Market/Swan Road



HCM 6th Signalized Intersection Summary 100: Pewaukee Road & Meadow Creek Market/Swan Road

Existing
AM Peak Hour

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	55	15	5	25	15	35	10	945	40	50	405	50
Future Volume (veh/h)	55	15	5	25	15	35	10	945	40	50	405	50
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No			No			No		
Adj Sat Flow, veh/h/ln	1752	1752	1752	1767	1767	1767	1880	1880	1811	1803	1803	1737
Adj Flow Rate, veh/h	60	16	5	27	16	38	11	1038	27	55	445	34
Peak Hour Factor	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91
Percent Heavy Veh, %	10	10	10	9	9	9	6	6	6	11	11	11
Cap, veh/h	169	128	40	200	46	110	667	2608	1121	400	2209	949
Arrive On Green	0.10	0.10	0.10	0.10	0.10	0.10	0.02	0.73	0.73	0.64	0.64	0.64
Sat Flow, veh/h	1264	1280	400	1314	465	1103	1790	3571	1535	511	3425	1472
Grp Volume(v), veh/h	60	0	21	27	0	54	11	1038	27	55	445	34
Grp Sat Flow(s),veh/h/ln	1264	0	1680	1314	0	1568	1790	1786	1535	511	1713	1472
Q Serve(g_s), s	4.0	0.0	1.0	1.6	0.0	2.8	0.2	9.5	0.4	3.9	4.6	0.7
Cycle Q Clear(g_c), s	6.8	0.0	1.0	2.6	0.0	2.8	0.2	9.5	0.4	6.1	4.6	0.7
Prop In Lane	1.00		0.24	1.00		0.70	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	169	0	168	200	0	157	667	2608	1121	400	2209	949
V/C Ratio(X)	0.35	0.00	0.13	0.13	0.00	0.34	0.02	0.40	0.02	0.14	0.20	0.04
Avail Cap(c_a), veh/h	221	0	236	247	0	213	805	2608	1121	400	2209	949
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	39.2	0.0	35.3	36.5	0.0	36.1	4.4	4.4	3.2	7.0	6.2	5.6
Incr Delay (d2), s/veh	0.5	0.0	0.1	0.1	0.0	0.5	0.0	0.5	0.0	0.7	0.2	0.1
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	1.2	0.0	0.4	0.5	0.0	1.1	0.0	2.3	0.1	0.4	1.3	0.2
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	39.7	0.0	35.4	36.6	0.0	36.6	4.4	4.9	3.2	7.7	6.4	5.6
LnGrp LOS	D	A	D	D	A	D	A	A	A	A	A	A
Approach Vol, veh/h	81			81			1076			534		
Approach Delay, s/veh	38.6			36.6			4.8			6.5		
Approach LOS	D			D			A			A		
Timer - Assigned Phs	1	2	4		6		8					
Phs Duration (G+Y+Rc), s	7.3	62.8	15.9		70.1		15.9					
Change Period (Y+Rc), s	5.5	7.3	* 7.3		7.3		7.3					
Max Green Setting (Gmax), s	8.5	45.7	* 12		59.7		11.7					
Max Q Clear Time (g_c+I1), s	2.2	8.1	8.8		11.5		4.8					
Green Ext Time (p_c), s	0.0	7.0	0.0		8.7		0.1					

Intersection Summary










HCM 6th Ctrl Delay	8.3
HCM 6th LOS	A




Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Lanes, Volumes, Timings
200: Highfield Road & Broken Hill Circle S

















Existing
AM Peak Hour

						
Lane Group	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Volume (vph)	5	20	35	5	5	20
Future Volume (vph)	5	20	35	5	5	20
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	12	12	12	12	12
Grade (%)	0%			0%	0%	
Storage Length (ft)		0	0		0	0
Storage Lanes		0	0		1	0
Taper Length (ft)			100		100	
Link Speed (mph)	25			25	25	
Link Distance (ft)	314			325	406	
Travel Time (s)	8.6			8.9	11.1	
Confl. Peds. (#/hr)						
Confl. Bikes (#/hr)						
Peak Hour Factor	0.77	0.77	0.77	0.77	0.77	0.77
Growth Factor	100%	100%	100%	100%	100%	100%
Heavy Vehicles (%)	17%	17%	1%	1%	12%	12%
Bus Blockages (#/hr)	0	0	0	0	0	0
Parking (#/hr)						
Mid-Block Traffic (%)	0%			0%	0%	
Shared Lane Traffic (%)						
Lane Group Flow (vph)	32	0	0	51	32	0
Sign Control	Free			Free	Stop	
Intersection Summary						
Area Type:	Other					
Control Type:	Unsignalized					

Intersection						
Int Delay, s/veh	5.3					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	5	20	35	5	5	20
Future Vol, veh/h	5	20	35	5	5	20
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	77	77	77	77	77	77
Heavy Vehicles, %	17	17	1	1	12	12
Mvmt Flow	6	26	45	6	6	26
Major/Minor	Major1		Major2		Minor1	
Conflicting Flow All	0	0	32	0	115	19
Stage 1	-	-	-	-	19	-
Stage 2	-	-	-	-	96	-
Critical Hdwy	-	-	4.11	-	6.52	6.32
Critical Hdwy Stg 1	-	-	-	-	5.52	-
Critical Hdwy Stg 2	-	-	-	-	5.52	-
Follow-up Hdwy	-	-	2.209	-	3.608	3.408
Pot Cap-1 Maneuver	-	-	1587	-	858	1031
Stage 1	-	-	-	-	978	-
Stage 2	-	-	-	-	903	-
Platoon blocked, %	-	-		-		
Mov Cap-1 Maneuver	-	-	1587	-	834	1031
Mov Cap-2 Maneuver	-	-	-	-	834	-
Stage 1	-	-	-	-	978	-
Stage 2	-	-	-	-	878	-
Approach	EB		WB		NB	
HCM Control Delay, s	0		6.4		8.8	
HCM LOS	A					
Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT	
Capacity (veh/h)	984	-	-	1587	-	
HCM Lane V/C Ratio	0.033	-	-	0.029	-	
HCM Control Delay (s)	8.8	-	-	7.3	0	
HCM Lane LOS	A	-	-	A	A	
HCM 95th %tile Q(veh)	0.1	-	-	0.1	-	

Lanes, Volumes, Timings
300: Highfield Road & Highfield Ct./Auburn Ct.

























Existing
AM Peak Hour










												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	5	1	10	1	1	1	35	20	1	1	50	5
Future Volume (vph)	5	1	10	1	1	1	35	20	1	1	50	5
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	12	12	12	12	12	12	12	12	12	12	12
Grade (%)		0%			0%			0%			0%	
Storage Length (ft)	0		0	0		0	0		0	0		0
Storage Lanes	0		0	0		0	0		0	0		0
Taper Length (ft)	100			100			100			100		
Link Speed (mph)		25			25			25			25	
Link Distance (ft)		327			194			539			406	
Travel Time (s)		8.9			5.3			14.7			11.1	
Confl. Peds. (#/hr)												
Confl. Bikes (#/hr)												
Peak Hour Factor	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86
Growth Factor	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%
Heavy Vehicles (%)	13%	13%	13%	1%	1%	1%	6%	6%	6%	4%	4%	4%
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	0	0	0	0
Parking (#/hr)												
Mid-Block Traffic (%)		0%			0%			0%			0%	
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	19	0	0	3	0	0	65	0	0	65	0
Sign Control		Stop			Stop			Free			Free	
Intersection Summary												
Area Type:	Other											
Control Type:	Unsignalized											

Intersection												
Int Delay, s/veh	3.4											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	5	1	10	1	1	1	35	20	1	1	50	5
Future Vol, veh/h	5	1	10	1	1	1	35	20	1	1	50	5
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	86	86	86	86	86	86	86	86	86	86	86	86
Heavy Vehicles, %	13	13	13	1	1	1	6	6	6	4	4	4
Mvmt Flow	6	1	12	1	1	1	41	23	1	1	58	6
Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	170	169	61	176	172	24	64	0	0	24	0	0
Stage 1	63	63	-	106	106	-	-	-	-	-	-	-
Stage 2	107	106	-	70	66	-	-	-	-	-	-	-
Critical Hdwy	7.23	6.63	6.33	7.11	6.51	6.21	4.16	-	-	4.14	-	-
Critical Hdwy Stg 1	6.23	5.63	-	6.11	5.51	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.23	5.63	-	6.11	5.51	-	-	-	-	-	-	-
Follow-up Hdwy	3.617	4.117	3.417	3.509	4.009	3.309	2.254	-	-	2.236	-	-
Pot Cap-1 Maneuver	770	704	974	789	723	1055	1513	-	-	1578	-	-
Stage 1	921	821	-	902	809	-	-	-	-	-	-	-
Stage 2	872	787	-	942	842	-	-	-	-	-	-	-
Platoon blocked, %								-	-		-	-
Mov Cap-1 Maneuver	752	684	974	762	703	1055	1513	-	-	1578	-	-
Mov Cap-2 Maneuver	752	684	-	762	703	-	-	-	-	-	-	-
Stage 1	896	820	-	878	787	-	-	-	-	-	-	-
Stage 2	846	766	-	929	841	-	-	-	-	-	-	-
Approach	EB		WB		NB		SB					
HCM Control Delay, s	9.2		9.4		4.7		0.1					
HCM LOS	A		A									
Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR					
Capacity (veh/h)	1513	-	-	871	815	1578	-	-				
HCM Lane V/C Ratio	0.027	-	-	0.021	0.004	0.001	-	-				
HCM Control Delay (s)	7.4	0	-	9.2	9.4	7.3	0	-				
HCM Lane LOS	A	A	-	A	A	A	A	-				
HCM 95th %tile Q(veh)	0.1	-	-	0.1	0	0	-	-				

Lanes, Volumes, Timings
400: Highfield Road & Capitol Drive

Existing
AM Peak Hour

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		 			 			 				
Traffic Volume (vph)	40	1095	10	5	970	30	15	1	20	25	1	35
Future Volume (vph)	40	1095	10	5	970	30	15	1	20	25	1	35
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	12	12	12	12	12	12	12	12	12	12	12
Grade (%)		0%			0%			0%			0%	
Storage Length (ft)	355		305	330		290	0		0	0		100
Storage Lanes	1		1	1		1	0		0	0		1
Taper Length (ft)	100			100			100			100		
Link Speed (mph)		55			55			25			25	
Link Distance (ft)		989			984			394			539	
Travel Time (s)		12.3			12.2			10.7			14.7	
Confl. Peds. (#/hr)												
Confl. Bikes (#/hr)												
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Growth Factor	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%
Heavy Vehicles (%)	5%	5%	5%	5%	5%	5%	5%	5%	5%	2%	2%	2%
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	0	0	0	0
Parking (#/hr)												
Mid-Block Traffic (%)		0%			0%			0%			0%	
Shared Lane Traffic (%)												
Lane Group Flow (vph)	43	1190	11	5	1054	33	0	39	0	0	28	38
Sign Control		Free			Free			Stop			Stop	
Intersection Summary												
Area Type:	Other											
Control Type:	Unsignalized											

Intersection												
Int Delay, s/veh	1.2											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	40	1095	10	5	970	30	15	1	20	25	1	35
Future Vol, veh/h	40	1095	10	5	970	30	15	1	20	25	1	35
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	355	-	305	330	-	290	-	-	-	-	-	100
Veh in Median Storage, #	-	0	-	-	0	-	-	2	-	-	2	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	5	5	5	5	5	5	5	5	5	2	2	2
Mvmt Flow	43	1190	11	5	1054	33	16	1	22	27	1	38

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	1087	0	0	1201	0	0	1814	2373	595	1746	2351	527
Stage 1	-	-	-	-	-	-	1276	1276	-	1064	1064	-
Stage 2	-	-	-	-	-	-	538	1097	-	682	1287	-
Critical Hdwy	4.2	-	-	4.2	-	-	7.6	6.6	7	7.54	6.54	6.94
Critical Hdwy Stg 1	-	-	-	-	-	-	6.6	5.6	-	6.54	5.54	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.6	5.6	-	6.54	5.54	-
Follow-up Hdwy	2.25	-	-	2.25	-	-	3.55	4.05	3.35	3.52	4.02	3.32
Pot Cap-1 Maneuver	620	-	-	560	-	-	47	33	440	55	35	496
Stage 1	-	-	-	-	-	-	172	230	-	238	298	-
Stage 2	-	-	-	-	-	-	487	281	-	406	233	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	620	-	-	560	-	-	41	30	440	49	32	496
Mov Cap-2 Maneuver	-	-	-	-	-	-	143	151	-	182	164	-
Stage 1	-	-	-	-	-	-	160	214	-	222	295	-
Stage 2	-	-	-	-	-	-	444	278	-	357	217	-

Approach	EB			WB			NB			SB		
HCM Control Delay, s	0.4			0.1			23.9			19.5		
HCM LOS							C			C		























Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1	SBLn2
Capacity (veh/h)	229	620	-	-	560	-	-	181	496
HCM Lane V/C Ratio	0.171	0.07	-	-	0.01	-	-	0.156	0.077
HCM Control Delay (s)	23.9	11.2	-	-	11.5	-	-	28.5	12.9
HCM Lane LOS	C	B	-	-	B	-	-	D	B
HCM 95th %tile Q(veh)	0.6	0.2	-	-	0	-	-	0.5	0.2

Lanes, Volumes, Timings

100: Pewaukee Road & Meadow Creek Market/Swan Road

Existing

PM Peak Hour

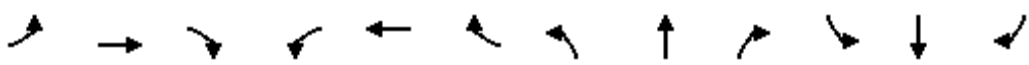
												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	105	45	110	85	20	5	120	1230	100	5	775	85
Future Volume (vph)	105	45	110	85	20	5	120	1230	100	5	775	85
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1972	1972	1900	1972	1972	1900
Lane Width (ft)	12	12	12	12	12	12	12	12	12	12	12	12
Grade (%)		0%			0%			0%			0%	
Storage Length (ft)	120		0	210		0	435		230	355		315
Storage Lanes	1		0	1		0	1		1	1		1
Taper Length (ft)	100			100			100			100		
Right Turn on Red			No			No			No			No
Link Speed (mph)		35			35			45			45	
Link Distance (ft)		420			477			663			577	
Travel Time (s)		8.2			9.3			10.0			8.7	
Confl. Peds. (#/hr)												
Confl. Bikes (#/hr)												
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Growth Factor	100%	100%	100%	100%	100%	100%	100%	100%	62%	100%	100%	62%
Heavy Vehicles (%)	1%	1%	1%	4%	4%	4%	1%	1%	1%	3%	3%	3%
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	0	0	0	0
Parking (#/hr)												
Mid-Block Traffic (%)		0%			0%			0%			0%	
Shared Lane Traffic (%)												
Lane Group Flow (vph)	109	162	0	89	26	0	125	1281	65	5	807	55
Turn Type	Perm	NA		Perm	NA		pm+pt	NA	Perm	Perm	NA	Perm
Protected Phases		4			8		1	6			2	
Permitted Phases	4			8			6		6	2		2
Detector Phase	4	4		8	8		1	6	6	2	2	2
Switch Phase												
Minimum Initial (s)	8.0	8.0		8.0	8.0		8.0	12.0	12.0	12.0	12.0	12.0
Minimum Split (s)	14.9	14.9		15.3	15.3		13.5	19.3	19.3	19.3	19.3	19.3
Total Split (s)	21.0	21.0		21.0	21.0		20.0	59.0	59.0	39.0	39.0	39.0
Total Split (%)	26.3%	26.3%		26.3%	26.3%		25.0%	73.8%	73.8%	48.8%	48.8%	48.8%
Maximum Green (s)	14.1	14.1		13.7	13.7		14.5	51.7	51.7	31.7	31.7	31.7
Yellow Time (s)	3.9	3.9		4.3	4.3		3.5	4.3	4.3	4.3	4.3	4.3
All-Red Time (s)	3.0	3.0		3.0	3.0		2.0	3.0	3.0	3.0	3.0	3.0
Lost Time Adjust (s)	0.0	0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	6.9	6.9		7.3	7.3		5.5	7.3	7.3	7.3	7.3	7.3
Lead/Lag							Lead			Lag	Lag	Lag
Lead-Lag Optimize?												
Vehicle Extension (s)	1.5	1.5		1.5	1.5		1.5	3.0	3.0	4.8	4.8	4.8
Minimum Gap (s)	1.5	1.5		1.5	1.5		1.5	2.0	2.0	3.8	3.8	3.8
Time Before Reduce (s)	30.0	30.0		30.0	30.0		25.0	19.0	19.0	29.0	29.0	29.0
Time To Reduce (s)	0.0	0.0		0.0	0.0		0.0	13.0	13.0	8.0	8.0	8.0
Recall Mode	None	None		None	None		None	C-Max	C-Max	C-Max	C-Max	C-Max
Walk Time (s)												
Flash Dont Walk (s)												
Pedestrian Calls (#/hr)												
v/c Ratio	0.55	0.68		0.54	0.11		0.25	0.51	0.06	0.02	0.41	0.06
Control Delay	42.2	46.7		44.2	29.9		5.4	7.4	5.0	11.4	12.7	11.1

Lanes, Volumes, Timings

100: Pewaukee Road & Meadow Creek Market/Swan Road

Existing

PM Peak Hour

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Queue Delay	0.0	0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	42.2	46.7		44.2	29.9		5.4	7.4	5.0	11.4	12.7	11.1
90th %ile Green (s)	14.1	14.1		13.7	13.7		8.0	51.7	51.7	38.2	38.2	38.2
90th %ile Term Code	Max	Max		Max	Max		Min	Coord	Coord	Coord	Coord	Coord
70th %ile Green (s)	13.6	13.6		13.2	13.2		8.0	52.2	52.2	38.7	38.7	38.7
70th %ile Term Code	Gap	Gap		Hold	Hold		Min	Coord	Coord	Coord	Coord	Coord
50th %ile Green (s)	11.5	11.5		11.1	11.1		8.0	54.3	54.3	40.8	40.8	40.8
50th %ile Term Code	Gap	Gap		Hold	Hold		Min	Coord	Coord	Coord	Coord	Coord
30th %ile Green (s)	9.4	9.4		9.0	9.0		8.0	56.4	56.4	42.9	42.9	42.9
30th %ile Term Code	Gap	Gap		Hold	Hold		Min	Coord	Coord	Coord	Coord	Coord
10th %ile Green (s)	8.4	8.4		8.0	8.0		0.0	57.4	57.4	57.4	57.4	57.4
10th %ile Term Code	Hold	Hold		Hold	Hold		Skip	Coord	Coord	Coord	Coord	Coord
Queue Length 50th (ft)	51	78		42	12		17	141	9	1	125	13
Queue Length 95th (ft)	98	135		85	32		36	207	24	7	182	33
Internal Link Dist (ft)		340			397			583			497	
Turn Bay Length (ft)	120			210			435		230	355		315
Base Capacity (vph)	245	296		204	303		617	2522	1087	225	1982	854
Starvation Cap Reductn	0	0		0	0		0	0	0	0	0	0
Spillback Cap Reductn	0	0		0	0		0	0	0	0	0	0
Storage Cap Reductn	0	0		0	0		0	0	0	0	0	0
Reduced v/c Ratio	0.44	0.55		0.44	0.09		0.20	0.51	0.06	0.02	0.41	0.06

Intersection Summary

Area Type: Other

Cycle Length: 80

Actuated Cycle Length: 80

Offset: 70 (88%), Referenced to phase 2:SBTL and 6:NBTL, Start of Yellow

Natural Cycle: 55























Control Type: Actuated-Coordinated

Splits and Phases: 100: Pewaukee Road & Meadow Creek Market/Swan Road



HCM 6th Signalized Intersection Summary 100: Pewaukee Road & Meadow Creek Market/Swan Road

Existing
PM Peak Hour

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	105	45	110	85	20	5	120	1230	100	5	775	85
Future Volume (veh/h)	105	45	110	85	20	5	120	1230	100	5	775	85
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No			No			No		
Adj Sat Flow, veh/h/ln	1885	1885	1885	1841	1841	1841	1957	1957	1885	1926	1926	1856
Adj Flow Rate, veh/h	109	47	115	89	21	5	125	1281	65	5	807	55
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Percent Heavy Veh, %	1	1	1	4	4	4	1	1	1	3	3	3
Cap, veh/h	312	83	203	189	246	59	491	2402	1032	282	1770	761
Arrive On Green	0.17	0.17	0.17	0.17	0.17	0.17	0.09	0.65	0.65	0.48	0.48	0.48
Sat Flow, veh/h	1396	485	1187	1205	1437	342	1863	3718	1598	418	3659	1572
Grp Volume(v), veh/h	109	0	162	89	0	26	125	1281	65	5	807	55
Grp Sat Flow(s),veh/h/ln	1396	0	1672	1205	0	1779	1863	1859	1598	418	1830	1572
Q Serve(g_s), s	5.7	0.0	7.1	5.9	0.0	1.0	2.3	14.9	1.2	0.5	11.7	1.5
Cycle Q Clear(g_c), s	6.7	0.0	7.1	13.0	0.0	1.0	2.3	14.9	1.2	2.4	11.7	1.5
Prop In Lane	1.00		0.71	1.00		0.19	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	312	0	286	189	0	305	491	2402	1032	282	1770	761
V/C Ratio(X)	0.35	0.00	0.57	0.47	0.00	0.09	0.25	0.53	0.06	0.02	0.46	0.07
Avail Cap(c_a), veh/h	319	0	295	189	0	305	654	2402	1032	282	1770	761
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	30.7	0.0	30.4	36.4	0.0	27.9	8.2	7.6	5.2	11.8	13.7	11.0
Incr Delay (d2), s/veh	0.2	0.0	1.4	0.7	0.0	0.0	0.1	0.9	0.1	0.1	0.8	0.2
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	1.9	0.0	2.9	1.7	0.0	0.4	0.7	4.5	0.3	0.1	4.3	0.5
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	30.9	0.0	31.8	37.0	0.0	27.9	8.3	8.5	5.3	11.9	14.5	11.2
LnGrp LOS	C	A	C	D	A	C	A	A	A	B	B	B
Approach Vol, veh/h	271			115			1471			867		
Approach Delay, s/veh	31.5			35.0			8.3			14.3		
Approach LOS	C			C			A			B		
Timer - Assigned Phs	1	2	4		6		8					
Phs Duration (G+Y+Rc), s	13.0	46.0	21.0		59.0		21.0					
Change Period (Y+Rc), s	5.5	7.3	* 7.3		7.3		7.3					
Max Green Setting (Gmax), s	14.5	31.7	* 14		51.7		13.7					
Max Q Clear Time (g_c+I1), s	4.3	13.7	9.1		16.9		15.0					
Green Ext Time (p_c), s	0.1	8.4	0.3		11.3		0.0					

Intersection Summary










HCM 6th Ctrl Delay	13.7
HCM 6th LOS	B




Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Lanes, Volumes, Timings
200: Highfield Road & Broken Hill Circle S

















Existing
PM Peak Hour

						
Lane Group	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Volume (vph)	10	10	30	5	10	30
Future Volume (vph)	10	10	30	5	10	30
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	12	12	12	12	12
Grade (%)	0%			0%	0%	
Storage Length (ft)		0	0		0	0
Storage Lanes		0	0		1	0
Taper Length (ft)			100		100	
Link Speed (mph)	25			25	25	
Link Distance (ft)	314			325	406	
Travel Time (s)	8.6			8.9	11.1	
Confl. Peds. (#/hr)						
Confl. Bikes (#/hr)						
Peak Hour Factor	0.88	0.88	0.88	0.88	0.88	0.88
Growth Factor	100%	100%	100%	100%	100%	100%
Heavy Vehicles (%)	1%	1%	9%	9%	2%	2%
Bus Blockages (#/hr)	0	0	0	0	0	0
Parking (#/hr)						
Mid-Block Traffic (%)	0%			0%	0%	
Shared Lane Traffic (%)						
Lane Group Flow (vph)	22	0	0	40	45	0
Sign Control	Free			Free	Stop	
Intersection Summary						
Area Type:	Other					
Control Type:	Unsignalized					

Intersection						
Int Delay, s/veh	6					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	10	10	30	5	10	30
Future Vol, veh/h	10	10	30	5	10	30
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	88	88	88	88	88	88
Heavy Vehicles, %	1	1	9	9	2	2
Mvmt Flow	11	11	34	6	11	34
Major/Minor	Major1		Major2		Minor1	
Conflicting Flow All	0	0	22	0	91	17
Stage 1	-	-	-	-	17	-
Stage 2	-	-	-	-	74	-
Critical Hdwy	-	-	4.19	-	6.42	6.22
Critical Hdwy Stg 1	-	-	-	-	5.42	-
Critical Hdwy Stg 2	-	-	-	-	5.42	-
Follow-up Hdwy	-	-	2.281	-	3.518	3.318
Pot Cap-1 Maneuver	-	-	1549	-	909	1062
Stage 1	-	-	-	-	1006	-
Stage 2	-	-	-	-	949	-
Platoon blocked, %	-	-		-		
Mov Cap-1 Maneuver	-	-	1549	-	889	1062
Mov Cap-2 Maneuver	-	-	-	-	889	-
Stage 1	-	-	-	-	1006	-
Stage 2	-	-	-	-	928	-
Approach	EB		WB		NB	
HCM Control Delay, s	0		6.3		8.7	
HCM LOS	A					
Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT	
Capacity (veh/h)	1013	-	-	1549	-	
HCM Lane V/C Ratio	0.045	-	-	0.022	-	
HCM Control Delay (s)	8.7	-	-	7.4	0	
HCM Lane LOS	A	-	-	A	A	
HCM 95th %tile Q(veh)	0.1	-	-	0.1	-	

Lanes, Volumes, Timings
300: Highfield Road & Highfield Ct./Auburn Ct.






















Existing
PM Peak Hour

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	5	1	35	1	1	1	15	35	1	1	35	5
Future Volume (vph)	5	1	35	1	1	1	15	35	1	1	35	5
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	12	12	12	12	12	12	12	12	12	12	12
Grade (%)		0%			0%			0%			0%	
Storage Length (ft)	0		0	0		0	0		0	0		0
Storage Lanes	0		0	0		0	0		0	0		0
Taper Length (ft)	100			100			100			100		
Link Speed (mph)		25			25			25			25	
Link Distance (ft)		327			194			539			406	
Travel Time (s)		8.9			5.3			14.7			11.1	
Confl. Peds. (#/hr)												
Confl. Bikes (#/hr)												
Peak Hour Factor	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86
Growth Factor	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%
Heavy Vehicles (%)	1%	1%	1%	1%	1%	1%	2%	2%	2%	7%	7%	7%
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	0	0	0	0
Parking (#/hr)												
Mid-Block Traffic (%)		0%			0%			0%			0%	
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	48	0	0	3	0	0	59	0	0	48	0
Sign Control		Stop			Stop			Free			Free	
Intersection Summary												
Area Type:	Other											
Control Type:	Unsignalized											

Intersection												
Int Delay, s/veh	3.7											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	5	1	35	1	1	1	15	35	1	1	35	5
Future Vol, veh/h	5	1	35	1	1	1	15	35	1	1	35	5
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	86	86	86	86	86	86	86	86	86	86	86	86
Heavy Vehicles, %	1	1	1	1	1	1	2	2	2	7	7	7
Mvmt Flow	6	1	41	1	1	1	17	41	1	1	41	6
Major/Minor	Minor2		Minor1			Major1			Major2			
Conflicting Flow All	123	122	44	143	125	42	47	0	0	42	0	0
Stage 1	46	46	-	76	76	-	-	-	-	-	-	-
Stage 2	77	76	-	67	49	-	-	-	-	-	-	-
Critical Hdwy	7.11	6.51	6.21	7.11	6.51	6.21	4.12	-	-	4.17	-	-
Critical Hdwy Stg 1	6.11	5.51	-	6.11	5.51	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.11	5.51	-	6.11	5.51	-	-	-	-	-	-	-
Follow-up Hdwy	3.509	4.009	3.309	3.509	4.009	3.309	2.218	-	-	2.263	-	-
Pot Cap-1 Maneuver	854	770	1029	829	767	1032	1560	-	-	1535	-	-
Stage 1	970	859	-	936	834	-	-	-	-	-	-	-
Stage 2	934	834	-	946	856	-	-	-	-	-	-	-
Platoon blocked, %								-	-		-	-
Mov Cap-1 Maneuver	845	761	1029	788	758	1032	1560	-	-	1535	-	-
Mov Cap-2 Maneuver	845	761	-	788	758	-	-	-	-	-	-	-
Stage 1	959	858	-	926	825	-	-	-	-	-	-	-
Stage 2	921	825	-	906	855	-	-	-	-	-	-	-
Approach	EB		WB			NB			SB			
HCM Control Delay, s	8.8		9.3			2.2			0.2			
HCM LOS	A		A									
Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR					
Capacity (veh/h)	1560	-	-	994	843	1535	-	-				
HCM Lane V/C Ratio	0.011	-	-	0.048	0.004	0.001	-	-				
HCM Control Delay (s)	7.3	0	-	8.8	9.3	7.3	0	-				
HCM Lane LOS	A	A	-	A	A	A	A	-				
HCM 95th %tile Q(veh)	0	-	-	0.2	0	0	-	-				

Lanes, Volumes, Timings
400: Highfield Road & Capitol Drive

Existing
PM Peak Hour

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	30	1145	25	15	1405	25	15	1	10	30	5	60
Future Volume (vph)	30	1145	25	15	1405	25	15	1	10	30	5	60
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	12	12	12	12	12	12	12	12	12	12	12
Grade (%)		0%			0%			0%			0%	
Storage Length (ft)	355		305	330		290	0		0	0		100
Storage Lanes	1		1	1		1	0		0	0		1
Taper Length (ft)	100			100			100			100		
Link Speed (mph)		55			55			25			25	
Link Distance (ft)		989			984			394			539	
Travel Time (s)		12.3			12.2			10.7			14.7	
Confl. Peds. (#/hr)												
Confl. Bikes (#/hr)												
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Growth Factor	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%
Heavy Vehicles (%)	2%	2%	2%	2%	2%	2%	1%	1%	1%	3%	3%	3%
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	0	0	0	0
Parking (#/hr)												
Mid-Block Traffic (%)		0%			0%			0%			0%	
Shared Lane Traffic (%)												
Lane Group Flow (vph)	31	1193	26	16	1464	26	0	27	0	0	36	63
Sign Control		Free			Free			Stop			Stop	
Intersection Summary												
Area Type:	Other											
Control Type:	Unsignalized											

Intersection												
Int Delay, s/veh	1.5											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↰	↗↗	↰	↰	↗↗	↰		↗↗			↰	↰
Traffic Vol, veh/h	30	1145	25	15	1405	25	15	1	10	30	5	60
Future Vol, veh/h	30	1145	25	15	1405	25	15	1	10	30	5	60
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	355	-	305	330	-	290	-	-	-	-	-	100
Veh in Median Storage, #	-	0	-	-	0	-	-	2	-	-	2	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	96	96	96	96	96	96	96	96	96	96	96	96
Heavy Vehicles, %	2	2	2	2	2	2	1	1	1	3	3	3
Mvmt Flow	31	1193	26	16	1464	26	16	1	10	31	5	63

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	1490	0	0	1219	0	0	2022	2777	597	2155	2777	732
Stage 1	-	-	-	-	-	-	1255	1255	-	1496	1496	-
Stage 2	-	-	-	-	-	-	767	1522	-	659	1281	-
Critical Hdwy	4.14	-	-	4.14	-	-	7.52	6.52	6.92	7.56	6.56	6.96
Critical Hdwy Stg 1	-	-	-	-	-	-	6.52	5.52	-	6.56	5.56	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.52	5.52	-	6.56	5.56	-
Follow-up Hdwy	2.22	-	-	2.22	-	-	3.51	4.01	3.31	3.53	4.03	3.33
Pot Cap-1 Maneuver	447	-	-	568	-	-	34	19	449	~ 27	18	361
Stage 1	-	-	-	-	-	-	183	243	-	127	183	-
Stage 2	-	-	-	-	-	-	363	181	-	417	233	-
Platoon blocked, %		-	-		-	-						
Mov Cap-1 Maneuver	447	-	-	568	-	-	25	17	449	~ 24	16	361
Mov Cap-2 Maneuver	-	-	-	-	-	-	134	115	-	107	124	-
Stage 1	-	-	-	-	-	-	170	226	-	118	178	-
Stage 2	-	-	-	-	-	-	283	176	-	377	217	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	0.3	0.1	28.2	30.6
HCM LOS			D	D

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1	SBLn2
Capacity (veh/h)	182	447	-	-	568	-	-	109	361
HCM Lane V/C Ratio	0.149	0.07	-	-	0.028	-	-	0.334	0.173
HCM Control Delay (s)	28.2	13.7	-	-	11.5	-	-	53.8	17
HCM Lane LOS	D	B	-	-	B	-	-	F	C
HCM 95th %tile Q(veh)	0.5	0.2	-	-	0.1	-	-	1.3	0.6

Notes												
~: Volume exceeds capacity			\$: Delay exceeds 300s			+: Computation Not Defined			*: All major volume in platoon			

APPENDIX C

SYNCHRO TRAFFIC ANALYSIS























Build Traffic Scenario A













Lanes, Volumes, Timings

Build Scenario A

100: Pewaukee Road & Meadow Creek Market/Swan Road

AM Peak Hour

													
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	
Lane Configurations													
Traffic Volume (vph)	55	15	5	25	15	35	10	955	40	50	415	50	
Future Volume (vph)	55	15	5	25	15	35	10	955	40	50	415	50	
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1972	1972	1900	1972	1972	1900	
Lane Width (ft)	12	12	12	12	12	12	12	12	12	12	12	12	
Grade (%)	0%			0%			0%			0%			
Storage Length (ft)	120		0	210		0	435		230	355		315	
Storage Lanes	1		0	1		0	1		1	1		1	
Taper Length (ft)	100			100			100			100			
Right Turn on Red	No			No			No			No			
Link Speed (mph)	35			35			45			45			
Link Distance (ft)	420			477			663			577			
Travel Time (s)	8.2			9.3			10.0			8.7			
Confl. Peds. (#/hr)													
Confl. Bikes (#/hr)													
Peak Hour Factor	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	
Growth Factor	100%	100%	100%	100%	100%	100%	100%	100%	62%	100%	100%	62%	
Heavy Vehicles (%)	10%	10%	10%	9%	9%	9%	6%	6%	6%	11%	11%	11%	
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	0	0	0	0	
Parking (#/hr)													
Mid-Block Traffic (%)	0%			0%			0%			0%			
Shared Lane Traffic (%)													
Lane Group Flow (vph)	60	21	0	27	54	0	11	1049	27	55	456	34	
Turn Type	Perm	NA		Perm	NA		pm+pt	NA	Perm	Perm	NA	Perm	
Protected Phases	4			8			1			2			
Permitted Phases	4			8			6		6	2		2	
Detector Phase	4	4		8	8		1	6	6	2	2	2	
Switch Phase													
Minimum Initial (s)	8.0	8.0		8.0	8.0		8.0	12.0	12.0	12.0	12.0	12.0	
Minimum Split (s)	14.9	14.9		15.3	15.3		13.5	19.3	19.3	19.3	19.3	19.3	
Total Split (s)	19.0	19.0		19.0	19.0		14.0	67.0	67.0	53.0	53.0	53.0	
Total Split (%)	22.1%	22.1%		22.1%	22.1%		16.3%	77.9%	77.9%	61.6%	61.6%	61.6%	
Maximum Green (s)	12.1	12.1		11.7	11.7		8.5	59.7	59.7	45.7	45.7	45.7	
Yellow Time (s)	3.9	3.9		4.3	4.3		3.5	4.3	4.3	4.3	4.3	4.3	
All-Red Time (s)	3.0	3.0		3.0	3.0		2.0	3.0	3.0	3.0	3.0	3.0	
Lost Time Adjust (s)	0.0	0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	0.0	
Total Lost Time (s)	6.9	6.9		7.3	7.3		5.5	7.3	7.3	7.3	7.3	7.3	
Lead/Lag							Lead					Lag	Lag
Lead-Lag Optimize?													
Vehicle Extension (s)	1.5	1.5		1.5	1.5		1.5	3.0	3.0	4.8	4.8	4.8	
Minimum Gap (s)	1.5	1.5		1.5	1.5		1.5	2.0	2.0	3.8	3.8	3.8	
Time Before Reduce (s)	30.0	30.0		30.0	30.0		25.0	19.0	19.0	29.0	29.0	29.0	
Time To Reduce (s)	0.0	0.0		0.0	0.0		0.0	13.0	13.0	8.0	8.0	8.0	
Recall Mode	None	None		None	None		None	C-Max	C-Max	C-Max	C-Max	C-Max	
Walk Time (s)													
Flash Dont Walk (s)													
Pedestrian Calls (#/hr)													
v/c Ratio	0.44	0.12		0.20	0.33		0.01	0.38	0.02	0.15	0.18	0.03	
Control Delay	46.1	35.3		38.1	40.9		3.2	4.7	3.7	8.0	5.5	6.2	

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Queue Delay	0.0	0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	46.1	35.3		38.1	40.9		3.2	4.7	3.7	8.0	5.5	6.2
90th %ile Green (s)	12.1	12.1		11.7	11.7		8.0	59.7	59.7	46.2	46.2	46.2
90th %ile Term Code	Max	Max		Hold	Hold		Min	Coord	Coord	Coord	Coord	Coord
70th %ile Green (s)	9.8	9.8		9.4	9.4		0.0	62.0	62.0	62.0	62.0	62.0
70th %ile Term Code	Gap	Gap		Hold	Hold		Skip	Coord	Coord	Coord	Coord	Coord
50th %ile Green (s)	8.4	8.4		8.0	8.0		0.0	63.4	63.4	63.4	63.4	63.4
50th %ile Term Code	Hold	Hold		Min	Min		Skip	Coord	Coord	Coord	Coord	Coord
30th %ile Green (s)	8.4	8.4		8.0	8.0		0.0	63.4	63.4	63.4	63.4	63.4
30th %ile Term Code	Hold	Hold		Min	Min		Skip	Coord	Coord	Coord	Coord	Coord
10th %ile Green (s)	0.0	0.0		0.0	0.0		0.0	78.7	78.7	78.7	78.7	78.7
10th %ile Term Code	Skip	Skip		Skip	Skip		Skip	Coord	Coord	Coord	Coord	Coord
Queue Length 50th (ft)	31	11		14	28		1	90	3	7	32	4
Queue Length 95th (ft)	67	31		37	61		5	144	11	38	93	21
Internal Link Dist (ft)		340			397			583			497	
Turn Bay Length (ft)	120			210			435		230	355		315
Base Capacity (vph)	175	234		176	211		741	2750	1185	362	2519	1086
Starvation Cap Reductn	0	0		0	0		0	0	0	0	0	0
Spillback Cap Reductn	0	0		0	0		0	0	0	0	0	0
Storage Cap Reductn	0	0		0	0		0	0	0	0	0	0
Reduced v/c Ratio	0.34	0.09		0.15	0.26		0.01	0.38	0.02	0.15	0.18	0.03

Intersection Summary

Area Type: Other

Cycle Length: 86

Actuated Cycle Length: 86

Offset: 4 (5%), Referenced to phase 2:SBTL and 6:NBTL, Start of Yellow

Natural Cycle: 55

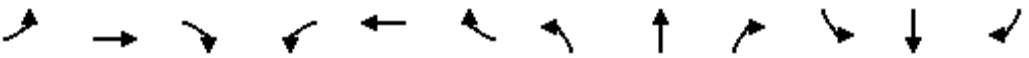










Control Type: Actuated-Coordinated

Splits and Phases: 100: Pewaukee Road & Meadow Creek Market/Swan Road



HCM 6th Signalized Intersection Summary 100: Pewaukee Road & Meadow Creek Market/Swan Road

Build Scenario A
AM Peak Hour

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	55	15	5	25	15	35	10	955	40	50	415	50
Future Volume (veh/h)	55	15	5	25	15	35	10	955	40	50	415	50
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No			No			No		
Adj Sat Flow, veh/h/ln	1752	1752	1752	1767	1767	1767	1880	1880	1811	1803	1803	1737
Adj Flow Rate, veh/h	60	16	5	27	16	38	11	1049	27	55	456	34
Peak Hour Factor	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91
Percent Heavy Veh, %	10	10	10	9	9	9	6	6	6	11	11	11
Cap, veh/h	169	128	40	200	46	110	660	2608	1121	396	2209	949
Arrive On Green	0.10	0.10	0.10	0.10	0.10	0.10	0.02	0.73	0.73	0.64	0.64	0.64
Sat Flow, veh/h	1264	1280	400	1314	465	1103	1790	3571	1535	505	3425	1472
Grp Volume(v), veh/h	60	0	21	27	0	54	11	1049	27	55	456	34
Grp Sat Flow(s),veh/h/ln	1264	0	1680	1314	0	1568	1790	1786	1535	505	1713	1472
Q Serve(g_s), s	4.0	0.0	1.0	1.6	0.0	2.8	0.2	9.6	0.4	4.0	4.7	0.7
Cycle Q Clear(g_c), s	6.8	0.0	1.0	2.6	0.0	2.8	0.2	9.6	0.4	6.3	4.7	0.7
Prop In Lane	1.00		0.24	1.00		0.70	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	169	0	168	200	0	157	660	2608	1121	396	2209	949
V/C Ratio(X)	0.35	0.00	0.13	0.13	0.00	0.34	0.02	0.40	0.02	0.14	0.21	0.04
Avail Cap(c_a), veh/h	221	0	236	247	0	213	798	2608	1121	396	2209	949
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	39.2	0.0	35.3	36.5	0.0	36.1	4.4	4.4	3.2	7.0	6.3	5.6
Incr Delay (d2), s/veh	0.5	0.0	0.1	0.1	0.0	0.5	0.0	0.5	0.0	0.7	0.2	0.1
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	1.2	0.0	0.4	0.5	0.0	1.1	0.0	2.3	0.1	0.4	1.4	0.2
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	39.7	0.0	35.4	36.6	0.0	36.6	4.4	4.9	3.2	7.8	6.5	5.6
LnGrp LOS	D	A	D	D	A	D	A	A	A	A	A	A
Approach Vol, veh/h	81				81				1087			
Approach Delay, s/veh	38.6				36.6				4.8			
Approach LOS	D				D				A			
Timer - Assigned Phs	1	2	4		6		8					
Phs Duration (G+Y+Rc), s	7.3	62.8	15.9		70.1		15.9					
Change Period (Y+Rc), s	5.5	7.3	* 7.3		7.3		7.3					
Max Green Setting (Gmax), s	8.5	45.7	* 12		59.7		11.7					
Max Q Clear Time (g_c+I1), s	2.2	8.3	8.8		11.6		4.8					
Green Ext Time (p_c), s	0.0	7.2	0.0		8.8		0.1					

Intersection Summary

HCM 6th Ctrl Delay	8.3
HCM 6th LOS	A




Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Lanes, Volumes, Timings
200: Highfield Road & Broken Hill Circle S

Build Scenario A
AM Peak Hour

















	→	↘	↙	←	↖	↗
Lane Group	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↰			↱	↘↙	
Traffic Volume (vph)	5	20	35	5	5	20
Future Volume (vph)	5	20	35	5	5	20
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	12	12	12	12	12
Grade (%)	0%			0%	0%	
Storage Length (ft)		0	0		0	0
Storage Lanes		0	0		1	0
Taper Length (ft)			100		100	
Link Speed (mph)	25			25	25	
Link Distance (ft)	314			325	406	
Travel Time (s)	8.6			8.9	11.1	
Confl. Peds. (#/hr)						
Confl. Bikes (#/hr)						
Peak Hour Factor	0.77	0.77	0.77	0.77	0.77	0.77
Growth Factor	100%	100%	100%	100%	100%	100%
Heavy Vehicles (%)	17%	17%	1%	1%	12%	12%
Bus Blockages (#/hr)	0	0	0	0	0	0
Parking (#/hr)						
Mid-Block Traffic (%)	0%			0%	0%	
Shared Lane Traffic (%)						
Lane Group Flow (vph)	32	0	0	51	32	0
Sign Control	Free			Free	Stop	
Intersection Summary						
Area Type:	Other					
Control Type:	Unsignalized					

Intersection						
Int Delay, s/veh	5.3					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	5	20	35	5	5	20
Future Vol, veh/h	5	20	35	5	5	20
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	77	77	77	77	77	77
Heavy Vehicles, %	17	17	1	1	12	12
Mvmt Flow	6	26	45	6	6	26
Major/Minor	Major1		Major2		Minor1	
Conflicting Flow All	0	0	32	0	115	19
Stage 1	-	-	-	-	19	-
Stage 2	-	-	-	-	96	-
Critical Hdwy	-	-	4.11	-	6.52	6.32
Critical Hdwy Stg 1	-	-	-	-	5.52	-
Critical Hdwy Stg 2	-	-	-	-	5.52	-
Follow-up Hdwy	-	-	2.209	-	3.608	3.408
Pot Cap-1 Maneuver	-	-	1587	-	858	1031
Stage 1	-	-	-	-	978	-
Stage 2	-	-	-	-	903	-
Platoon blocked, %	-	-		-		
Mov Cap-1 Maneuver	-	-	1587	-	834	1031
Mov Cap-2 Maneuver	-	-	-	-	834	-
Stage 1	-	-	-	-	978	-
Stage 2	-	-	-	-	878	-
Approach	EB		WB		NB	
HCM Control Delay, s	0		6.4		8.8	
HCM LOS	A					
Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT	
Capacity (veh/h)	984	-	-	1587	-	
HCM Lane V/C Ratio	0.033	-	-	0.029	-	
HCM Control Delay (s)	8.8	-	-	7.3	0	
HCM Lane LOS	A	-	-	A	A	
HCM 95th %tile Q(veh)	0.1	-	-	0.1	-	

Lanes, Volumes, Timings
300: Highfield Road & Highfield Ct./Auburn Ct.

Build Scenario A

























AM Peak Hour










												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	5	1	60	1	1	1	95	20	1	1	50	5
Future Volume (vph)	5	1	60	1	1	1	95	20	1	1	50	5
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	12	12	12	12	12	12	12	12	12	12	12
Grade (%)		0%			0%			0%			0%	
Storage Length (ft)	0		0	0		0	0		0	0		0
Storage Lanes	0		0	0		0	0		0	0		0
Taper Length (ft)	100			100			100			100		
Link Speed (mph)		25			25			25			25	
Link Distance (ft)		327			194			539			406	
Travel Time (s)		8.9			5.3			14.7			11.1	
Confl. Peds. (#/hr)												
Confl. Bikes (#/hr)												
Peak Hour Factor	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86
Growth Factor	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%
Heavy Vehicles (%)	13%	13%	13%	1%	1%	1%	6%	6%	6%	4%	4%	4%
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	0	0	0	0
Parking (#/hr)												
Mid-Block Traffic (%)		0%			0%			0%			0%	
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	77	0	0	3	0	0	134	0	0	65	0
Sign Control		Stop			Stop			Free			Free	
Intersection Summary												
Area Type:	Other											
Control Type:	Unsignalized											

Intersection												
Int Delay, s/veh	5.7											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	5	1	60	1	1	1	95	20	1	1	50	5
Future Vol, veh/h	5	1	60	1	1	1	95	20	1	1	50	5
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	86	86	86	86	86	86	86	86	86	86	86	86
Heavy Vehicles, %	13	13	13	1	1	1	6	6	6	4	4	4
Mvmt Flow	6	1	70	1	1	1	110	23	1	1	58	6
Major/Minor	Minor2		Minor1		Major1				Major2			
Conflicting Flow All	308	307	61	343	310	24	64	0	0	24	0	0
Stage 1	63	63	-	244	244	-	-	-	-	-	-	-
Stage 2	245	244	-	99	66	-	-	-	-	-	-	-
Critical Hdwy	7.23	6.63	6.33	7.11	6.51	6.21	4.16	-	-	4.14	-	-
Critical Hdwy Stg 1	6.23	5.63	-	6.11	5.51	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.23	5.63	-	6.11	5.51	-	-	-	-	-	-	-
Follow-up Hdwy	3.617	4.117	3.417	3.509	4.009	3.309	2.254	-	-	2.236	-	-
Pot Cap-1 Maneuver	623	589	974	613	606	1055	1513	-	-	1578	-	-
Stage 1	921	821	-	762	706	-	-	-	-	-	-	-
Stage 2	735	684	-	910	842	-	-	-	-	-	-	-
Platoon blocked, %								-	-		-	-
Mov Cap-1 Maneuver	586	545	974	536	561	1055	1513	-	-	1578	-	-
Mov Cap-2 Maneuver	586	545	-	536	561	-	-	-	-	-	-	-
Stage 1	853	820	-	706	654	-	-	-	-	-	-	-
Stage 2	679	633	-	843	841	-	-	-	-	-	-	-
Approach	EB		WB		NB				SB			
HCM Control Delay, s	9.3		10.5		6.2				0.1			
HCM LOS	A		B									
Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR					
Capacity (veh/h)	1513	-	-	917	653	1578	-	-				
HCM Lane V/C Ratio	0.073	-	-	0.084	0.005	0.001	-	-				
HCM Control Delay (s)	7.6	0	-	9.3	10.5	7.3	0	-				
HCM Lane LOS	A	A	-	A	B	A	A	-				
HCM 95th %tile Q(veh)	0.2	-	-	0.3	0	0	-	-				

Lanes, Volumes, Timings
400: Highfield Road & Capitol Drive

Build Scenario A
AM Peak Hour

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		 			 			 				
Traffic Volume (vph)	85	1095	10	5	970	45	15	1	20	35	1	75
Future Volume (vph)	85	1095	10	5	970	45	15	1	20	35	1	75
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	12	12	12	12	12	12	12	12	12	12	12
Grade (%)		0%			0%			0%			0%	
Storage Length (ft)	355		305	330		290	0		0	0		100
Storage Lanes	1		1	1		1	0		0	0		1
Taper Length (ft)	100			100			100			100		
Link Speed (mph)		55			55			25			25	
Link Distance (ft)		989			984			394			539	
Travel Time (s)		12.3			12.2			10.7			14.7	
Confl. Peds. (#/hr)												
Confl. Bikes (#/hr)												
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Growth Factor	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%
Heavy Vehicles (%)	5%	5%	5%	5%	5%	5%	5%	5%	5%	2%	2%	2%
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	0	0	0	0
Parking (#/hr)												
Mid-Block Traffic (%)		0%			0%			0%			0%	
Shared Lane Traffic (%)												
Lane Group Flow (vph)	92	1190	11	5	1054	49	0	39	0	0	39	82
Sign Control		Free			Free			Stop			Stop	
Intersection Summary												
Area Type:	Other											
Control Type:	Unsignalized											























Intersection												
Int Delay, s/veh	1.9											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	85	1095	10	5	970	45	15	1	20	35	1	75
Future Vol, veh/h	85	1095	10	5	970	45	15	1	20	35	1	75
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	355	-	305	330	-	290	-	-	-	-	-	100
Veh in Median Storage, #	-	0	-	-	0	-	-	2	-	-	2	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	5	5	5	5	5	5	5	5	5	2	2	2
Mvmt Flow	92	1190	11	5	1054	49	16	1	22	38	1	82
Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	1103	0	0	1201	0	0	1912	2487	595	1844	2449	527
Stage 1	-	-	-	-	-	-	1374	1374	-	1064	1064	-
Stage 2	-	-	-	-	-	-	538	1113	-	780	1385	-
Critical Hdwy	4.2	-	-	4.2	-	-	7.6	6.6	7	7.54	6.54	6.94
Critical Hdwy Stg 1	-	-	-	-	-	-	6.6	5.6	-	6.54	5.54	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.6	5.6	-	6.54	5.54	-
Follow-up Hdwy	2.25	-	-	2.25	-	-	3.55	4.05	3.35	3.52	4.02	3.32
Pot Cap-1 Maneuver	611	-	-	560	-	-	40	28	440	46	31	496
Stage 1	-	-	-	-	-	-	149	206	-	238	298	-
Stage 2	-	-	-	-	-	-	487	276	-	354	209	-
Platoon blocked, %		-	-		-	-						
Mov Cap-1 Maneuver	611	-	-	560	-	-	29	24	440	~ 38	26	496
Mov Cap-2 Maneuver	-	-	-	-	-	-	113	120	-	158	141	-
Stage 1	-	-	-	-	-	-	127	175	-	202	295	-
Stage 2	-	-	-	-	-	-	402	274	-	284	177	-
Approach	EB			WB			NB			SB		
HCM Control Delay, s	0.9			0.1			28.3			20.7		
HCM LOS							D			C		
Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1	SBLn2			
Capacity (veh/h)	193	611	-	-	560	-	-	157	496			
HCM Lane V/C Ratio	0.203	0.151	-	-	0.01	-	-	0.249	0.164			
HCM Control Delay (s)	28.3	11.9	-	-	11.5	-	-	35.4	13.7			
HCM Lane LOS	D	B	-	-	B	-	-	E	B			
HCM 95th %tile Q(veh)	0.7	0.5	-	-	0	-	-	0.9	0.6			
Notes												
~: Volume exceeds capacity		\$: Delay exceeds 300s			+: Computation Not Defined				*: All major volume in platoon			


Lanes, Volumes, Timings

100: Pewaukee Road & Meadow Creek Market/Swan Road

Build Scenario A

PM Peak Hour

														
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR		
Lane Configurations														
Traffic Volume (vph)	105	45	110	85	20	5	120	1240	100	5	785	85		
Future Volume (vph)	105	45	110	85	20	5	120	1240	100	5	785	85		
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1972	1972	1900	1972	1972	1900		
Lane Width (ft)	12	12	12	12	12	12	12	12	12	12	12	12		
Grade (%)	0%			0%			0%			0%				
Storage Length (ft)	120		0	210		0	435		230	355		315		
Storage Lanes	1		0	1		0	1		1	1		1		
Taper Length (ft)	100			100			100			100				
Right Turn on Red	No			No			No			No				
Link Speed (mph)	35			35			45			45				
Link Distance (ft)	420			477			663			577				
Travel Time (s)	8.2			9.3			10.0			8.7				
Confl. Peds. (#/hr)														
Confl. Bikes (#/hr)														
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96		
Growth Factor	100%	100%	100%	100%	100%	100%	100%	100%	62%	100%	100%	62%		
Heavy Vehicles (%)	1%	1%	1%	4%	4%	4%	1%	1%	1%	3%	3%	3%		
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	0	0	0	0		
Parking (#/hr)														
Mid-Block Traffic (%)	0%			0%			0%			0%				
Shared Lane Traffic (%)														
Lane Group Flow (vph)	109	162	0	89	26	0	125	1292	65	5	818	55		
Turn Type	Perm	NA		Perm	NA		pm+pt	NA	Perm	Perm	NA	Perm		
Protected Phases	4			8			1			2				
Permitted Phases	4			8			6		6	2		2		
Detector Phase	4	4		8	8		1	6	6	2	2	2		
Switch Phase														
Minimum Initial (s)	8.0	8.0		8.0	8.0		8.0	12.0	12.0	12.0	12.0	12.0		
Minimum Split (s)	14.9	14.9		15.3	15.3		13.5	19.3	19.3	19.3	19.3	19.3		
Total Split (s)	21.0	21.0		21.0	21.0		20.0	59.0	59.0	39.0	39.0	39.0		
Total Split (%)	26.3%	26.3%		26.3%	26.3%		25.0%	73.8%	73.8%	48.8%	48.8%	48.8%		
Maximum Green (s)	14.1	14.1		13.7	13.7		14.5	51.7	51.7	31.7	31.7	31.7		
Yellow Time (s)	3.9	3.9		4.3	4.3		3.5	4.3	4.3	4.3	4.3	4.3		
All-Red Time (s)	3.0	3.0		3.0	3.0		2.0	3.0	3.0	3.0	3.0	3.0		
Lost Time Adjust (s)	0.0	0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	0.0		
Total Lost Time (s)	6.9	6.9		7.3	7.3		5.5	7.3	7.3	7.3	7.3	7.3		
Lead/Lag							Lead					Lag	Lag	Lag
Lead-Lag Optimize?														
Vehicle Extension (s)	1.5	1.5		1.5	1.5		1.5	3.0	3.0	4.8	4.8	4.8		
Minimum Gap (s)	1.5	1.5		1.5	1.5		1.5	2.0	2.0	3.8	3.8	3.8		
Time Before Reduce (s)	30.0	30.0		30.0	30.0		25.0	19.0	19.0	29.0	29.0	29.0		
Time To Reduce (s)	0.0	0.0		0.0	0.0		0.0	13.0	13.0	8.0	8.0	8.0		
Recall Mode	None	None		None	None		None	C-Max	C-Max	C-Max	C-Max	C-Max		
Walk Time (s)														
Flash Dont Walk (s)														
Pedestrian Calls (#/hr)														
v/c Ratio	0.55	0.68		0.54	0.11		0.25	0.51	0.06	0.02	0.41	0.06		
Control Delay	42.2	46.7		44.2	29.9		5.5	7.5	5.0	11.4	12.7	11.1		

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Queue Delay	0.0	0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	42.2	46.7		44.2	29.9		5.5	7.5	5.0	11.4	12.7	11.1
90th %ile Green (s)	14.1	14.1		13.7	13.7		8.0	51.7	51.7	38.2	38.2	38.2
90th %ile Term Code	Max	Max		Max	Max		Min	Coord	Coord	Coord	Coord	Coord
70th %ile Green (s)	13.6	13.6		13.2	13.2		8.0	52.2	52.2	38.7	38.7	38.7
70th %ile Term Code	Gap	Gap		Hold	Hold		Min	Coord	Coord	Coord	Coord	Coord
50th %ile Green (s)	11.5	11.5		11.1	11.1		8.0	54.3	54.3	40.8	40.8	40.8
50th %ile Term Code	Gap	Gap		Hold	Hold		Min	Coord	Coord	Coord	Coord	Coord
30th %ile Green (s)	9.4	9.4		9.0	9.0		8.0	56.4	56.4	42.9	42.9	42.9
30th %ile Term Code	Gap	Gap		Hold	Hold		Min	Coord	Coord	Coord	Coord	Coord
10th %ile Green (s)	8.4	8.4		8.0	8.0		0.0	57.4	57.4	57.4	57.4	57.4
10th %ile Term Code	Hold	Hold		Hold	Hold		Skip	Coord	Coord	Coord	Coord	Coord
Queue Length 50th (ft)	51	78		42	12		17	142	9	1	127	13
Queue Length 95th (ft)	98	135		85	32		36	210	24	7	185	33
Internal Link Dist (ft)		340			397			583			497	
Turn Bay Length (ft)	120			210			435		230	355		315
Base Capacity (vph)	245	296		204	303		613	2522	1087	223	1982	854
Starvation Cap Reductn	0	0		0	0		0	0	0	0	0	0
Spillback Cap Reductn	0	0		0	0		0	0	0	0	0	0
Storage Cap Reductn	0	0		0	0		0	0	0	0	0	0
Reduced v/c Ratio	0.44	0.55		0.44	0.09		0.20	0.51	0.06	0.02	0.41	0.06

Intersection Summary

Area Type: Other

Cycle Length: 80

Actuated Cycle Length: 80

Offset: 70 (88%), Referenced to phase 2:SBTL and 6:NBTL, Start of Yellow

Natural Cycle: 55























Control Type: Actuated-Coordinated

Splits and Phases: 100: Pewaukee Road & Meadow Creek Market/Swan Road



HCM 6th Signalized Intersection Summary 100: Pewaukee Road & Meadow Creek Market/Swan Road

Build Scenario A
PM Peak Hour

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	105	45	110	85	20	5	120	1240	100	5	785	85
Future Volume (veh/h)	105	45	110	85	20	5	120	1240	100	5	785	85
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No			No			No		
Adj Sat Flow, veh/h/ln	1885	1885	1885	1841	1841	1841	1957	1957	1885	1926	1926	1856
Adj Flow Rate, veh/h	109	47	115	89	21	5	125	1292	65	5	818	55
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Percent Heavy Veh, %	1	1	1	4	4	4	1	1	1	3	3	3
Cap, veh/h	312	83	203	189	246	59	487	2402	1032	279	1770	761
Arrive On Green	0.17	0.17	0.17	0.17	0.17	0.17	0.09	0.65	0.65	0.48	0.48	0.48
Sat Flow, veh/h	1396	485	1187	1205	1437	342	1863	3718	1598	413	3659	1572
Grp Volume(v), veh/h	109	0	162	89	0	26	125	1292	65	5	818	55
Grp Sat Flow(s),veh/h/ln	1396	0	1672	1205	0	1779	1863	1859	1598	413	1830	1572
Q Serve(g_s), s	5.7	0.0	7.1	5.9	0.0	1.0	2.3	15.1	1.2	0.5	11.9	1.5
Cycle Q Clear(g_c), s	6.7	0.0	7.1	13.0	0.0	1.0	2.3	15.1	1.2	2.6	11.9	1.5
Prop In Lane	1.00		0.71	1.00		0.19	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	312	0	286	189	0	305	487	2402	1032	279	1770	761
V/C Ratio(X)	0.35	0.00	0.57	0.47	0.00	0.09	0.26	0.54	0.06	0.02	0.46	0.07
Avail Cap(c_a), veh/h	319	0	295	189	0	305	650	2402	1032	279	1770	761
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	30.7	0.0	30.4	36.4	0.0	27.9	8.3	7.7	5.2	11.9	13.7	11.0
Incr Delay (d2), s/veh	0.2	0.0	1.4	0.7	0.0	0.0	0.1	0.9	0.1	0.1	0.9	0.2
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	1.9	0.0	2.9	1.7	0.0	0.4	0.7	4.5	0.3	0.1	4.4	0.5
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	30.9	0.0	31.8	37.0	0.0	27.9	8.4	8.5	5.3	12.0	14.6	11.2
LnGrp LOS	C	A	C	D	A	C	A	A	A	B	B	B
Approach Vol, veh/h	271			115			1482			878		
Approach Delay, s/veh	31.5			35.0			8.4			14.4		
Approach LOS	C			C			A			B		
Timer - Assigned Phs	1	2	4		6		8					
Phs Duration (G+Y+Rc), s	13.0	46.0	21.0		59.0		21.0					
Change Period (Y+Rc), s	5.5	7.3	* 7.3		7.3		7.3					
Max Green Setting (Gmax), s	14.5	31.7	* 14		51.7		13.7					
Max Q Clear Time (g_c+I1), s	4.3	13.9	9.1		17.1		15.0					
Green Ext Time (p_c), s	0.1	8.4	0.3		11.4		0.0					

Intersection Summary










HCM 6th Ctrl Delay	13.7
HCM 6th LOS	B




Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Lanes, Volumes, Timings
200: Highfield Road & Broken Hill Circle S

Build Scenario A
PM Peak Hour

						
Lane Group	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Volume (vph)	10	10	30	5	10	30
Future Volume (vph)	10	10	30	5	10	30
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	12	12	12	12	12
Grade (%)	0%			0%	0%	
Storage Length (ft)		0	0		0	0
Storage Lanes		0	0		1	0
Taper Length (ft)			100		100	
Link Speed (mph)	25			25	25	
Link Distance (ft)	314			325	406	
Travel Time (s)	8.6			8.9	11.1	
Confl. Peds. (#/hr)						
Confl. Bikes (#/hr)						
Peak Hour Factor	0.88	0.88	0.88	0.88	0.88	0.88
Growth Factor	100%	100%	100%	100%	100%	100%
Heavy Vehicles (%)	1%	1%	9%	9%	2%	2%
Bus Blockages (#/hr)	0	0	0	0	0	0
Parking (#/hr)						
Mid-Block Traffic (%)	0%			0%	0%	
Shared Lane Traffic (%)						
Lane Group Flow (vph)	22	0	0	40	45	0
Sign Control	Free			Free	Stop	
Intersection Summary						
Area Type:	Other					
Control Type:	Unsignalized					

Intersection						
Int Delay, s/veh	6					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	10	10	30	5	10	30
Future Vol, veh/h	10	10	30	5	10	30
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	88	88	88	88	88	88
Heavy Vehicles, %	1	1	9	9	2	2
Mvmt Flow	11	11	34	6	11	34

















Major/Minor	Major1	Major2	Minor1			
Conflicting Flow All	0	0	22	0	91	17
Stage 1	-	-	-	-	17	-
Stage 2	-	-	-	-	74	-
Critical Hdwy	-	-	4.19	-	6.42	6.22
Critical Hdwy Stg 1	-	-	-	-	5.42	-
Critical Hdwy Stg 2	-	-	-	-	5.42	-
Follow-up Hdwy	-	-	2.281	-	3.518	3.318
Pot Cap-1 Maneuver	-	-	1549	-	909	1062
Stage 1	-	-	-	-	1006	-
Stage 2	-	-	-	-	949	-
Platoon blocked, %	-	-	-	-	-	-
Mov Cap-1 Maneuver	-	-	1549	-	889	1062
Mov Cap-2 Maneuver	-	-	-	-	889	-
Stage 1	-	-	-	-	1006	-
Stage 2	-	-	-	-	928	-

Approach	EB	WB	NB
HCM Control Delay, s	0	6.3	8.7
HCM LOS			A

Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT
Capacity (veh/h)	1013	-	-	1549	-
HCM Lane V/C Ratio	0.045	-	-	0.022	-
HCM Control Delay (s)	8.7	-	-	7.4	0
HCM Lane LOS	A	-	-	A	A
HCM 95th %tile Q(veh)	0.1	-	-	0.1	-

Lanes, Volumes, Timings
300: Highfield Road & Highfield Ct./Auburn Ct.






















Build Scenario A
PM Peak Hour










												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	5	1	90	1	1	1	65	35	1	1	35	5
Future Volume (vph)	5	1	90	1	1	1	65	35	1	1	35	5
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	12	12	12	12	12	12	12	12	12	12	12
Grade (%)		0%			0%			0%			0%	
Storage Length (ft)	0		0	0		0	0		0	0		0
Storage Lanes	0		0	0		0	0		0	0		0
Taper Length (ft)	100			100			100			100		
Link Speed (mph)		25			25			25			25	
Link Distance (ft)		327			194			539			406	
Travel Time (s)		8.9			5.3			14.7			11.1	
Confl. Peds. (#/hr)												
Confl. Bikes (#/hr)												
Peak Hour Factor	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86
Growth Factor	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%
Heavy Vehicles (%)	1%	1%	1%	1%	1%	1%	2%	2%	2%	7%	7%	7%
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	0	0	0	0
Parking (#/hr)												
Mid-Block Traffic (%)		0%			0%			0%			0%	
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	112	0	0	3	0	0	118	0	0	48	0
Sign Control		Stop			Stop			Free			Free	
Intersection Summary												
Area Type:	Other											
Control Type:	Unsignalized											

Intersection												
Int Delay, s/veh	5.8											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	5	1	90	1	1	1	65	35	1	1	35	5
Future Vol, veh/h	5	1	90	1	1	1	65	35	1	1	35	5
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	86	86	86	86	86	86	86	86	86	86	86	86
Heavy Vehicles, %	1	1	1	1	1	1	2	2	2	7	7	7
Mvmt Flow	6	1	105	1	1	1	76	41	1	1	41	6
Major/Minor	Minor2		Minor1		Major1		Major2					
Conflicting Flow All	241	240	44	293	243	42	47	0	0	42	0	0
Stage 1	46	46	-	194	194	-	-	-	-	-	-	-
Stage 2	195	194	-	99	49	-	-	-	-	-	-	-
Critical Hdwy	7.11	6.51	6.21	7.11	6.51	6.21	4.12	-	-	4.17	-	-
Critical Hdwy Stg 1	6.11	5.51	-	6.11	5.51	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.11	5.51	-	6.11	5.51	-	-	-	-	-	-	-
Follow-up Hdwy	3.509	4.009	3.309	3.509	4.009	3.309	2.218	-	-	2.263	-	-
Pot Cap-1 Maneuver	715	663	1029	661	660	1032	1560	-	-	1535	-	-
Stage 1	970	859	-	810	742	-	-	-	-	-	-	-
Stage 2	809	742	-	910	856	-	-	-	-	-	-	-
Platoon blocked, %								-	-	-	-	-
Mov Cap-1 Maneuver	686	629	1029	570	626	1032	1560	-	-	1535	-	-
Mov Cap-2 Maneuver	686	629	-	570	626	-	-	-	-	-	-	-
Stage 1	922	858	-	770	705	-	-	-	-	-	-	-
Stage 2	766	705	-	816	855	-	-	-	-	-	-	-
Approach	EB		WB		NB		SB					
HCM Control Delay, s	9.1		10.2		4.8		0.2					
HCM LOS	A		B									
Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1WBLn1	SBL	SBT	SBR					
Capacity (veh/h)	1560	-	-	996	694	1535	-	-				
HCM Lane V/C Ratio	0.048	-	-	0.112	0.005	0.001	-	-				
HCM Control Delay (s)	7.4	0	-	9.1	10.2	7.3	0	-				
HCM Lane LOS	A	A	-	A	B	A	A	-				
HCM 95th %tile Q(veh)	0.2	-	-	0.4	0	0	-	-				

Lanes, Volumes, Timings
400: Highfield Road & Capitol Drive

Build Scenario A
PM Peak Hour

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	70	1145	25	15	1405	35	15	1	10	45	5	100
Future Volume (vph)	70	1145	25	15	1405	35	15	1	10	45	5	100
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	12	12	12	12	12	12	12	12	12	12	12
Grade (%)		0%			0%			0%			0%	
Storage Length (ft)	355		305	330		290	0		0	0		100
Storage Lanes	1		1	1		1	0		0	0		1
Taper Length (ft)	100			100			100			100		
Link Speed (mph)		55			55			25			25	
Link Distance (ft)		989			984			394			539	
Travel Time (s)		12.3			12.2			10.7			14.7	
Confl. Peds. (#/hr)												
Confl. Bikes (#/hr)												
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Growth Factor	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%
Heavy Vehicles (%)	2%	2%	2%	2%	2%	2%	1%	1%	1%	3%	3%	3%
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	0	0	0	0
Parking (#/hr)												
Mid-Block Traffic (%)		0%			0%			0%			0%	
Shared Lane Traffic (%)												
Lane Group Flow (vph)	73	1193	26	16	1464	36	0	27	0	0	52	104
Sign Control		Free			Free			Stop			Stop	
Intersection Summary												
Area Type:	Other											
Control Type:	Unsignalized											

Intersection												
Int Delay, s/veh	2.8											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	70	1145	25	15	1405	35	15	1	10	45	5	100
Future Vol, veh/h	70	1145	25	15	1405	35	15	1	10	45	5	100
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	355	-	305	330	-	290	-	-	-	-	-	100
Veh in Median Storage, #	-	0	-	-	0	-	-	2	-	-	2	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	96	96	96	96	96	96	96	96	96	96	96	96
Heavy Vehicles, %	2	2	2	2	2	2	1	1	1	3	3	3
Mvmt Flow	73	1193	26	16	1464	36	16	1	10	47	5	104

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	1500	0	0	1219	0	0	2106	2871	597	2239	2861	732
Stage 1	-	-	-	-	-	-	1339	1339	-	1496	1496	-
Stage 2	-	-	-	-	-	-	767	1532	-	743	1365	-
Critical Hdwy	4.14	-	-	4.14	-	-	7.52	6.52	6.92	7.56	6.56	6.96
Critical Hdwy Stg 1	-	-	-	-	-	-	6.52	5.52	-	6.56	5.56	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.52	5.52	-	6.56	5.56	-
Follow-up Hdwy	2.22	-	-	2.22	-	-	3.51	4.01	3.31	3.53	4.03	3.33
Pot Cap-1 Maneuver	443	-	-	568	-	-	30	17	449	~ 23	16	361
Stage 1	-	-	-	-	-	-	163	222	-	127	183	-
Stage 2	-	-	-	-	-	-	363	179	-	371	212	-
Platoon blocked, %		-	-		-	-						
Mov Cap-1 Maneuver	443	-	-	568	-	-	18	14	449	~ 19	13	361
Mov Cap-2 Maneuver	-	-	-	-	-	-	101	84	-	94	111	-
Stage 1	-	-	-	-	-	-	136	185	-	106	178	-
Stage 2	-	-	-	-	-	-	244	174	-	301	177	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	0.8	0.1	36.2	39.8
HCM LOS			E	E

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1	SBLn2
Capacity (veh/h)	142	443	-	-	568	-	-	95	361
HCM Lane V/C Ratio	0.191	0.165	-	-	0.028	-	-	0.548	0.289
HCM Control Delay (s)	36.2	14.7	-	-	11.5	-	-	81.5	19
HCM Lane LOS	E	B	-	-	B	-	-	F	C
HCM 95th %tile Q(veh)	0.7	0.6	-	-	0.1	-	-	2.5	1.2

Notes			
~: Volume exceeds capacity	\$: Delay exceeds 300s	+: Computation Not Defined	*: All major volume in platoon

APPENDIX D

SYNCHRO TRAFFIC ANALYSIS























Build Traffic Scenario B













Lanes, Volumes, Timings

Build Scenario B

100: Pewaukee Road & Meadow Creek Market/Swan Road

AM Peak Hour

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	55	15	5	30	15	40	10	950	40	60	405	50
Future Volume (vph)	55	15	5	30	15	40	10	950	40	60	405	50
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1972	1972	1900	1972	1972	1900
Lane Width (ft)	12	12	12	12	12	12	12	12	12	12	12	12
Grade (%)	0%			0%			0%			0%		
Storage Length (ft)	120		0	210		0	435		230	355		315
Storage Lanes	1		0	1		0	1		1	1		1
Taper Length (ft)	100			100			100			100		
Right Turn on Red			No			No			No			No
Link Speed (mph)	35			35			45			45		
Link Distance (ft)	420			477			663			577		
Travel Time (s)	8.2			9.3			10.0			8.7		
Confl. Peds. (#/hr)												
Confl. Bikes (#/hr)												
Peak Hour Factor	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91
Growth Factor	100%	100%	100%	100%	100%	100%	100%	100%	62%	100%	100%	62%
Heavy Vehicles (%)	10%	10%	10%	9%	9%	9%	6%	6%	6%	11%	11%	11%
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	0	0	0	0
Parking (#/hr)												
Mid-Block Traffic (%)	0%			0%			0%			0%		
Shared Lane Traffic (%)												
Lane Group Flow (vph)	60	21	0	33	60	0	11	1044	27	66	445	34
Turn Type	Perm	NA		Perm	NA		pm+pt	NA	Perm	Perm	NA	Perm
Protected Phases	4			8			1	6			2	
Permitted Phases	4			8			6		6	2		2
Detector Phase	4	4		8	8		1	6	6	2	2	2
Switch Phase												
Minimum Initial (s)	8.0	8.0		8.0	8.0		8.0	12.0	12.0	12.0	12.0	12.0
Minimum Split (s)	14.9	14.9		15.3	15.3		13.5	19.3	19.3	19.3	19.3	19.3
Total Split (s)	19.0	19.0		19.0	19.0		14.0	67.0	67.0	53.0	53.0	53.0
Total Split (%)	22.1%	22.1%		22.1%	22.1%		16.3%	77.9%	77.9%	61.6%	61.6%	61.6%
Maximum Green (s)	12.1	12.1		11.7	11.7		8.5	59.7	59.7	45.7	45.7	45.7
Yellow Time (s)	3.9	3.9		4.3	4.3		3.5	4.3	4.3	4.3	4.3	4.3
All-Red Time (s)	3.0	3.0		3.0	3.0		2.0	3.0	3.0	3.0	3.0	3.0
Lost Time Adjust (s)	0.0	0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	6.9	6.9		7.3	7.3		5.5	7.3	7.3	7.3	7.3	7.3
Lead/Lag							Lead			Lag		Lag
Lead-Lag Optimize?												
Vehicle Extension (s)	1.5	1.5		1.5	1.5		1.5	3.0	3.0	4.8	4.8	4.8
Minimum Gap (s)	1.5	1.5		1.5	1.5		1.5	2.0	2.0	3.8	3.8	3.8
Time Before Reduce (s)	30.0	30.0		30.0	30.0		25.0	19.0	19.0	29.0	29.0	29.0
Time To Reduce (s)	0.0	0.0		0.0	0.0		0.0	13.0	13.0	8.0	8.0	8.0
Recall Mode	None	None		None	None		None	C-Max	C-Max	C-Max	C-Max	C-Max
Walk Time (s)												
Flash Dont Walk (s)												
Pedestrian Calls (#/hr)												
v/c Ratio	0.45	0.12		0.24	0.37		0.01	0.38	0.02	0.18	0.18	0.03
Control Delay	46.3	35.3		39.3	42.1		3.2	4.7	3.7	8.4	5.5	6.2

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Queue Delay	0.0	0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	46.3	35.3		39.3	42.1		3.2	4.7	3.7	8.4	5.5	6.2
90th %ile Green (s)	12.1	12.1		11.7	11.7		8.0	59.7	59.7	46.2	46.2	46.2
90th %ile Term Code	Max	Max		Hold	Hold		Min	Coord	Coord	Coord	Coord	Coord
70th %ile Green (s)	9.8	9.8		9.4	9.4		0.0	62.0	62.0	62.0	62.0	62.0
70th %ile Term Code	Gap	Gap		Hold	Hold		Skip	Coord	Coord	Coord	Coord	Coord
50th %ile Green (s)	8.4	8.4		8.0	8.0		0.0	63.4	63.4	63.4	63.4	63.4
50th %ile Term Code	Hold	Hold		Min	Min		Skip	Coord	Coord	Coord	Coord	Coord
30th %ile Green (s)	8.4	8.4		8.0	8.0		0.0	63.4	63.4	63.4	63.4	63.4
30th %ile Term Code	Hold	Hold		Min	Min		Skip	Coord	Coord	Coord	Coord	Coord
10th %ile Green (s)	0.0	0.0		0.0	0.0		0.0	78.7	78.7	78.7	78.7	78.7
10th %ile Term Code	Skip	Skip		Skip	Skip		Skip	Coord	Coord	Coord	Coord	Coord
Queue Length 50th (ft)	31	11		17	31		1	90	3	9	31	4
Queue Length 95th (ft)	67	31		43	66		5	143	11	45	90	21
Internal Link Dist (ft)		340			397			583			497	
Turn Bay Length (ft)	120			210			435		230	355		315
Base Capacity (vph)	174	234		176	211		747	2750	1185	363	2519	1086
Starvation Cap Reductn	0	0		0	0		0	0	0	0	0	0
Spillback Cap Reductn	0	0		0	0		0	0	0	0	0	0
Storage Cap Reductn	0	0		0	0		0	0	0	0	0	0
Reduced v/c Ratio	0.34	0.09		0.19	0.28		0.01	0.38	0.02	0.18	0.18	0.03

Intersection Summary

Area Type: Other

Cycle Length: 86

Actuated Cycle Length: 86

Offset: 4 (5%), Referenced to phase 2:SBTL and 6:NBTL, Start of Yellow

Natural Cycle: 55























Control Type: Actuated-Coordinated

Splits and Phases: 100: Pewaukee Road & Meadow Creek Market/Swan Road



HCM 6th Signalized Intersection Summary 100: Pewaukee Road & Meadow Creek Market/Swan Road

Build Scenario B
AM Peak Hour

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	55	15	5	30	15	40	10	950	40	60	405	50
Future Volume (veh/h)	55	15	5	30	15	40	10	950	40	60	405	50
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No			No			No		
Adj Sat Flow, veh/h/ln	1752	1752	1752	1767	1767	1767	1880	1880	1811	1803	1803	1737
Adj Flow Rate, veh/h	60	16	5	33	16	44	11	1044	27	66	445	34
Peak Hour Factor	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91	0.91
Percent Heavy Veh, %	10	10	10	9	9	9	6	6	6	11	11	11
Cap, veh/h	170	134	42	206	43	119	662	2593	1114	395	2194	943
Arrive On Green	0.10	0.10	0.10	0.10	0.10	0.10	0.02	0.73	0.73	0.64	0.64	0.64
Sat Flow, veh/h	1258	1280	400	1314	416	1144	1790	3571	1535	508	3425	1472
Grp Volume(v), veh/h	60	0	21	33	0	60	11	1044	27	66	445	34
Grp Sat Flow(s),veh/h/ln	1258	0	1680	1314	0	1561	1790	1786	1535	508	1713	1472
Q Serve(g_s), s	4.0	0.0	1.0	2.0	0.0	3.1	0.2	9.7	0.4	5.0	4.6	0.7
Cycle Q Clear(g_c), s	7.1	0.0	1.0	3.0	0.0	3.1	0.2	9.7	0.4	7.4	4.6	0.7
Prop In Lane	1.00		0.24	1.00		0.73	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	170	0	175	206	0	163	662	2593	1114	395	2194	943
V/C Ratio(X)	0.35	0.00	0.12	0.16	0.00	0.37	0.02	0.40	0.02	0.17	0.20	0.04
Avail Cap(c_a), veh/h	216	0	236	248	0	212	801	2593	1114	395	2194	943
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	39.2	0.0	34.9	36.3	0.0	35.9	4.5	4.6	3.3	7.4	6.4	5.7
Incr Delay (d2), s/veh	0.5	0.0	0.1	0.1	0.0	0.5	0.0	0.5	0.0	0.9	0.2	0.1
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	1.2	0.0	0.4	0.6	0.0	1.2	0.0	2.4	0.1	0.5	1.4	0.2
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	39.6	0.0	35.0	36.4	0.0	36.4	4.5	5.0	3.3	8.3	6.6	5.8
LnGrp LOS	D	A	D	D	A	D	A	A	A	A	A	A
Approach Vol, veh/h	81			93			1082			545		
Approach Delay, s/veh	38.4			36.4			5.0			6.8		
Approach LOS	D			D			A			A		
Timer - Assigned Phs	1	2	4		6		8					
Phs Duration (G+Y+Rc), s	7.3	62.4	16.3		69.7		16.3					
Change Period (Y+Rc), s	5.5	7.3	* 7.3		7.3		7.3					
Max Green Setting (Gmax), s	8.5	45.7	* 12		59.7		11.7					
Max Q Clear Time (g_c+I1), s	2.2	9.4	9.1		11.7		5.1					
Green Ext Time (p_c), s	0.0	7.2	0.0		8.7		0.1					

Intersection Summary










HCM 6th Ctrl Delay 8.6
HCM 6th LOS A




Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Lanes, Volumes, Timings
200: Highfield Road & Broken Hill Circle S

















Build Scenario B
AM Peak Hour

						
Lane Group	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Volume (vph)	5	30	35	5	15	20
Future Volume (vph)	5	30	35	5	15	20
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	12	12	12	12	12
Grade (%)	0%			0%	0%	
Storage Length (ft)		0	0		0	0
Storage Lanes		0	0		1	0
Taper Length (ft)			100		100	
Link Speed (mph)	25			25	25	
Link Distance (ft)	314			325	406	
Travel Time (s)	8.6			8.9	11.1	
Confl. Peds. (#/hr)						
Confl. Bikes (#/hr)						
Peak Hour Factor	0.77	0.77	0.77	0.77	0.77	0.77
Growth Factor	100%	100%	100%	100%	100%	100%
Heavy Vehicles (%)	17%	17%	1%	1%	12%	12%
Bus Blockages (#/hr)	0	0	0	0	0	0
Parking (#/hr)						
Mid-Block Traffic (%)	0%			0%	0%	
Shared Lane Traffic (%)						
Lane Group Flow (vph)	45	0	0	51	45	0
Sign Control	Free			Free	Stop	
Intersection Summary						
Area Type:	Other					
Control Type:	Unsignalized					

Intersection						
Int Delay, s/veh	5.2					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	5	30	35	5	15	20
Future Vol, veh/h	5	30	35	5	15	20
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	77	77	77	77	77	77
Heavy Vehicles, %	17	17	1	1	12	12
Mvmt Flow	6	39	45	6	19	26
Major/Minor	Major1		Major2		Minor1	
Conflicting Flow All	0	0	45	0	122	26
Stage 1	-	-	-	-	26	-
Stage 2	-	-	-	-	96	-
Critical Hdwy	-	-	4.11	-	6.52	6.32
Critical Hdwy Stg 1	-	-	-	-	5.52	-
Critical Hdwy Stg 2	-	-	-	-	5.52	-
Follow-up Hdwy	-	-	2.209	-	3.608	3.408
Pot Cap-1 Maneuver	-	-	1570	-	850	1022
Stage 1	-	-	-	-	971	-
Stage 2	-	-	-	-	903	-
Platoon blocked, %	-	-		-		
Mov Cap-1 Maneuver	-	-	1570	-	825	1022
Mov Cap-2 Maneuver	-	-	-	-	825	-
Stage 1	-	-	-	-	971	-
Stage 2	-	-	-	-	877	-
Approach	EB		WB		NB	
HCM Control Delay, s	0		6.4		9.1	
HCM LOS	A					
Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT	
Capacity (veh/h)	927	-	-	1570	-	
HCM Lane V/C Ratio	0.049	-	-	0.029	-	
HCM Control Delay (s)	9.1	-	-	7.4	0	
HCM Lane LOS	A	-	-	A	A	
HCM 95th %tile Q(veh)	0.2	-	-	0.1	-	

Lanes, Volumes, Timings
300: Highfield Road & Highfield Ct./Auburn Ct.


























Build Scenario B
AM Peak Hour










												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	15	1	50	1	1	1	85	20	1	1	50	15
Future Volume (vph)	15	1	50	1	1	1	85	20	1	1	50	15
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	12	12	12	12	12	12	12	12	12	12	12
Grade (%)		0%			0%			0%			0%	
Storage Length (ft)	0		0	0		0	0		0	0		0
Storage Lanes	0		0	0		0	0		0	0		0
Taper Length (ft)	100			100			100			100		
Link Speed (mph)		25			25			25			25	
Link Distance (ft)		327			194			539			406	
Travel Time (s)		8.9			5.3			14.7			11.1	
Confl. Peds. (#/hr)												
Confl. Bikes (#/hr)												
Peak Hour Factor	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86
Growth Factor	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%
Heavy Vehicles (%)	13%	13%	13%	1%	1%	1%	6%	6%	6%	4%	4%	4%
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	0	0	0	0
Parking (#/hr)												
Mid-Block Traffic (%)		0%			0%			0%			0%	
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	76	0	0	3	0	0	123	0	0	76	0
Sign Control		Stop			Stop			Free			Free	
Intersection Summary												
Area Type:	Other											
Control Type:	Unsignalized											

Intersection												
Int Delay, s/veh	5.5											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	15	1	50	1	1	1	85	20	1	1	50	15
Future Vol, veh/h	15	1	50	1	1	1	85	20	1	1	50	15
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	86	86	86	86	86	86	86	86	86	86	86	86
Heavy Vehicles, %	13	13	13	1	1	1	6	6	6	4	4	4
Mvmt Flow	17	1	58	1	1	1	99	23	1	1	58	17
Major/Minor	Minor2		Minor1			Major1			Major2			
Conflicting Flow All	292	291	67	320	299	24	75	0	0	24	0	0
Stage 1	69	69	-	222	222	-	-	-	-	-	-	-
Stage 2	223	222	-	98	77	-	-	-	-	-	-	-
Critical Hdwy	7.23	6.63	6.33	7.11	6.51	6.21	4.16	-	-	4.14	-	-
Critical Hdwy Stg 1	6.23	5.63	-	6.11	5.51	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.23	5.63	-	6.11	5.51	-	-	-	-	-	-	-
Follow-up Hdwy	3.617	4.117	3.417	3.509	4.009	3.309	2.254	-	-	2.236	-	-
Pot Cap-1 Maneuver	639	601	967	635	615	1055	1499	-	-	1578	-	-
Stage 1	914	816	-	783	722	-	-	-	-	-	-	-
Stage 2	755	700	-	911	833	-	-	-	-	-	-	-
Platoon blocked, %								-	-		-	-
Mov Cap-1 Maneuver	604	560	967	565	573	1055	1499	-	-	1578	-	-
Mov Cap-2 Maneuver	604	560	-	565	573	-	-	-	-	-	-	-
Stage 1	853	815	-	731	674	-	-	-	-	-	-	-
Stage 2	702	653	-	854	832	-	-	-	-	-	-	-
Approach	EB		WB			NB			SB			
HCM Control Delay, s	9.7		10.4			6.1			0.1			
HCM LOS	A		B									
Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR				
Capacity (veh/h)	1499	-	-	843	672	1578	-	-				
HCM Lane V/C Ratio	0.066	-	-	0.091	0.005	0.001	-	-				
HCM Control Delay (s)	7.6	0	-	9.7	10.4	7.3	0	-				
HCM Lane LOS	A	A	-	A	B	A	A	-				
HCM 95th %tile Q(veh)	0.2	-	-	0.3	0	0	-	-				

Lanes, Volumes, Timings
400: Highfield Road & Capitol Drive

Build Scenario B
AM Peak Hour

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		 			 			 			 	
Traffic Volume (vph)	75	1100	10	5	970	45	15	1	20	30	1	70
Future Volume (vph)	75	1100	10	5	970	45	15	1	20	30	1	70
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	12	12	12	12	12	12	12	12	12	12	12
Grade (%)		0%			0%			0%			0%	
Storage Length (ft)	355		305	330		290	0		0	0		100
Storage Lanes	1		1	1		1	0		0	0		1
Taper Length (ft)	100			100			100			100		
Link Speed (mph)		55			55			25			25	
Link Distance (ft)		989			984			394			539	
Travel Time (s)		12.3			12.2			10.7			14.7	
Confl. Peds. (#/hr)												
Confl. Bikes (#/hr)												
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Growth Factor	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%
Heavy Vehicles (%)	5%	5%	5%	5%	5%	5%	5%	5%	5%	2%	2%	2%
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	0	0	0	0
Parking (#/hr)												
Mid-Block Traffic (%)		0%			0%			0%			0%	
Shared Lane Traffic (%)												
Lane Group Flow (vph)	82	1196	11	5	1054	49	0	39	0	0	34	76
Sign Control		Free			Free			Stop			Stop	
Intersection Summary												
Area Type:	Other											
Control Type:	Unsignalized											























Intersection												
Int Delay, s/veh	1.7											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	75	1100	10	5	970	45	15	1	20	30	1	70
Future Vol, veh/h	75	1100	10	5	970	45	15	1	20	30	1	70
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	355	-	305	330	-	290	-	-	-	-	-	100
Veh in Median Storage, #	-	0	-	-	0	-	-	2	-	-	2	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	92	92	92	92	92	92	92	92	92	92	92	92
Heavy Vehicles, %	5	5	5	5	5	5	5	5	5	2	2	2
Mvmt Flow	82	1196	11	5	1054	49	16	1	22	33	1	76
Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	1103	0	0	1207	0	0	1898	2473	598	1827	2435	527
Stage 1	-	-	-	-	-	-	1360	1360	-	1064	1064	-
Stage 2	-	-	-	-	-	-	538	1113	-	763	1371	-
Critical Hdwy	4.2	-	-	4.2	-	-	7.6	6.6	7	7.54	6.54	6.94
Critical Hdwy Stg 1	-	-	-	-	-	-	6.6	5.6	-	6.54	5.54	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.6	5.6	-	6.54	5.54	-
Follow-up Hdwy	2.25	-	-	2.25	-	-	3.55	4.05	3.35	3.52	4.02	3.32
Pot Cap-1 Maneuver	611	-	-	557	-	-	41	28	438	48	31	496
Stage 1	-	-	-	-	-	-	152	209	-	238	298	-
Stage 2	-	-	-	-	-	-	487	276	-	363	212	-
Platoon blocked, %	-	-	-	-	-	-	-	-	-	-	-	-
Mov Cap-1 Maneuver	611	-	-	557	-	-	31	24	438	40	27	496
Mov Cap-2 Maneuver	-	-	-	-	-	-	118	125	-	163	145	-
Stage 1	-	-	-	-	-	-	132	181	-	206	295	-
Stage 2	-	-	-	-	-	-	407	274	-	297	184	-
Approach	EB			WB			NB			SB		
HCM Control Delay, s	0.7			0.1			27.5			19.6		
HCM LOS							D			C		
Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1	SBLn2			
Capacity (veh/h)	199	611	-	-	557	-	-	162	496			
HCM Lane V/C Ratio	0.197	0.133	-	-	0.01	-	-	0.208	0.153			
HCM Control Delay (s)	27.5	11.8	-	-	11.5	-	-	33	13.6			
HCM Lane LOS	D	B	-	-	B	-	-	D	B			
HCM 95th %tile Q(veh)	0.7	0.5	-	-	0	-	-	0.8	0.5			


Lanes, Volumes, Timings

Build Scenario B

100: Pewaukee Road & Meadow Creek Market/Swan Road

PM Peak Hour

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	105	45	110	90	20	10	120	1235	100	15	775	85
Future Volume (vph)	105	45	110	90	20	10	120	1235	100	15	775	85
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1972	1972	1900	1972	1972	1900
Lane Width (ft)	12	12	12	12	12	12	12	12	12	12	12	12
Grade (%)		0%			0%			0%			0%	
Storage Length (ft)	120		0	210		0	435		230	355		315
Storage Lanes	1		0	1		0	1		1	1		1
Taper Length (ft)	100			100			100			100		
Right Turn on Red			No			No			No			No
Link Speed (mph)		35			35			45			45	
Link Distance (ft)		420			477			663			577	
Travel Time (s)		8.2			9.3			10.0			8.7	
Confl. Peds. (#/hr)												
Confl. Bikes (#/hr)												
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Growth Factor	100%	100%	100%	100%	100%	100%	100%	100%	62%	100%	100%	62%
Heavy Vehicles (%)	1%	1%	1%	4%	4%	4%	1%	1%	1%	3%	3%	3%
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	0	0	0	0
Parking (#/hr)												
Mid-Block Traffic (%)		0%			0%			0%			0%	
Shared Lane Traffic (%)												
Lane Group Flow (vph)	109	162	0	94	31	0	125	1286	65	16	807	55
Turn Type	Perm	NA		Perm	NA		pm+pt	NA	Perm	Perm	NA	Perm
Protected Phases		4			8		1	6			2	
Permitted Phases	4			8			6		6	2		2
Detector Phase	4	4		8	8		1	6	6	2	2	2
Switch Phase												
Minimum Initial (s)	8.0	8.0		8.0	8.0		8.0	12.0	12.0	12.0	12.0	12.0
Minimum Split (s)	14.9	14.9		15.3	15.3		13.5	19.3	19.3	19.3	19.3	19.3
Total Split (s)	21.0	21.0		21.0	21.0		20.0	59.0	59.0	39.0	39.0	39.0
Total Split (%)	26.3%	26.3%		26.3%	26.3%		25.0%	73.8%	73.8%	48.8%	48.8%	48.8%
Maximum Green (s)	14.1	14.1		13.7	13.7		14.5	51.7	51.7	31.7	31.7	31.7
Yellow Time (s)	3.9	3.9		4.3	4.3		3.5	4.3	4.3	4.3	4.3	4.3
All-Red Time (s)	3.0	3.0		3.0	3.0		2.0	3.0	3.0	3.0	3.0	3.0
Lost Time Adjust (s)	0.0	0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	6.9	6.9		7.3	7.3		5.5	7.3	7.3	7.3	7.3	7.3
Lead/Lag							Lead			Lag	Lag	Lag
Lead-Lag Optimize?												
Vehicle Extension (s)	1.5	1.5		1.5	1.5		1.5	3.0	3.0	4.8	4.8	4.8
Minimum Gap (s)	1.5	1.5		1.5	1.5		1.5	2.0	2.0	3.8	3.8	3.8
Time Before Reduce (s)	30.0	30.0		30.0	30.0		25.0	19.0	19.0	29.0	29.0	29.0
Time To Reduce (s)	0.0	0.0		0.0	0.0		0.0	13.0	13.0	8.0	8.0	8.0
Recall Mode	None	None		None	None		None	C-Max	C-Max	C-Max	C-Max	C-Max
Walk Time (s)												
Flash Dont Walk (s)												
Pedestrian Calls (#/hr)												
v/c Ratio	0.55	0.68		0.57	0.13		0.25	0.51	0.06	0.07	0.41	0.06
Control Delay	42.4	46.7		45.9	30.3		5.4	7.5	5.0	12.3	12.7	11.1

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Queue Delay	0.0	0.0		0.0	0.0		0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	42.4	46.7		45.9	30.3		5.4	7.5	5.0	12.3	12.7	11.1
90th %ile Green (s)	14.1	14.1		13.7	13.7		8.0	51.7	51.7	38.2	38.2	38.2
90th %ile Term Code	Max	Max		Max	Max		Min	Coord	Coord	Coord	Coord	Coord
70th %ile Green (s)	13.6	13.6		13.2	13.2		8.0	52.2	52.2	38.7	38.7	38.7
70th %ile Term Code	Gap	Gap		Hold	Hold		Min	Coord	Coord	Coord	Coord	Coord
50th %ile Green (s)	11.5	11.5		11.1	11.1		8.0	54.3	54.3	40.8	40.8	40.8
50th %ile Term Code	Gap	Gap		Hold	Hold		Min	Coord	Coord	Coord	Coord	Coord
30th %ile Green (s)	9.4	9.4		9.0	9.0		8.0	56.4	56.4	42.9	42.9	42.9
30th %ile Term Code	Gap	Gap		Hold	Hold		Min	Coord	Coord	Coord	Coord	Coord
10th %ile Green (s)	8.4	8.4		8.0	8.0		0.0	57.4	57.4	57.4	57.4	57.4
10th %ile Term Code	Hold	Hold		Hold	Hold		Skip	Coord	Coord	Coord	Coord	Coord
Queue Length 50th (ft)	51	78		45	14		17	142	9	4	125	13
Queue Length 95th (ft)	98	135		89	37		36	208	24	16	182	33
Internal Link Dist (ft)		340			397			583			497	
Turn Bay Length (ft)	120			210			435		230	355		315
Base Capacity (vph)	244	296		204	297		617	2522	1087	224	1982	854
Starvation Cap Reductn	0	0		0	0		0	0	0	0	0	0
Spillback Cap Reductn	0	0		0	0		0	0	0	0	0	0
Storage Cap Reductn	0	0		0	0		0	0	0	0	0	0
Reduced v/c Ratio	0.45	0.55		0.46	0.10		0.20	0.51	0.06	0.07	0.41	0.06

Intersection Summary

Area Type: Other

Cycle Length: 80

Actuated Cycle Length: 80

Offset: 70 (88%), Referenced to phase 2:SBTL and 6:NBTL, Start of Yellow

Natural Cycle: 55

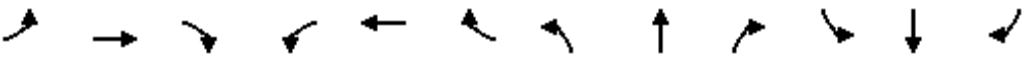










Control Type: Actuated-Coordinated

Splits and Phases: 100: Pewaukee Road & Meadow Creek Market/Swan Road



HCM 6th Signalized Intersection Summary 100: Pewaukee Road & Meadow Creek Market/Swan Road

Build Scenario B
PM Peak Hour

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	105	45	110	90	20	10	120	1235	100	15	775	85
Future Volume (veh/h)	105	45	110	90	20	10	120	1235	100	15	775	85
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No			No			No		
Adj Sat Flow, veh/h/ln	1885	1885	1885	1841	1841	1841	1957	1957	1885	1926	1926	1856
Adj Flow Rate, veh/h	109	47	115	94	21	10	125	1286	65	16	807	55
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Percent Heavy Veh, %	1	1	1	4	4	4	1	1	1	3	3	3
Cap, veh/h	307	83	203	189	202	96	491	2402	1032	281	1770	761
Arrive On Green	0.17	0.17	0.17	0.17	0.17	0.17	0.09	0.65	0.65	0.48	0.48	0.48
Sat Flow, veh/h	1389	485	1187	1205	1179	561	1863	3718	1598	416	3659	1572
Grp Volume(v), veh/h	109	0	162	94	0	31	125	1286	65	16	807	55
Grp Sat Flow(s),veh/h/ln	1389	0	1672	1205	0	1740	1863	1859	1598	416	1830	1572
Q Serve(g_s), s	5.7	0.0	7.1	6.2	0.0	1.2	2.3	15.0	1.2	1.7	11.7	1.5
Cycle Q Clear(g_c), s	6.9	0.0	7.1	13.3	0.0	1.2	2.3	15.0	1.2	3.7	11.7	1.5
Prop In Lane	1.00		0.71	1.00		0.32	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	307	0	286	189	0	298	491	2402	1032	281	1770	761
V/C Ratio(X)	0.36	0.00	0.57	0.50	0.00	0.10	0.25	0.54	0.06	0.06	0.46	0.07
Avail Cap(c_a), veh/h	314	0	295	189	0	298	654	2402	1032	281	1770	761
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	30.9	0.0	30.4	36.5	0.0	28.0	8.2	7.7	5.2	12.2	13.7	11.0
Incr Delay (d2), s/veh	0.3	0.0	1.4	0.7	0.0	0.1	0.1	0.9	0.1	0.4	0.8	0.2
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	1.9	0.0	2.9	1.8	0.0	0.5	0.7	4.5	0.3	0.2	4.3	0.5
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	31.2	0.0	31.8	37.3	0.0	28.0	8.3	8.5	5.3	12.6	14.5	11.2
LnGrp LOS	C	A	C	D	A	C	A	A	A	B	B	B
Approach Vol, veh/h	271			125			1476			878		
Approach Delay, s/veh	31.6			35.0			8.4			14.3		
Approach LOS	C			C			A			B		
Timer - Assigned Phs	1	2	4			6	8					
Phs Duration (G+Y+Rc), s	13.0	46.0	21.0			59.0	21.0					
Change Period (Y+Rc), s	5.5	7.3	* 7.3			7.3	7.3					
Max Green Setting (Gmax), s	14.5	31.7	* 14			51.7	13.7					
Max Q Clear Time (g_c+I1), s	4.3	13.7	9.1			17.0	15.3					
Green Ext Time (p_c), s	0.1	8.6	0.3			11.3	0.0					

Intersection Summary










HCM 6th Ctrl Delay	13.7
HCM 6th LOS	B




Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Lanes, Volumes, Timings
200: Highfield Road & Broken Hill Circle S

















Build Scenario B
PM Peak Hour

						
Lane Group	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Volume (vph)	10	20	30	5	20	30
Future Volume (vph)	10	20	30	5	20	30
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	12	12	12	12	12
Grade (%)	0%			0%	0%	
Storage Length (ft)		0	0		0	0
Storage Lanes		0	0		1	0
Taper Length (ft)			100		100	
Link Speed (mph)	25			25	25	
Link Distance (ft)	314			325	406	
Travel Time (s)	8.6			8.9	11.1	
Confl. Peds. (#/hr)						
Confl. Bikes (#/hr)						
Peak Hour Factor	0.88	0.88	0.88	0.88	0.88	0.88
Growth Factor	100%	100%	100%	100%	100%	100%
Heavy Vehicles (%)	1%	1%	9%	9%	2%	2%
Bus Blockages (#/hr)	0	0	0	0	0	0
Parking (#/hr)						
Mid-Block Traffic (%)	0%			0%	0%	
Shared Lane Traffic (%)						
Lane Group Flow (vph)	34	0	0	40	57	0
Sign Control	Free			Free	Stop	
Intersection Summary						
Area Type:	Other					
Control Type:	Unsignalized					

Intersection						
Int Delay, s/veh	5.8					
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations						
Traffic Vol, veh/h	10	20	30	5	20	30
Future Vol, veh/h	10	20	30	5	20	30
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Stop	Stop
RT Channelized	-	None	-	None	-	None
Storage Length	-	-	-	-	0	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	88	88	88	88	88	88
Heavy Vehicles, %	1	1	9	9	2	2
Mvmt Flow	11	23	34	6	23	34
Major/Minor	Major1		Major2		Minor1	
Conflicting Flow All	0	0	34	0	97	23
Stage 1	-	-	-	-	23	-
Stage 2	-	-	-	-	74	-
Critical Hdwy	-	-	4.19	-	6.42	6.22
Critical Hdwy Stg 1	-	-	-	-	5.42	-
Critical Hdwy Stg 2	-	-	-	-	5.42	-
Follow-up Hdwy	-	-	2.281	-	3.518	3.318
Pot Cap-1 Maneuver	-	-	1533	-	902	1054
Stage 1	-	-	-	-	1000	-
Stage 2	-	-	-	-	949	-
Platoon blocked, %	-	-		-		
Mov Cap-1 Maneuver	-	-	1533	-	882	1054
Mov Cap-2 Maneuver	-	-	-	-	882	-
Stage 1	-	-	-	-	1000	-
Stage 2	-	-	-	-	928	-
Approach	EB		WB		NB	
HCM Control Delay, s	0		6.3		8.9	
HCM LOS	A					
Minor Lane/Major Mvmt	NBLn1	EBT	EBR	WBL	WBT	
Capacity (veh/h)	978	-	-	1533	-	
HCM Lane V/C Ratio	0.058	-	-	0.022	-	
HCM Control Delay (s)	8.9	-	-	7.4	0	
HCM Lane LOS	A	-	-	A	A	
HCM 95th %tile Q(veh)	0.2	-	-	0.1	-	

Lanes, Volumes, Timings
300: Highfield Road & Highfield Ct./Auburn Ct.


























Build Scenario B
PM Peak Hour










												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	15	1	80	1	1	1	55	35	1	1	35	15
Future Volume (vph)	15	1	80	1	1	1	55	35	1	1	35	15
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	12	12	12	12	12	12	12	12	12	12	12
Grade (%)		0%			0%			0%			0%	
Storage Length (ft)	0		0	0		0	0		0	0		0
Storage Lanes	0		0	0		0	0		0	0		0
Taper Length (ft)	100			100			100			100		
Link Speed (mph)		25			25			25			25	
Link Distance (ft)		327			194			539			406	
Travel Time (s)		8.9			5.3			14.7			11.1	
Confl. Peds. (#/hr)												
Confl. Bikes (#/hr)												
Peak Hour Factor	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86	0.86
Growth Factor	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%
Heavy Vehicles (%)	1%	1%	1%	1%	1%	1%	2%	2%	2%	7%	7%	7%
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	0	0	0	0
Parking (#/hr)												
Mid-Block Traffic (%)		0%			0%			0%			0%	
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	111	0	0	3	0	0	106	0	0	59	0
Sign Control		Stop			Stop			Free			Free	
Intersection Summary												
Area Type:	Other											
Control Type:	Unsignalized											

Intersection												
Int Delay, s/veh	5.5											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		↕			↕			↕			↕	
Traffic Vol, veh/h	15	1	80	1	1	1	55	35	1	1	35	15
Future Vol, veh/h	15	1	80	1	1	1	55	35	1	1	35	15
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Stop	Stop	Stop	Stop	Stop	Stop	Free	Free	Free	Free	Free	Free
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	-	-	-	-	-	-	-	-	-	-	-	-
Veh in Median Storage, #	-	0	-	-	0	-	-	0	-	-	0	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	86	86	86	86	86	86	86	86	86	86	86	86
Heavy Vehicles, %	1	1	1	1	1	1	2	2	2	7	7	7
Mvmt Flow	17	1	93	1	1	1	64	41	1	1	41	17
Major/Minor	Minor2		Minor1			Major1			Major2			
Conflicting Flow All	223	222	50	269	230	42	58	0	0	42	0	0
Stage 1	52	52	-	170	170	-	-	-	-	-	-	-
Stage 2	171	170	-	99	60	-	-	-	-	-	-	-
Critical Hdwy	7.11	6.51	6.21	7.11	6.51	6.21	4.12	-	-	4.17	-	-
Critical Hdwy Stg 1	6.11	5.51	-	6.11	5.51	-	-	-	-	-	-	-
Critical Hdwy Stg 2	6.11	5.51	-	6.11	5.51	-	-	-	-	-	-	-
Follow-up Hdwy	3.509	4.009	3.309	3.509	4.009	3.309	2.218	-	-	2.263	-	-
Pot Cap-1 Maneuver	735	678	1021	686	672	1032	1546	-	-	1535	-	-
Stage 1	963	854	-	834	760	-	-	-	-	-	-	-
Stage 2	833	760	-	910	847	-	-	-	-	-	-	-
Platoon blocked, %								-	-		-	-
Mov Cap-1 Maneuver	709	649	1021	602	643	1032	1546	-	-	1535	-	-
Mov Cap-2 Maneuver	709	649	-	602	643	-	-	-	-	-	-	-
Stage 1	923	853	-	799	728	-	-	-	-	-	-	-
Stage 2	796	728	-	825	846	-	-	-	-	-	-	-
Approach	EB		WB			NB			SB			
HCM Control Delay, s	9.3		10			4.5			0.1			
HCM LOS	A		B									
Minor Lane/Major Mvmt	NBL	NBT	NBR	EBLn1	WBLn1	SBL	SBT	SBR				
Capacity (veh/h)	1546	-	-	950	717	1535	-	-				
HCM Lane V/C Ratio	0.041	-	-	0.118	0.005	0.001	-	-				
HCM Control Delay (s)	7.4	0	-	9.3	10	7.3	0	-				
HCM Lane LOS	A	A	-	A	B	A	A	-				
HCM 95th %tile Q(veh)	0.1	-	-	0.4	0	0	-	-				

Lanes, Volumes, Timings
400: Highfield Road & Capitol Drive

Build Scenario B
PM Peak Hour

												
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		 			 			 			 	
Traffic Volume (vph)	60	1150	25	15	1405	35	15	1	10	40	5	95
Future Volume (vph)	60	1150	25	15	1405	35	15	1	10	40	5	95
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width (ft)	12	12	12	12	12	12	12	12	12	12	12	12
Grade (%)		0%			0%			0%			0%	
Storage Length (ft)	355		305	330		290	0		0	0		100
Storage Lanes	1		1	1		1	0		0	0		1
Taper Length (ft)	100			100			100			100		
Link Speed (mph)		55			55			25			25	
Link Distance (ft)		989			984			394			539	
Travel Time (s)		12.3			12.2			10.7			14.7	
Confl. Peds. (#/hr)												
Confl. Bikes (#/hr)												
Peak Hour Factor	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96	0.96
Growth Factor	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%	100%
Heavy Vehicles (%)	2%	2%	2%	2%	2%	2%	1%	1%	1%	3%	3%	3%
Bus Blockages (#/hr)	0	0	0	0	0	0	0	0	0	0	0	0
Parking (#/hr)												
Mid-Block Traffic (%)		0%			0%			0%			0%	
Shared Lane Traffic (%)												
Lane Group Flow (vph)	63	1198	26	16	1464	36	0	27	0	0	47	99
Sign Control		Free			Free			Stop			Stop	
Intersection Summary												
Area Type:	Other											
Control Type:	Unsignalized											

Intersection												
Int Delay, s/veh	2.4											
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Vol, veh/h	60	1150	25	15	1405	35	15	1	10	40	5	95
Future Vol, veh/h	60	1150	25	15	1405	35	15	1	10	40	5	95
Conflicting Peds, #/hr	0	0	0	0	0	0	0	0	0	0	0	0
Sign Control	Free	Free	Free	Free	Free	Free	Stop	Stop	Stop	Stop	Stop	Stop
RT Channelized	-	-	None	-	-	None	-	-	None	-	-	None
Storage Length	355	-	305	330	-	290	-	-	-	-	-	100
Veh in Median Storage, #	-	0	-	-	0	-	-	2	-	-	2	-
Grade, %	-	0	-	-	0	-	-	0	-	-	0	-
Peak Hour Factor	96	96	96	96	96	96	96	96	96	96	96	96
Heavy Vehicles, %	2	2	2	2	2	2	1	1	1	3	3	3
Mvmt Flow	63	1198	26	16	1464	36	16	1	10	42	5	99

Major/Minor	Major1			Major2			Minor1			Minor2		
Conflicting Flow All	1500	0	0	1224	0	0	2091	2856	599	2222	2846	732
Stage 1	-	-	-	-	-	-	1324	1324	-	1496	1496	-
Stage 2	-	-	-	-	-	-	767	1532	-	726	1350	-
Critical Hdwy	4.14	-	-	4.14	-	-	7.52	6.52	6.92	7.56	6.56	6.96
Critical Hdwy Stg 1	-	-	-	-	-	-	6.52	5.52	-	6.56	5.56	-
Critical Hdwy Stg 2	-	-	-	-	-	-	6.52	5.52	-	6.56	5.56	-
Follow-up Hdwy	2.22	-	-	2.22	-	-	3.51	4.01	3.31	3.53	4.03	3.33
Pot Cap-1 Maneuver	443	-	-	565	-	-	30	17	447	~ 24	17	361
Stage 1	-	-	-	-	-	-	166	225	-	127	183	-
Stage 2	-	-	-	-	-	-	363	179	-	380	215	-
Platoon blocked, %		-	-		-	-						
Mov Cap-1 Maneuver	443	-	-	565	-	-	18	14	447	~ 20	14	361
Mov Cap-2 Maneuver	-	-	-	-	-	-	107	91	-	97	114	-
Stage 1	-	-	-	-	-	-	142	193	-	109	178	-
Stage 2	-	-	-	-	-	-	249	174	-	317	184	-

Approach	EB	WB	NB	SB
HCM Control Delay, s	0.7	0.1	34.2	35.4
HCM LOS			D	E

Minor Lane/Major Mvmt	NBLn1	EBL	EBT	EBR	WBL	WBT	WBR	SBLn1	SBLn2
Capacity (veh/h)	150	443	-	-	565	-	-	99	361
HCM Lane V/C Ratio	0.181	0.141	-	-	0.028	-	-	0.473	0.274
HCM Control Delay (s)	34.2	14.5	-	-	11.6	-	-	70.5	18.7
HCM Lane LOS	D	B	-	-	B	-	-	F	C
HCM 95th %tile Q(veh)	0.6	0.5	-	-	0.1	-	-	2.1	1.1

Notes			
~: Volume exceeds capacity	\$: Delay exceeds 300s	+: Computation Not Defined	*: All major volume in platoon

APPENDIX E

TRAFFIC GAP CALCULATIONS

Gap Analysis Calculations

Capitol Drive & Highfield Road

Inputs

Lanes on Major St. (both dir.)	4 lanes	EB=2 lanes, WB=2 lanes
%HV on Minor Street (AM)	1.6%	New traffic from minor = low truck %
%HV on Minor Street (PM, SAT)	3.3%	New traffic from minor = low truck %
Grade	0.0%	
Gap Acceptance	2 stage	
Legs at Intersection	4 leg	

Calculations

	AM					PM, SAT				
	T _{c,x}	T _{f,x}	1 Veh	2 Veh	3 Veh	T _{c,x}	T _{f,x}	1 Veh	2 Veh	3 Veh
Left turn from major	3.1	2.2	3.1	8.5	13.8	3.2	2.2	3.2	8.6	14.0
Right Turn from Minor	5.9	3.3	5.9	15.2	24.4	6.0	3.3	6.0	15.3	24.6
Through Traffic on Minor	5.5	4.0	5.5	15.1	24.6	5.6	4.0	5.6	15.2	24.8
Left Turn from Minor	6.5	3.5	6.5	16.6	26.6	6.6	3.5	6.6	16.7	26.8

Equations

$$T_{c,x} = T_{c,base} + T_{c,HV}P_{HV} + T_{c,G}G - T_{c,T} - T_{3,LT}$$

$$T_{f,x} = T_{f,base} + T_{f,HV}P_{HV}$$

Definitions

T _{c,x}	Critical gap for movement
T _{c,base}	base critical gap (Exhibit 17-5)
T _{c,HV}	HV adjustment factor (1.0 2-ln, 2.0 4-ln) for major street
P _{HV}	proportion of heavy vehicles for minor movement
T _{c,G}	grade adjustment factor (0.1 mvmts 9 & 12, 0.2 mvmts 7, 8, 10, & 11)
G	percent grade divided by 100
T _{c,T}	2-stage gap acceptance adjustment factor (1.0 for 1st or 2nd stage, 0.0 for only 1 stage)
T _{3,LT}	intersection geometry adjustment factor (0.7 minor street left turn at 3-leg intersection, 0.0 otherwise)
T _{f,x}	minor movement follow-up time
T _{f,base}	base follow-up time from exhibit 17-5
T _{f,HV}	heavy vehicle adjustment factor (0.9 for 2-ln major, 1.0 for 4-ln major)
P _{HV}	proportion of heavy vehicles for minor movement

Look Up Tables

Major Street	T _{c,HV}	T _{f,HV}
2 lanes	1.0	0.9
4 lanes	2.0	1.0

Grade Adjust.	T _{c,G}
Minor RT	0.1
Minor LT/TH	0.2

Gap Acceptance	T _{c,t}
1 stage	0.0
2 stage	1.0

Geometry	T _{3,lt}
3-leg	0.7
4-leg	0.0

HCM Exhibit 17-5	Base Critical Gap T _{c,base} (s)		Base follow-up
Vehicle Movement	2-Ln Major	4-Ln Major	T _{f,base} (s)
LT from Major	4.1	4.1	2.2
RT from Minor	6.2	6.9	3.3
TH on Minor	6.5	6.5	4.0
LT from Minor	7.1	7.5	3.5

Calculated Values

T _{c,HV}	T _{f,HV}
2.0	1.0

G
0.0

T _{c,t}
1.0

T _{3,lt}
0.0

T _{c,base}	T _{f,base}	T _{c,G}
4.1	2.2	0.0
6.9	3.3	0.1
6.5	4.0	0.2
7.5	3.5	0.2

	P _{HV}
AM	0.02
PM	0.03

Weekday AM 4/13/20203																
Westbound Gaps																
Start Time	2 - 3	4 - 5	6 - 7	8 - 9	10 - 11	12 - 13	14 - 15	16 - 17	18 - 19	20 - 21	22 - 23	24 - 25	26 - 27	28 - 29	>29	
7:15 AM	6	22	10	13	7	5	1	0	2	1	0	0	0	0	0	2
7:30 AM	8	14	11	9	9	4	2	2	0	0	0	2	1	2	0	0
7:45 AM	9	15	14	7	2	6	6	1	1	2	0	0	0	0	0	2
8:00 AM	11	16	17	7	8	3	3	2	2	2	1	1	0	0	0	0
Total	34	67	52	36	26	18	12	5	5	5	1	3	1	2	4	
Eastbound Gaps																
Start Time	2 - 3	4 - 5	6 - 7	8 - 9	10 - 11	12 - 13	14 - 15	16 - 17	18 - 19	20 - 21	22 - 23	24 - 25	26 - 27	28 - 29	>29	
7:15 AM	9	9	7	7	5	2	1	3	3	0	0	0	0	0	0	4
7:30 AM	8	11	6	2	5	3	2	1	0	0	3	1	0	0	0	3
7:45 AM	6	16	7	5	3	3	3	2	3	1	2	0	0	0	0	2
8:00 AM	11	15	5	4	0	4	3	3	1	1	1	0	3	0	0	2
Total	34	51	25	18	13	12	9	9	7	2	6	1	3	0	0	11
Combined Gaps																
Start Time	2 - 3	4 - 5	6 - 7	8 - 9	10 - 11	12 - 13	14 - 15	16 - 17	18 - 19	20 - 21	22 - 23	24 - 25	26 - 27	28 - 29	>29	
7:15 AM	23	15	8	4	4	1	1	0	0	0	0	0	0	0	0	1
7:30 AM	14	25	11	7	1	1	0	0	0	0	0	1	0	0	0	0
7:45 AM	21	17	12	4	0	3	3	0	1	0	0	0	0	0	0	0
8:00 AM	23	14	11	5	4	2	2	0	0	0	0	0	0	0	0	0
Total	81	71	42	20	9	7	6	0	1	0	0	1	0	0	0	1

Gap Calcs	SB RT from Minor		
	Gap Times	Gaps	
1 Veh (x1)	5.9	15.08	142
2 Veh (x2)	15.18	24.3	44
3 Veh (x3)	24.4	& up	29
Equivalent Gaps			214

Westbound Gap

Gap Calcs	NB RT from Minor		
	Gap Times	Gaps	
1 Veh (x1)	5.9	15.08	75
2 Veh (x2)	15.18	24.3	57
3 Veh (x3)	24.4	& up	45
Equivalent Gaps			177

Eastbound Gap

Gap Calcs	SB TH from Minor			SB LT from Minor		
	Gap Times	Gaps		Gap Times	Gaps	
1 Veh (x1)	5.5	14.98	155	6.5	16.48	73
2 Veh (x2)	15.08	24.5	46	16.58	26.5	47
3 Veh (x3)	24.6	& up	28	26.6	& up	40
Equivalent Gaps			228			160

Stage 1-WB Gap

Stage 2-EB Gap

Gap Calcs	NB TH from Minor			NB LT from Minor		
	Gap Times	Gaps		Gap Times	Gaps	
1 Veh (x1)	5.5	14.98	85	6.5	16.48	132
2 Veh (x2)	15.08	24.5	58	16.58	26.5	36
3 Veh (x3)	24.6	& up	44	26.6	& up	20
Equivalent Gaps			187			189

Stage 1-EB Gap

Stage 2-WB Gap

Gap Calcs	EB LT from Major		
	Gap Times	Gaps	
1 Veh (x1)	3.1	8.38	141
2 Veh (x2)	8.5	13.7	141
3 Veh (x3)	13.8	& up	122
Equivalent Gaps			404

Westbound Gap

Gap Calcs	WB LT from Major		
	Gap Times	Gaps	
1 Veh (x1)	3.1	8.38	95
2 Veh (x2)	8.5	13.7	76
3 Veh (x3)	13.8	& up	149
Equivalent Gaps			320

Eastbound Gap

Weekday PM 4/13/23																
Westbound Gaps																
Start Time	2 - 3	4 - 5	6 - 7	8 - 9	10 - 11	12 - 13	14 - 15	16 - 17	18 - 19	20 - 21	22 - 23	24 - 25	26 - 27	28 - 29	>29	
4:15 PM	2	15	14	9	4	1	1	1	0	0	0	1	0	3	0	
4:30 PM	6	20	15	12	5	0	1	2	0	0	0	0	1	0	0	
4:45 PM	5	13	13	7	1	4	2	1	1	1	1	0	0	0	0	
5:00 PM	9	21	21	6	0	1	4	1	0	0	1	0	0	0	1	
Total	22	69	63	34	10	6	8	5	1	1	2	1	1	3	1	
Eastbound Gaps																
Start Time	2 - 3	4 - 5	6 - 7	8 - 9	10 - 11	12 - 13	14 - 15	16 - 17	18 - 19	20 - 21	22 - 23	24 - 25	26 - 27	28 - 29	>29	
4:15 PM	7	8	7	6	3	0	0	1	3	1	1	1	0	1	4	
4:30 PM	11	7	4	2	0	4	2	3	0	1	0	2	1	0	7	
4:45 PM	6	13	8	3	3	3	1	3	0	0	1	1	1	1	3	
5:00 PM	8	8	4	4	1	4	3	0	1	2	0	1	0	1	6	
Total	32	36	23	15	7	11	6	7	4	4	2	5	2	3	20	
Combined Gaps																
Start Time	2 - 3	4 - 5	6 - 7	8 - 9	10 - 11	12 - 13	14 - 15	16 - 17	18 - 19	20 - 21	22 - 23	24 - 25	26 - 27	28 - 29	>29	
4:15 PM	11	16	10	3	2	1	1	0	0	1	0	0	0	0	0	
4:30 PM	13	18	8	5	3	1	1	0	0	0	0	0	0	0	0	
4:45 PM	9	13	5	2	3	3	1	0	0	0	0	0	0	0	0	
5:00 PM	12	20	10	4	0	1	2	0	0	0	0	0	0	0	0	
Total	45	67	33	14	8	6	5	0	0	1	0	0	0	0	0	

Gap Calcs	SB RT from Minor		
	Gap Times	Gaps	
1 Veh (x1)	6.0	15.17	118
2 Veh (x2)	15.27	24.5	25
3 Veh (x3)	24.6	& up	17
Equivalent Gaps			160

Westbound Gap

Gap Calcs	NB RT from Minor		
	Gap Times	Gaps	
1 Veh (x1)	6.0	15.17	60
2 Veh (x2)	15.27	24.5	41
3 Veh (x3)	24.6	& up	86
Equivalent Gaps			187

Eastbound Gap

Gap Calcs	SB TH from Minor			SB LT from Minor		
	Gap Times	Gaps		Gap Times	Gaps	
1 Veh (x1)	5.6	15.07	131	6.6	16.57	57
2 Veh (x2)	15.17	24.7	26	16.67	26.7	41
3 Veh (x3)	24.8	& up	17	26.8	& up	73
Equivalent Gaps			174			171

Stage 1-WB Gap

Stage 2-EB Gap

Gap Calcs	NB TH from Minor			NB LT from Minor		
	Gap Times	Gaps		Gap Times	Gaps	
1 Veh (x1)	5.6	15.07	66	6.6	16.57	104
2 Veh (x2)	15.17	24.7	43	16.67	26.7	18
3 Veh (x3)	24.8	& up	85	26.8	& up	14
Equivalent Gaps			194			135

Stage 1-EB Gap

Stage 2-WB Gap

Gap Calcs	EB LT from Major		
	Gap Times	Gaps	
1 Veh (x1)	3.2	8.47	149
2 Veh (x2)	8.6	13.9	83
3 Veh (x3)	14.0	& up	46
Equivalent Gaps			278

Westbound Gap

Gap Calcs	WB LT from Major		
	Gap Times	Gaps	
1 Veh (x1)	3.2	8.47	75
2 Veh (x2)	8.6	13.9	58
3 Veh (x3)	14.0	& up	142
Equivalent Gaps			275

Eastbound Gap



Guidepost Montessori at Pewaukee

Parcel 2 of CSM 10366 – Pewaukee, WI 53072

Plan Commission Meeting

PWC #0906994

December 2023

Higher Ground Education

Family of Schools and Educational Offerings for ALL ages



Pre-School - Elementary



Middle and High School



School of Thought



Global Training Program

Our Mission



*We exist to guide, empower, and
nurture independence in children.*

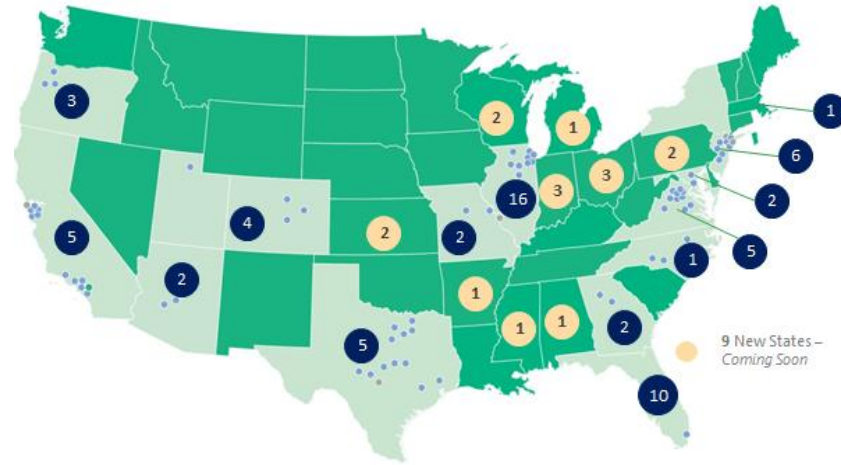
www.guidepostmontessori.com



Growing Global Community

*We are building on a vision for
significant exchange of students
and staff between markets*

Guidepost US



- 125+ open schools in the United States

Guidepost China



Guidepost International



Paris, France



Barcelona, Spain



Vienna, Austria

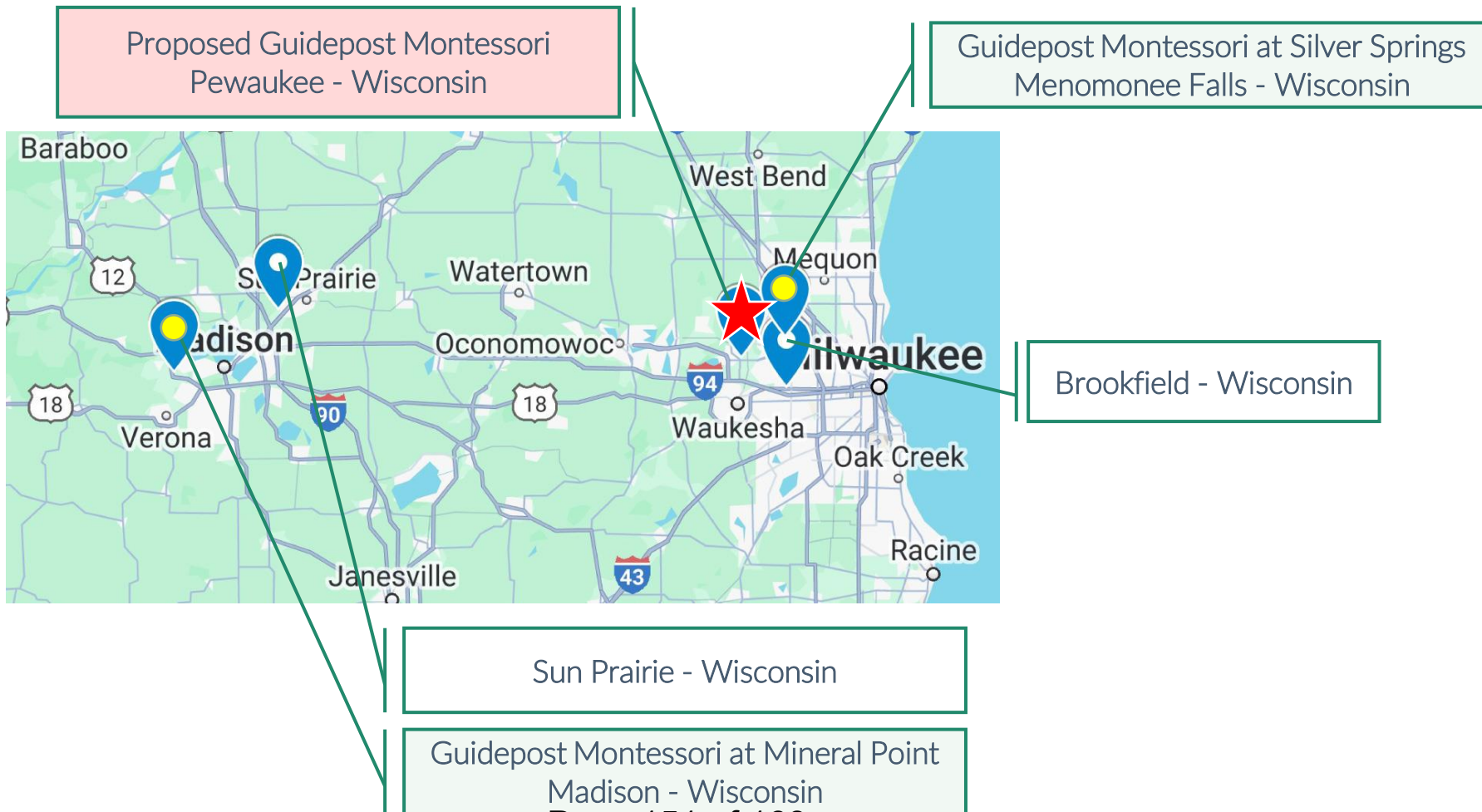


Toronto, Canada

Guidepost is a part of the local Wisconsin Community



Guidepost has (2) open Wisconsin locations and is excited to add Pewaukee and more!



Current Open Locations – (2) ●

Silver Springs – Menomonee Falls, WI

Mineral Point – Madison, WI

Proposed New Locations – (2)

Pewaukee – Wisconsin ★

Sun Prairie – Wisconsin

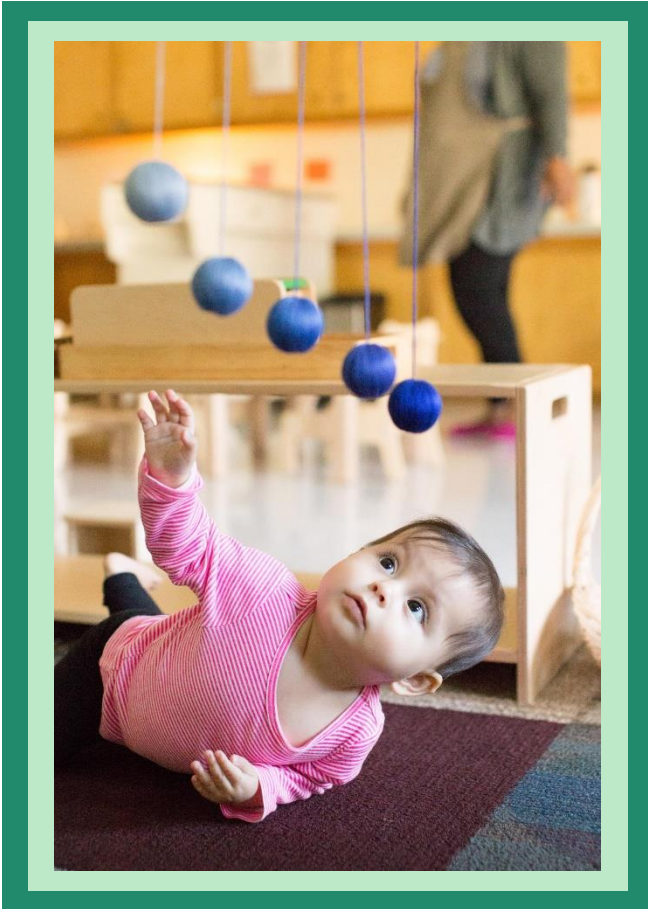
Pipeline

Brookfield – Wisconsin

Searching for other Locations



Guidepost Provides High-Quality Montessori Education



What is Montessori Education?

Montessori education is the oldest, most widely recognized and tested form of early childhood education on the planet.

Montessori education prepares children for the twenty first century by equipping them with the skills they need to flourish in a constantly changing world.

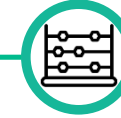
How we deliver: High Quality Education



Classrooms with natural light



Beautiful Learning
Environments



High-end classroom
finishes & furniture



Prepared Environment

From academic readiness — including language, math, sensorial, cultural, and practical life advancement — to developing strong social skills, the prepared environment of a Montessori classroom provides children with a foundation on which to thrive.





Key Factors

Morning Drop-off Period
(~2.5) hours

Half-Day Dismissals
(~0.5) hours

Afternoon Pick-up Period
(~3.5) hours

Convenient and Good Neighbor Pick up and Drop Off Operations

Drop-off & Pick-up Procedure

- Morning Drop-off Period = (~2.5) hours
- Half-Day Dismissals = (~0.5) hours
- Afternoon Pick-up Period = (~3.5) hours

Sample School Schedule

7:00 - 7:30	Staff Arrival
7:30 - 10:00	Drop Off Arrival Window <ul style="list-style-type: none">• 7:30am – 9am is pre-care
9:00 - 11:00	Morning Work Period Starts
11:00 – 12:00	Lunch and Outside Play
12:00 - 12:30	Half-Day Dismissal
12:30- 2:30	Nap Time
2:30 - 4:30	Snack and Outside Play
2:30 - 6:00	Pick-up Departure Window <ul style="list-style-type: none">• 3pm – 6pm is extended-care

*We look forward to opening our doors
to your community!!!*

CITY OF
PEWAUKEE
WISCONSIN

THANK YOU
Questions about
Higher Ground Education?



Key Factors

Zoning District
B6 – Mixed Use Business

Parking Spaces
(1) space per (2) Employees
(8) Required Spaces

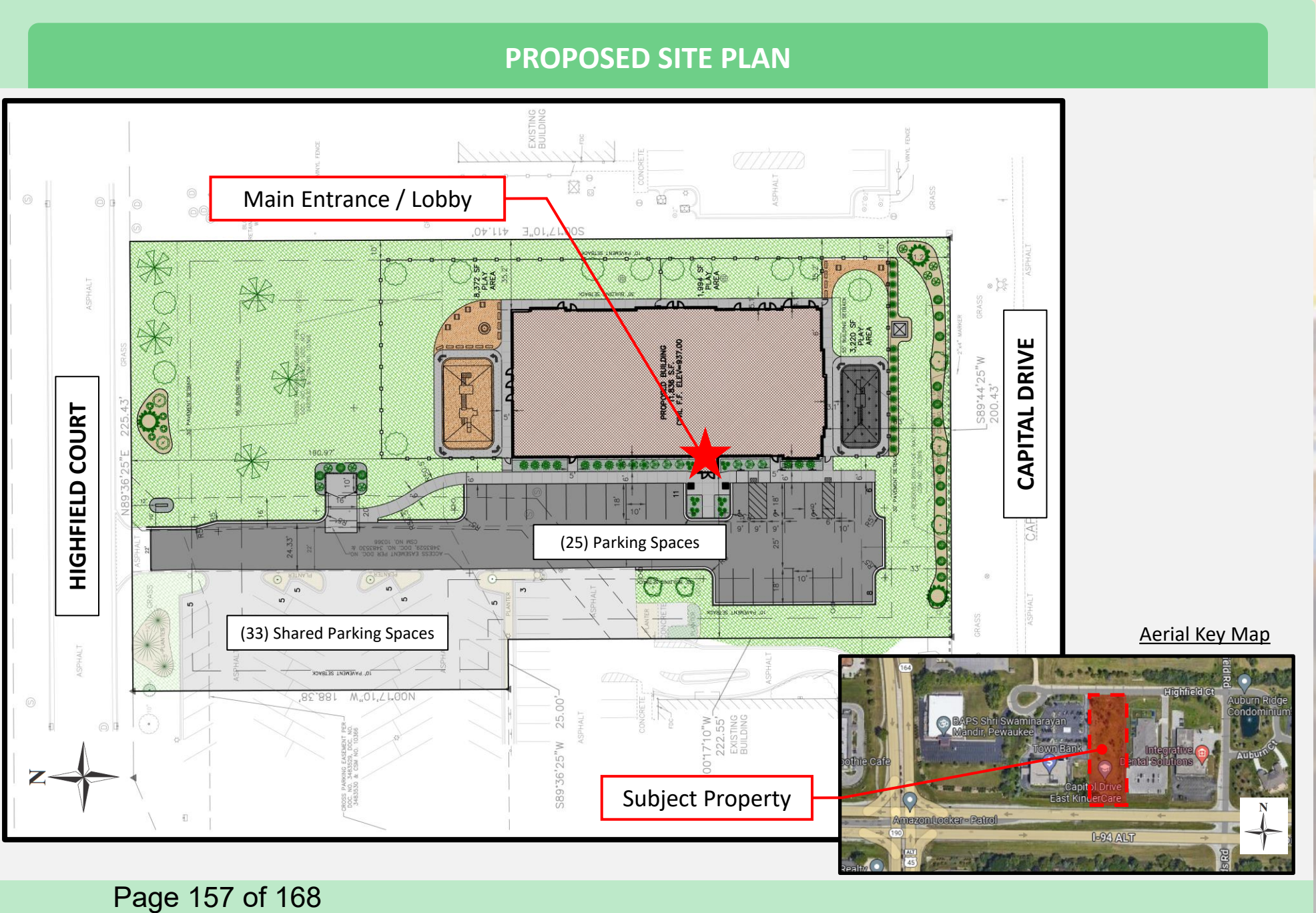
(25) Provided On-Site Spaces
(33) Shared Parking Spaces

Building Square Footage
TOTAL: +/- 11,836 SF

Classroom Programming
(1) - Nido
(1) - Young Toddler
(3) - Toddler
(5) - Children's House

TOTAL CHILDREN: 152

Administrative & Support
Outdoor Playground Areas
Gross Motor Activity (GMA)
Lobby
Kitchen / Staff Break Rm
HOS Office
Meeting Room
Storage
Restrooms



❖
 Key Factors

Classroom Programming

- (1) - Nido
- (1) - Young Toddler
- (3) - Toddler
- (5) - Children's House

TOTAL CHILDREN: 152

- Administrative & Support
- Gross Motor Activity (GMA)
- Lobby
- Kitchen / Staff Break Rm
- HOS Office
- Meeting Room
- Storage
- Restrooms

PROPOSED FLOOR PLAN

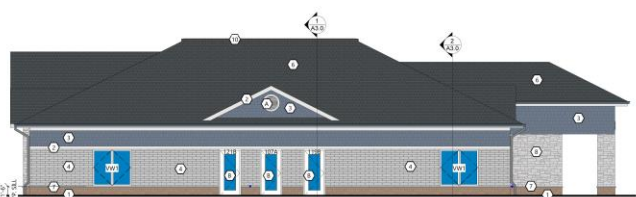




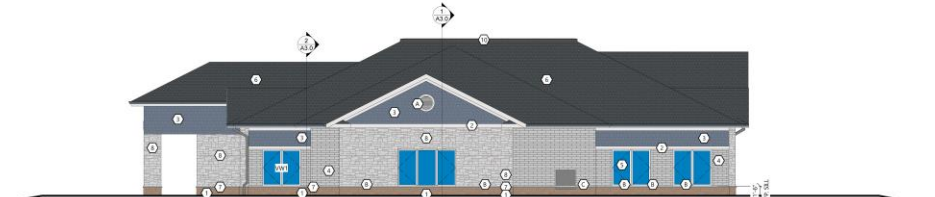
Exterior Finish Key

- 10 Metal Coping
- 9 Casement Window
- 8 Stone Veneer
- 7 Precast Conc Sill
- 6 Asphalt Shingles
- 5 Alum Storefront
- 4 Brick Veneer
- 3 Shake Siding
- 2 Hardie Board Trim
- 1 Thin Stone Veneer

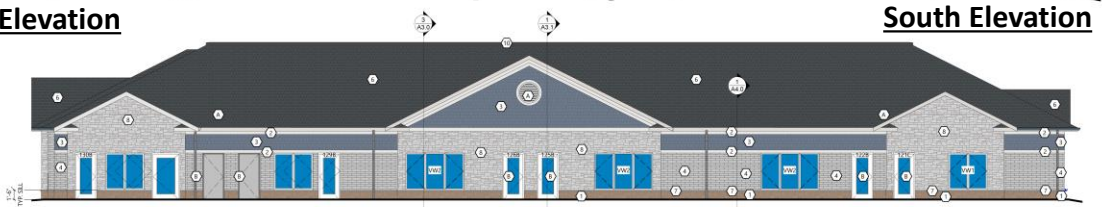
PROPOSED ELEVATIONS



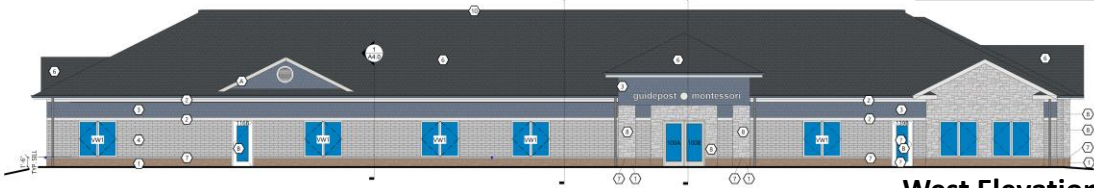
North Elevation



South Elevation



East Elevation



West Elevation

THANK YOU

Questions about the Site, Floor Plans, or Elevations?



Key Factors

Traffic Impact Study
(4) Intersections
(4) Roadway Segments

Trip Generation Analyses

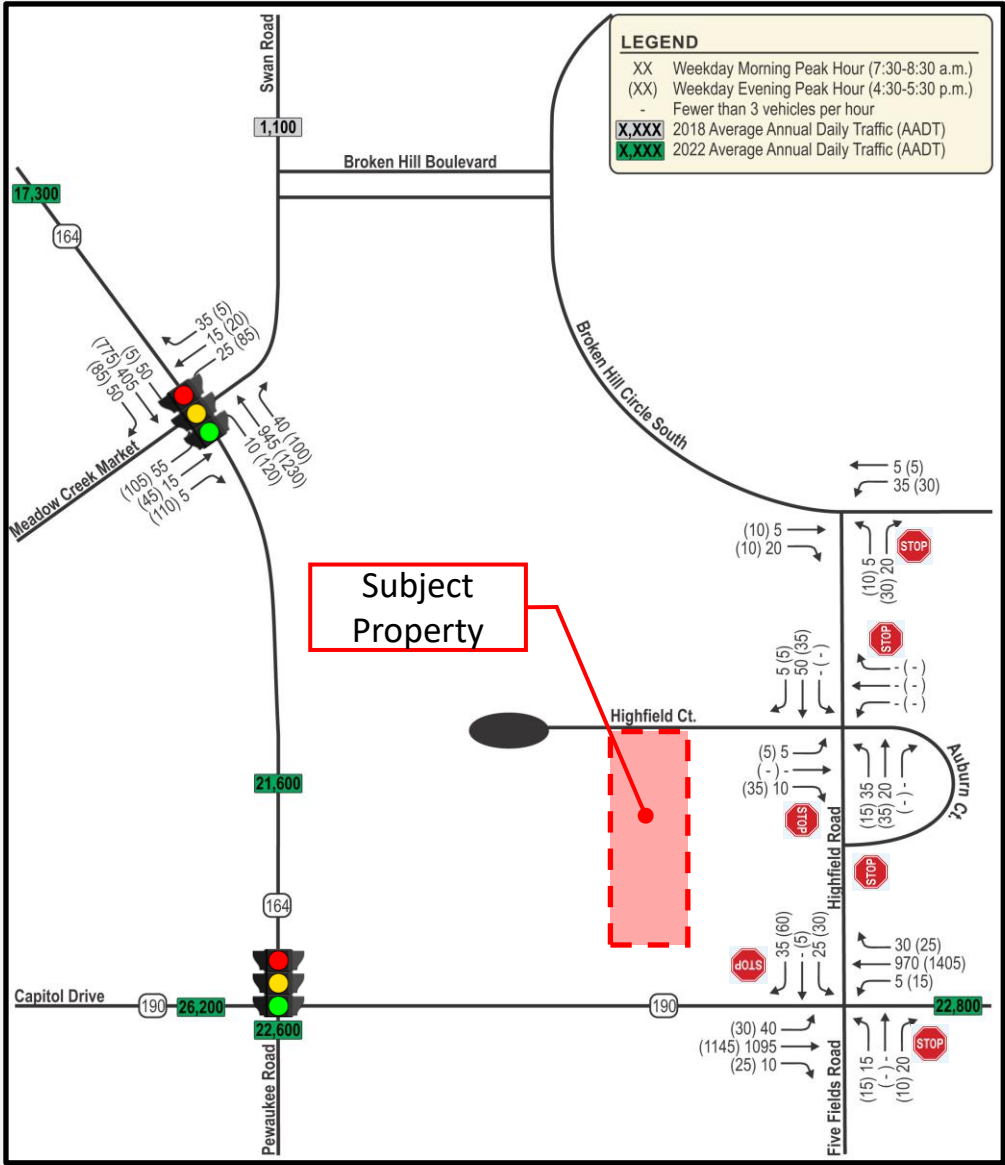
Level of Service (LOS)
Analyses

Modifications to the study
intersections are not
recommended.

Based on the gap study,
there are enough excess
gaps in Capitol Drive traffic
to accommodate both
existing traffic and future
traffic with acceptable
delays (LOS D or better).



TRAFFIC IMPACT STUDY – EXISTING PEAK HOUR TRAFFIC VOLUMES (Exhibit 4)





Key Factors

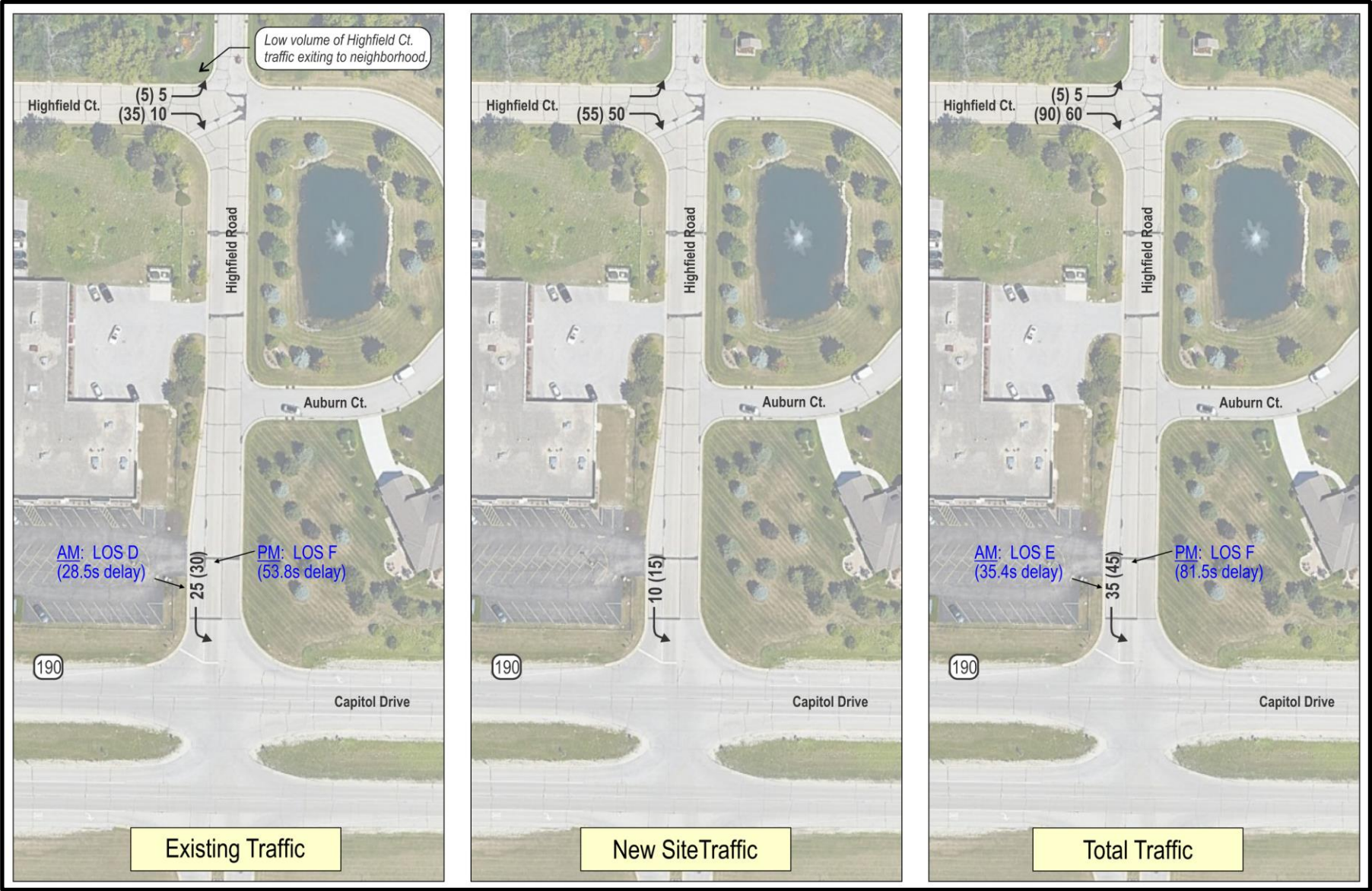
Low Volume of Highfield Court traffic exiting to residential neighborhood.

Legend

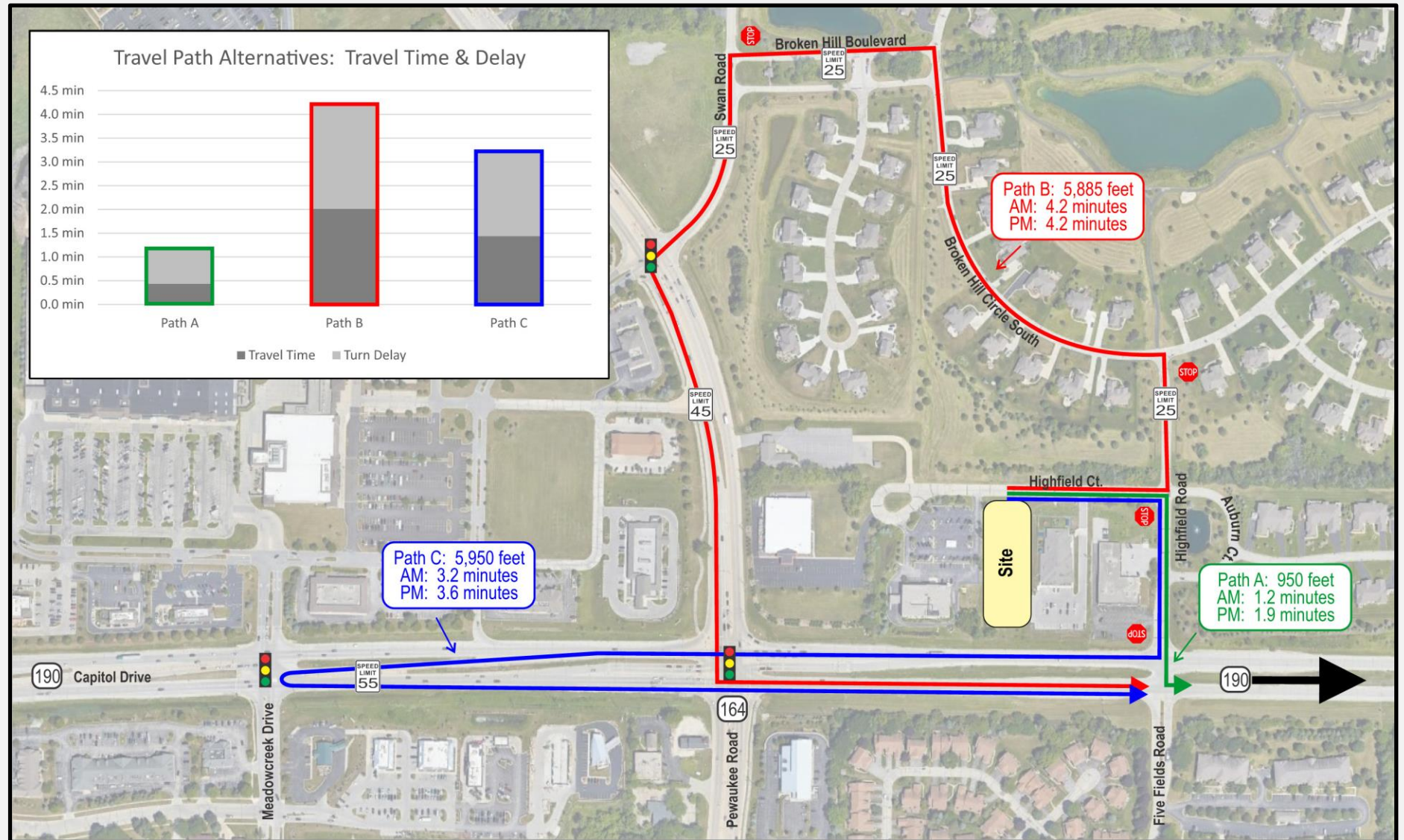
XX AM Peak Hour (7:30 – 8:30 am)
(XX) PM Peak Hour (4:30 – 5:30 am)



TRAFFIC MEMO – EXHIBIT 1: PEAK HOUR TRAFFIC VOLUMES



Path B (red) cutting through the Broken Hill neighborhood takes the longest amount of time.



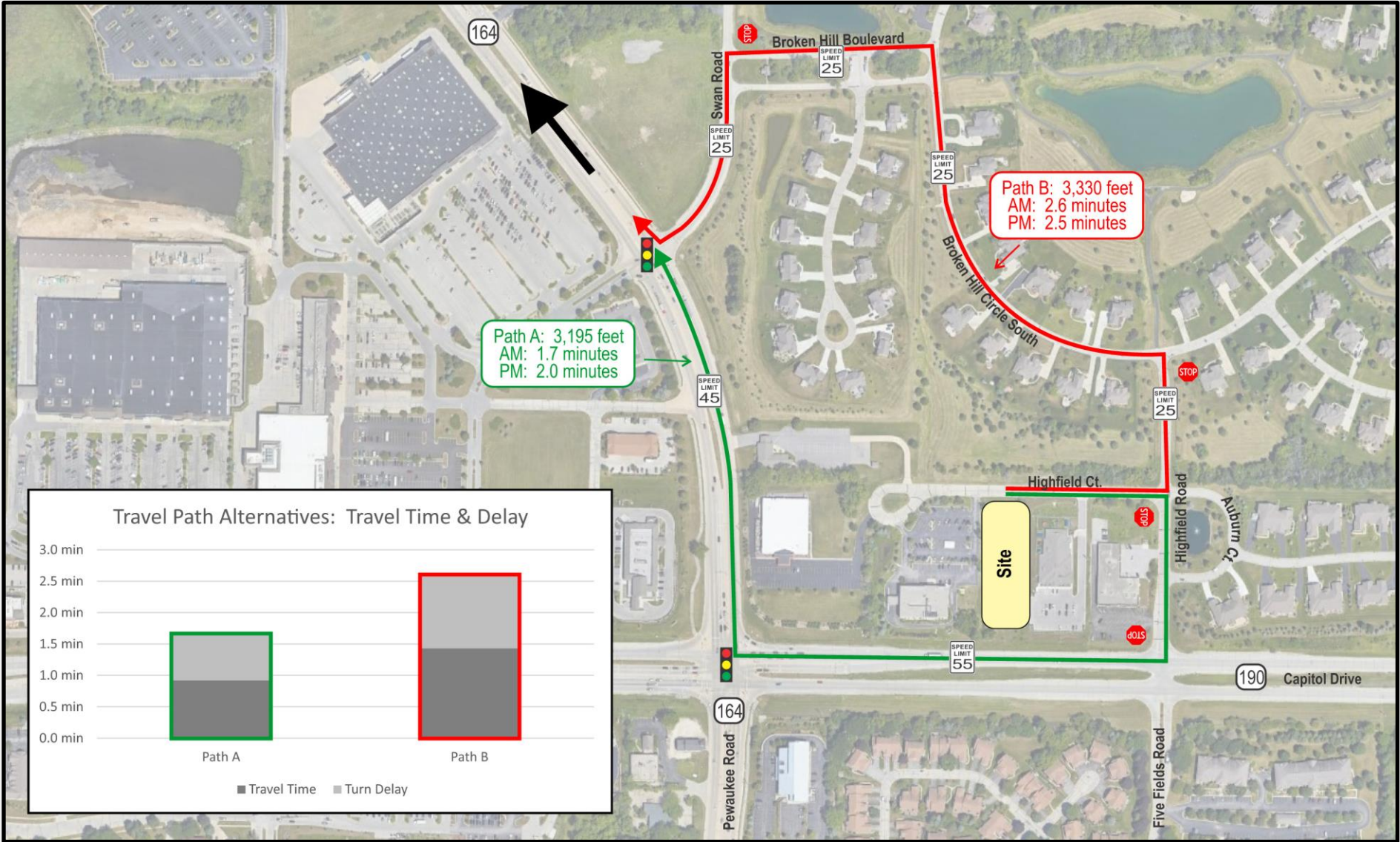
❖
Key Factors

Path A (green) is the direct route onto Capitol Drive to reach northbound Pewaukee Road.

Path B (red) cutting through the Broken Hill neighborhood is the longer route that takes more time to reach northbound Pewaukee Road.



TRAFFIC MEMO – EXHIBIT 3: ALTERNATE ROUTES TRAVEL TIME (North onto Pewaukee Rd)



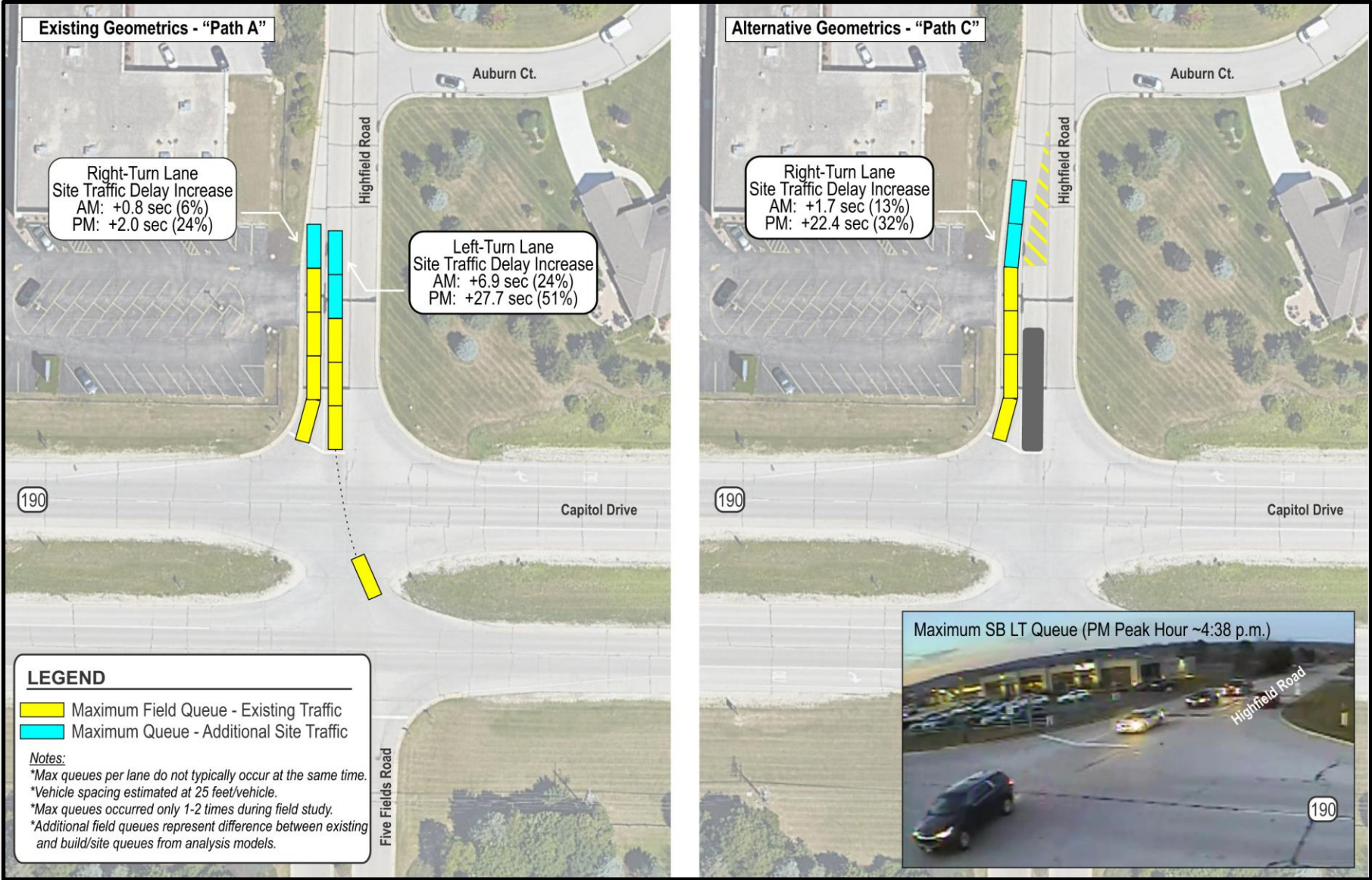
Proposed modifications include constructing a raised concrete channelization island and installing right-turn only signs and no left-turn signs to channelize all eastbound Highfield Court traffic to southbound Highfield Road.



An aerial photograph of a T-intersection where Highfield Ct. meets Highfield Road. Highfield Road runs vertically, and Highfield Ct. branches off to the left. Auburn Ct. branches off to the right. A raised concrete island is located at the intersection. Various traffic signs are present: a 'No Left Turn' sign on Highfield Ct., a 'Right Turn Only' sign with a right-turn arrow on Highfield Ct., a 'STOP' sign on Highfield Ct., and a 'Right Turn Only' sign with a right-turn arrow on Auburn Ct. The area is surrounded by green grass, trees, and a small pond on the right. Labels with arrows point to the 'No Left Turn' sign, the 'Right Turn Only' sign on Highfield Ct., the 'STOP' sign, the 'Right Turn Only' sign on Auburn Ct., and the 'Raised Concrete Island'.

Key Factors

To mitigate southbound left-turn delays, the southbound approach could be configured with a raised median on Highfield Road to restrict traffic to turn right only onto westbound Capitol Drive. Traffic heading eastbound on Capitol Drive would be able to U-turn at Meadowcreek Drive as shown on previous Exhibit 2.





Key Factors

Any new use on the development site will increase trips in the study area. The mitigation traffic mitigation measures noted in this report for Highfield Court would eliminate exiting traffic from cutting through the Broken Hill neighborhood to reach Pewaukee Road or Capitol Drive.

TRAFFIC MEMO – EXHIBIT 6: ALTERNATIVE LAND USES TRIP GENERATION

Comparison Trip Generation Table									
Land Use	ITE Code	Size ^{2, 3}	Weekday Daily	AM Peak			PM Peak		
				In	Out	Total	In	Out	Total
Early Childhood Center (Day Care Center)	565	152 Students	620 (4.09)	60 (53%)	50 (47%)	110 FCE	50 (47%)	55 (53%)	105 FCE
Medical-Dental Office Building (Stand-alone)	720	11,836 SF	430 (36.00)	30 (79%)	5 (21%)	35 FCE	15 (30%)	30 (70%)	45 FCE
Strip Retail Plaza (<40k)	822	11,836 SF	640 (54.45)	20 (60%)	10 (40%)	30 (2.36)	45 (50%)	45 (50%)	90 FCE
Pharmacy/Drugstore with Drive-Through Window	881	13,000 SF	1,410 (108.40)	25 (52%)	25 (48%)	50 (3.74)	70 (50%)	65 (50%)	135 (10.25)
General Office Building	710	11,836 SF	180 FCE	20 (88%)	5 (12%)	25 FCE	5 (17%)	25 (83%)	30 FCE
Drive-in Bank	912	3 Lanes	390 FCE	15 (61%)	10 (39%)	25 FCE	40 (49%)	40 (51%)	80 (27.07)
High-Turnover (Sit-Down) Restaurant	932	5,000 SF	540 (107.20)	30 (55%)	20 (45%)	50 (9.57)	25 (61%)	20 (39%)	45 (9.05)
Fast-Food Restaurant with Drive-Through Window	934	3,000 SF	1,400 (467.48)	70 (51%)	65 (49%)	135 (44.61)	50 (52%)	50 (48%)	100 (33.03)

¹ ITE Trip Rates (X.XX) and/or Fitted Curve Equations (FCE) are from the ITE Trip Generation Manual, 11th Edition.

² Office and retail uses were estimated to be the same size as the proposed day care use (11,836 sf).

³ The bank, pharmacy, and restaurant uses were estimated to be average typical sizes for similar existing uses in the area.

THANK YOU

Questions and Discussion

**CITY OF PEWAUKEE
COMMON COUNCIL AGENDA ITEM 6.**

DATE: January 15, 2024

DEPARTMENT: Planning

PROVIDED BY: Nick Fuchs

SUBJECT:

Discussion and Possible Action Regarding **Resolution 24-01-02** Releasing a Forty-Five Foot Reserved Right-of-Way Restriction Upon Lots 1 and 2 of Certified Survey Map No. 10366 as Requested by Quattro Development Reservation [Fuchs]

BACKGROUND:

FINANCIAL IMPACT:

RECOMMENDED MOTION:

A motion approving the release of the 45' Reserved Right-of-Way restriction located upon Lots 1 and 2 of CSM No. 10366.

ATTACHMENTS:

Description

Resolution 24-01-02

Resolution 23-09-14

Quattro Development Resolution

Reservation release request

CSM 10366

**A RESOLUTION AUTHORIZING THE RELEASE, WAIVER, VACATION AND
REVISION OF A FORTY-FIVE FOOT RESERVED RIGHT-OF-WAY RESTRICTION
UPON LOTS 1 AND 2 OF CERTIFIED SURVEY MAP NO. 10366 FOR PROPERTIES
LOCATED AT APPROXIMATELY N35 W23877 HIGHFIELD COURT BEARING TAX
KEY NUMBERS PWC 0906-993 AND PWC 0906-994**

WHEREAS, the Plan Commission having approved a Conditional Use and Site and Building Plan Review Application upon the application of Quattro Development, LLC on March 16, 2023, and the Plan Commission having conditioned approval thereof in part upon Common Council approval of the release of a 45' Reserved Right-of-Way restriction located upon Parcels 1 and 2 of Certified Survey Map No. 10366, recorded April 13, 2007 in Volume 98 of Certified Survey Maps on Pages 03 to 209, inclusive as Document No. 3472291 with the Office of the Register of Deeds for Waukesha County, Wisconsin, as further described below:

Release of the 45' Reserved Right-Of-Way located along the south property line of Parcel 1 of CSM No. 10366.

Release of the 45' Reserved Right-Of-Way located along the south property line of Parcel 2 of CSM No. 10366; and

WHEREAS, the City Engineering Department and City Planner have reviewed the proposed release of a 45' Reserved Right-of-Way restriction and recommended approval thereof to the Common Council; and

WHEREAS, the 45' Reserved Right-of-Way has been determined to no longer be needed considering the developments of the subject properties and adjacent parcels to the east; and

WHEREAS, the previously recorded 45' Reserved Right-of-Way restriction described above are located upon Certified Survey Map No. 10366, which properties are legally described as follows:

Commencing at the Southwest corner of the Northwest $\frac{1}{4}$ of said Section; thence N00°20'35"W along the West line of the Northwest $\frac{1}{4}$ of said Section 671.90 feet; thence N89°36'25"E 430.02 feet to the Northwest corner of Parcel 2 of said Certified Survey Map; thence S00°17'10"E along the West line of Parcel 2 of said Certified Survey Map 155.00 feet to the point of beginning; thence N 89°36'25"E along the South right-of-way line of Highfield Drive; thence N44°14'24" E along the Southeasterly right-of-way line of said Drive 116.99 feet; thence N66°55'27"E along the DRAFT Southeasterly right-of-way line of said Drive 30.47 feet; thence N89°36'25"E along the South right-of-way line of said Drive 268.82 feet to a point on the East line of Parcel 2 of said Certified Survey Map; thence S00°17'10"E along the East line of Parcel 2 of said Certified Survey Map 411.40 feet to the Southeast corner of Parcel 2 of said Certified Survey Map and a point on the North right-of-way line of Capitol Drive; thence S89°44'25"W along the South line of Parcel 2 of said Certified Survey Map and the North right-of-way line of said Drive 474.81 feet to the Southwest corner of Parcel 2 of said Certified Survey Map; thence N00°17'10"W along the West line of Parcel 2 of said Certified Survey Map 315.31 feet to the point of beginning. Containing 4.17 acres of land more or less.

WHEREAS, Wis. Stats section 236.293 provides in part that any restriction placed on platted land by covenant, grant of easement or in any other manner, which was required by a public body vests in the public body the right to enforce the restriction at law or in equity and that the restriction may be released or waived in writing by the public body having the right of enforcement; and

WHEREAS, the Common Council has determined that the release, waiver, vacation and revision of the prior recorded 45' Reserved Right-of-Way is reasonable as it is no longer necessary for the orderly development of the subject properties or appropriate following the approval of the aforesaid site and building development plans submitted by Quattro Development, LLC.

NOW, THEREFORE, BE IT RESOLVED, by the Mayor and Common Council of the City of Pewaukee, Wisconsin, that the 45' Reserved Right-of-Way upon Parcels 1 and 2 of Certified Survey Map No. 10366, recorded April 13, 2007 in Volume 98 of Certified Survey Maps on Pages 03 to 209, inclusive as Document No. 3472291 with the Office of the Register of Deeds for Waukesha County, Wisconsin, be and the same are hereby waived and released. BE IT

FURTHER RESOLVED that the City Clerk is hereby directed to record this Resolution in the Office of the Register of Deeds for Waukesha County, Wisconsin following the approval of the proposed Quattro Development, LLC Conditional Use and Site and Building Plan Review applications.

Passed and adopted this 15th day of January, 2024.

FOR THE COMMON COUNCIL OF THE CITY OF
PEWAUKEE, WAUKESHA COUNTY, WISCONSIN

Steve Bierce, Mayor

ATTEST:

Kelly Tarczewski, Clerk/Treasurer

**A RESOLUTION AUTHORIZING THE RELEASE, WAIVER, VACATION AND
REVISION OF A FORTY-FIVE FOOT RESERVED RIGHT-OF-WAY RESTRICTION
UPON LOTS 1 AND 2 OF CERTIFIED SURVEY MAP NO. 10366 FOR PROPERTIES
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Passed and adopted this 5th day of September, 2023.

FOR THE COMMON COUNCIL OF THE CITY OF
PEWAUKEE, WAUKESHA COUNTY, WISCONSIN

Steve Bierce, Mayor

ATTEST:

Kelly Tarczewski, Clerk/Treasurer

WAUKESHA COUNTY

CITY OF PEWAUKEE

STATE OF WISCONSIN

RESOLUTION NO. __-__

A RESOLUTION AUTHORIZING THE RELEASE, WAIVER, VACATION AND REVISION OF A FORTY-FIVE FOOT RESERVED RIGHT-OF-WAY RESTRICTION UPON LOTS 1 AND 2 OF CERTIFIED SURVEY MAP NO. 10366 FOR PROPERTIES LOCATED AT APPROXIMATELY N35W23877 HIGHFIELD COURT BEARING TAX KEY NOS. 0906993 AND 0906994

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Passed and adopted this ____th day of _____, 2023.

FOR THE COMMON COUNCIL OF THE CITY OF
PEWAUKEE, WAUKESHA COUNTY, WISCONSIN

ATTEST:

Steve Bierce, Mayor

Kelly Tarczewski, Clerk/Treasurer



1100 Jorie Blvd. • Suite 140
Oak Brook, IL 60523
(630) 870-1921

January 26, 2023

Mr. Nick Fuchs
Planner & Community Development Director
City of Pewaukee
W240N3065 Pewaukee Road
Pewaukee, WI 53072

Re: Release of 45' Reserved Right-of-Way from
Lot 2 of Certified Survey Map No. 10366
Pewaukee, WI

Dear Mr. Fuchs:

This letter is in regards to the existing 45' reserved right-of-way along the southern property line of Lot 2 of Certified Survey Map No. 13066.

Per our prior conversations, we have confirmed with the Wisconsin DOT that this was not a DOT requirement, but rather a City of Pewaukee requirement. In reviewing the aerial maps of this area, it appears that the lots adjacent to this property have encroached into the reserved right-of-way leading us to believe that the City has no plans to develop within this area. To develop Lot 2 to its full potential, we respectfully request that the City vacate / discontinue this future right-of-way from Lot 2.

Please review and let us know if the City would be open to this request.

If you have any further questions regarding this matter, please feel free to contact me at (630) 870-1921.

Sincerely,

Quattro Development, LLC

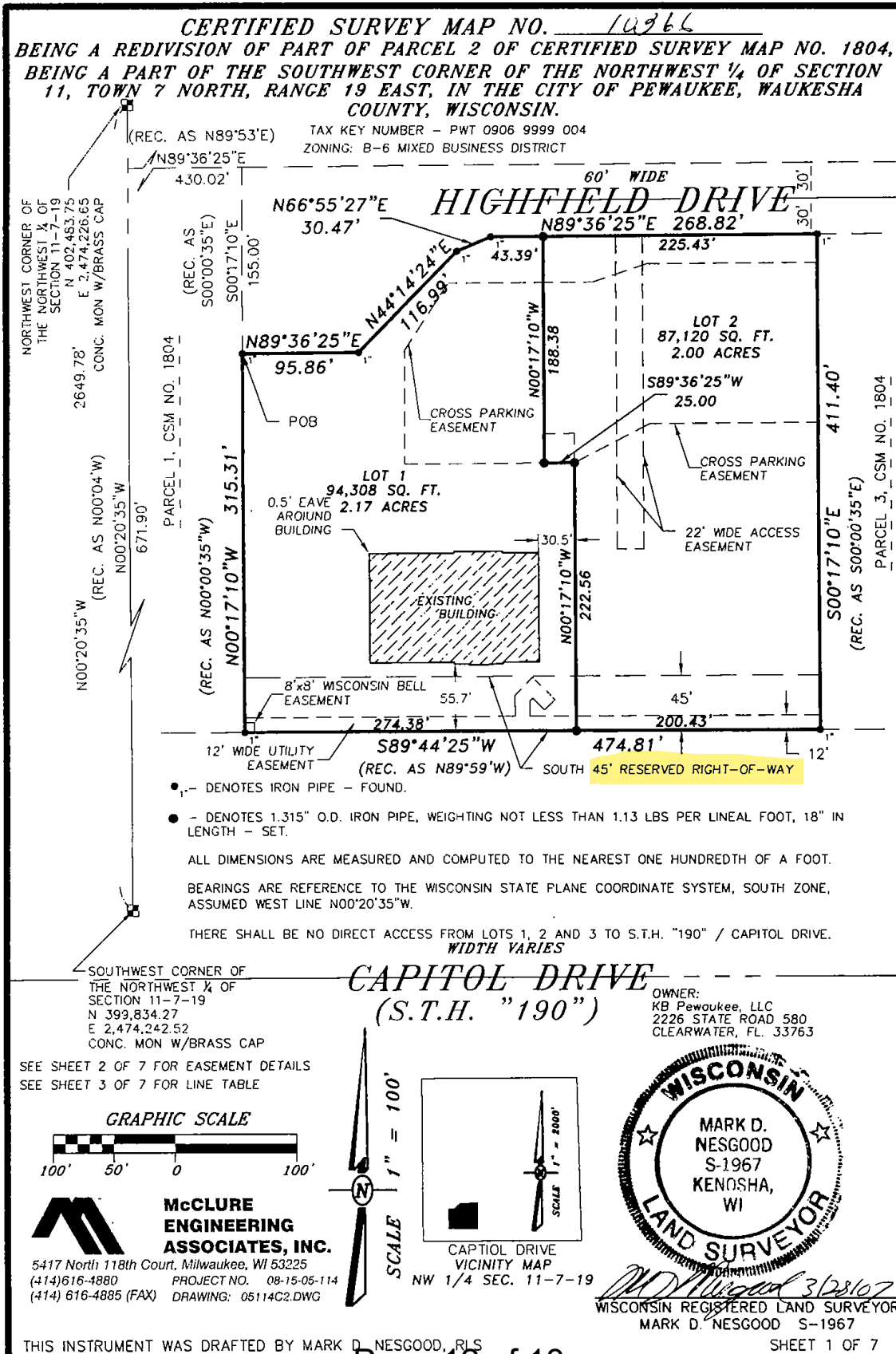
A handwritten signature in blue ink, appearing to read "B. Dahlman", is written over a horizontal line.

Brett J. Dahlman, P.E.
Director of Construction & Development
brett@quattrodevelopment.com

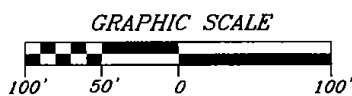
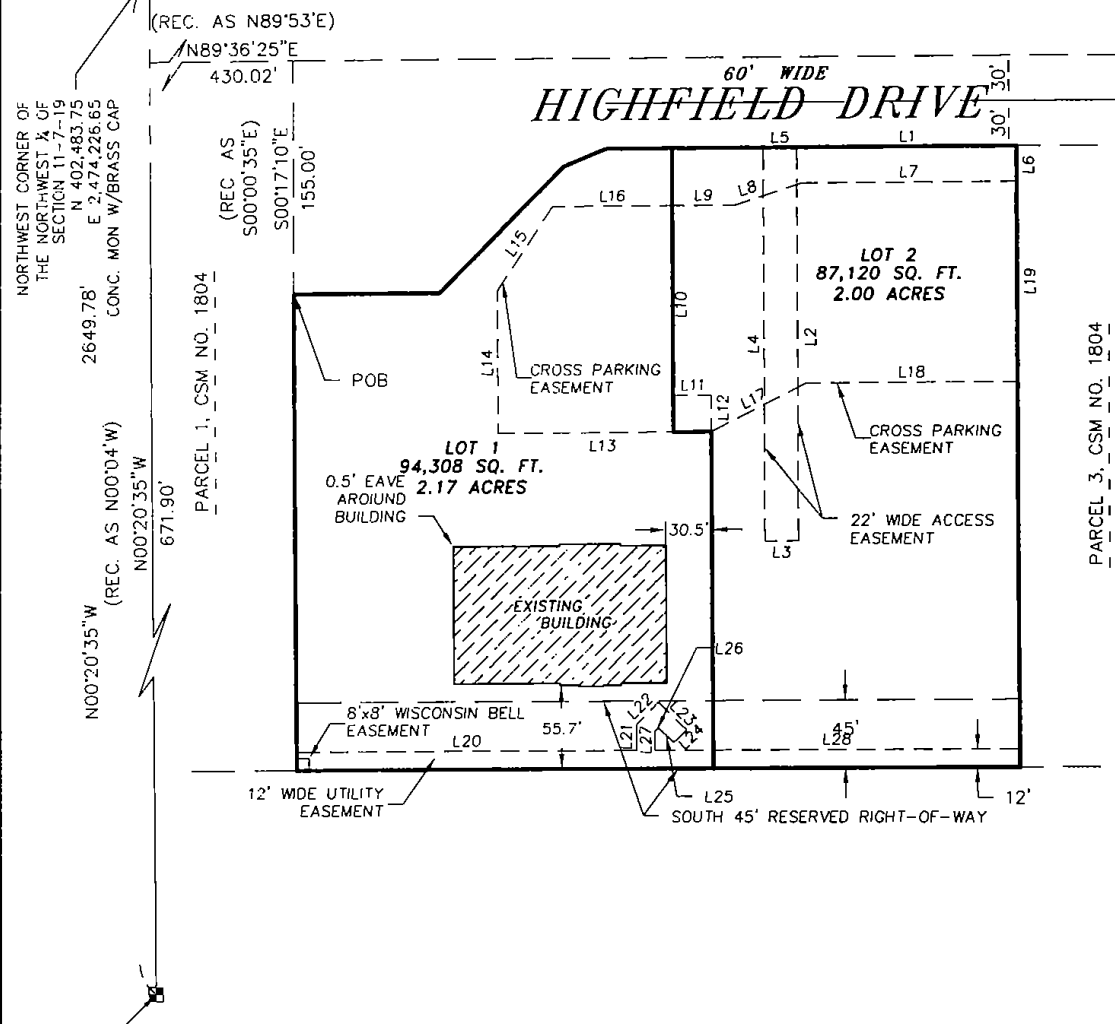
Attachment: CSM 10366



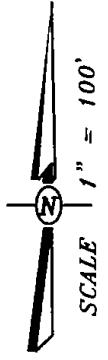
3472291



CERTIFIED SURVEY MAP NO. 10366
BEING A REDIVISION OF PART OF PARCEL 2 OF CERTIFIED SURVEY MAP NO. 1804,
BEING A PART OF THE SOUTHWEST CORNER OF THE NORTHWEST 1/4 OF SECTION
11, TOWN 7 NORTH, RANGE 19 EAST, IN THE CITY OF PEWAUKEE, WAUKESHA
COUNTY, WISCONSIN.



McCLURE ENGINEERING ASSOCIATES, INC.
 5417 North 118th Court, Milwaukee, WI 53225
 (414) 616-4880 PROJECT NO. 08-15-05-114
 (414) 616-4885 (FAX) DRAWING: 05114C2.DWG



Mark D. Nesgood 3/28/07
 WISCONSIN REGISTERED LAND SURVEYOR
 MARK D. NESGOOD S-1967

h0p

CERTIFIED SURVEY MAP NO. 10366
BEING A REDIVISION OF PART OF PARCEL 2 OF CERTIFIED SURVEY MAP NO. 1804,
BEING A PART OF THE SOUTHWEST CORNER OF THE NORTHWEST 1/4 OF SECTION
11, TOWN 7 NORTH, RANGE 19 EAST, IN THE CITY OF PEWAUKEE, WAUKESHA
COUNTY, WISCONSIN.

LINE TABLE		
LINE	BEARING	DISTANCE
L1	S89°36'25"W	143.52'
L2	S00°16'37"E	260.80'
L3	S89°43'25"W	22.00'
L4	N00°16'37"W	260.75'
L5	S89°36'25"W	22.00'
L6	S00°17'10"E	23.68'
L7	S89°36'25"W	140.49'
L8	S71°16'14"W	46.32'
L9	S89°36'25"W	41.00'
L10	S00°17'10"E	125.82'
L11	N89°36'25"E	25.00'
L12	S00°17'10"E	24.30'
L13	S89°36'25"W	140.49'
L14	N00°17'10"W	94.45'
L15	N32°59'24"E	66.67'
L16	N89°36'25"E	78.91'
L17	N62°29'07"E	70.01'
L18	N89°36'25"E	138.18'
L19	N00°17'10"W	132.78'
L20	N89°44'25"E	223.97'
L21	N00°32'10"W	17.46'
L22	N44°27'51"E	20.49'
L23	S45°32'09"E	26.00'
L24	S44°27'51"W	12.00'
L25	N45°32'09"W	14.00'
L26	S44°27'51"W	3.51'
L27	S00°32'10"E	12.54'
L28	N89°44'25"E	238.84'



Mark D. Nesgood 3/14/07
 WISCONSIN REGISTERED LAND SURVEYOR
 MARK D. NESGOOD S-1967

265

CERTIFIED SURVEY MAP NO. 10366

BEING A REDIVISION OF A PART OF PARCEL 2 OF CERTIFIED SURVEY MAP NO. 1804, BEING A PART OF THE SOUTHWEST $\frac{1}{4}$ OF THE NORTHWEST $\frac{1}{4}$ OF SECTION 11, TOWN 7 NORTH, RANGE 19 EAST, IN THE CITY OF PEWAUKEE, WAUKESHA COUNTY, WISCONSIN.

SURVEYOR'S CERTIFICATE

STATE OF WISCONSIN)
MILWAUKEE COUNTY) ^{SS}

I, Mark D. Nesgood, Registered Land Surveyor, do hereby certify that I have surveyed, divided and mapped a Redivision of a part of Parcel 2 of Certified Survey Map No. 1804, being a part of the Southwest $\frac{1}{4}$ of the Northwest $\frac{1}{4}$ of Section 11, Town 7 North, Range 19 East, City of Pewaukee, Waukesha County, Wisconsin, bounded and described as follows:

Commencing at the Southwest corner of the Northwest $\frac{1}{4}$ of said Section; thence N00°20'35"W along the West line of the Northwest $\frac{1}{4}$ of said Section 671.90 feet; thence N89°36'25"E 430.02 feet to the Northwest corner of Parcel 2 of said Certified Survey Map; thence S00°17'10"E along the West line of Parcel 2 of said Certified Survey Map 155.00 feet to the point of beginning; thence N89°36'25"E along the South right-of-way line of Highfield Drive; thence N44°14'24"E along the Southeasterly right-of-way line of said Drive 116.99 feet; thence N66°55'27"E along the Southeasterly right-of-way line of said Drive 30.47 feet; thence N89°36'25"E along the South right-of-way line of said Drive 268.82 feet to a point on the East line of Parcel 2 of said Certified Survey Map; thence S00°17'10"E along the East line of Parcel 2 of said Certified Survey Map 411.40 feet to the Southeast corner of Parcel 2 of said Certified Survey Map and a point on the North right-of-way line of Capitol Drive; thence S89°44'25"W along the South line of Parcel 2 of said Certified Survey Map and the North right-of-way line of said Drive 474.81 feet to the Southwest corner of Parcel 2 of said Certified Survey Map; thence N00°17'10"W along the West line of Parcel 2 of said Certified Survey Map 315.31 feet to the point of beginning. Containing 4.17 acres of land more or less.

THAT I have made such survey, land division and map by the direction of KB Pewaukee, LLC, a Florida limited liability company, owner of said land.

THAT such map is a correct representation of all exterior boundaries and of the land surveyed and the land division thereof made.

THAT I have fully complied with the provisions of Chapter 236 of the Wisconsin State Statutes and the regulations of the City of Pewaukee in surveying, dividing and mapping the same.

Dated this 28th day of March, 2007




Wisconsin Registered Land Surveyor
Mark D. Nesgood S-1967

908

CERTIFIED SURVEY MAP NO. 10366

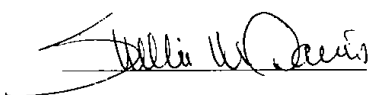
BEING A REDIVISION OF A PART OF PARCEL 2 OF CERTIFIED SURVEY MAP NO. 1804, BEING A PART OF THE SOUTHWEST ¼ OF THE NORTHWEST ¼ OF SECTION 11, TOWN 7 NORTH, RANGE 19 EAST, IN THE CITY OF PEWAUKEE, WAUKESHA COUNTY, WISCONSIN.

OWNERS CERTIFICATE

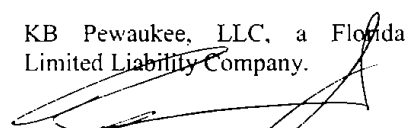
As Owner, I do hereby certify that I have caused the land described on this plat to be surveyed, divided and mapped as represented on this plat. We also certify that this Certified Survey Map is Required to be submitted to the following for approval: City of Pewaukee.

WITNESS the hand and seal of said Owner this 29th day of March, 2007.

IN THE PRESENCE OF:



KB Pewaukee, LLC, a Florida
Limited Liability Company.



_____, Member
Robert E. Schmidt, Jr.
Managing Member

STATE OF Florida)
Pinellas COUNTY) SS

PERSONALLY came before me this 29 day of March, 2007, the above named Robert E. Schmidt Member of KB Pewaukee, LLC a Florida Limited Liability Company, to me known to be the person who executed the foregoing instruction and acknowledged the same.



Amanda D. Norwick
Commission # DD641050
Expires: FEB. 15, 2011
WWW.AARONNOTARY.COM


Notary Public, State of Florida
My commission expires Feb. 15, 2011



THIS INSTRUMENT WAS DRAFTED BY MARK D. NESGOOD, R.L.S.

SHEET 5 OF 7

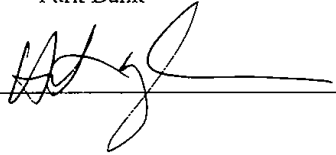
CERTIFIED SURVEY MAP NO. 10366

BEING A REDIVISION OF A PART OF PARCEL 2 OF CERTIFIED SURVEY MAP NO. 1804, BEING A PART OF THE SOUTHWEST ¼ OF THE NORTHWEST ¼ OF SECTION 11, TOWN 7 NORTH, RANGE 19 EAST, IN THE CITY OF PEWAUKEE, WAUKESHA COUNTY, WISCONSIN.

CONSENT OF CORPORATE MORTGAGEE

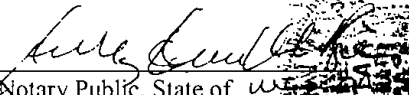
Park Bank, mortgagee of the above described land, does hereby consent to the surveying, dividing and mapping of the land described on this map, and does hereby consent to the foregoing owner's certificate.


Park Bank



STATE OF _____)
COUNTY) ss

Personally came before me this 2nd day of APRIL, 2007, HEATHER BAYLOR of the above named Corporation, to be known to be the person who executed the foregoing instrument, and to me known to be such VICE PRESIDENT of said Corporation, and acknowledged that he executed the foregoing instrument as such officer as the deed of said corporation, by its authority.


Notary Public, State of WISCONSIN
My Commission expires 4-15-2010



THIS INSTRUMENT WAS DRAFTED BY MARK D. NESGOOD, R.L.S.

SHEET 6 OF 7

CERTIFIED SURVEY MAP NO. 10366

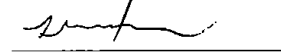
BEING A REDIVISION OF A PART OF PARCEL 2 OF CERTIFIED SURVEY MAP NO. 1804, BEING A PART OF THE SOUTHWEST ¼ OF THE NORTHWEST ¼ OF SECTION 11, TOWN 7 NORTH, RANGE 19 EAST, IN THE CITY OF PEWAUKEE, WAUKESHA COUNTY, WISCONSIN.

CITY PLAN COMMISSION APPROVAL:

Approved by the Plan Commission, City of Pewaukee, this 15th day of March, 2007.



Scott Klein – Chairman



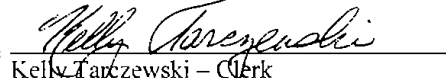
David Mokros – Secretary

CITY COMMON COUNCIL APPROVAL:

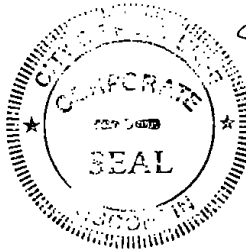
Approved by the Common Council, City of Pewaukee, this 19th day of March, 2007.



Scott Klein – Mayor



Kelly Tarczewski – Clerk



Vol 48 PAGE 203-209
3472291

REGISTER'S OFFICE
WAUKESHA COUNTY, WI
RECORDED ON

04-13-2007 11:51 AM

MICHAEL J. HASSLINGER
REGISTER OF DEEDS

REC. FEE: 16.00
REC. FEE-CD: 5.00
REC. FEE-ST: 2.00
TRAN. FEE:
TRAN. FEE-STATE:
PAGES: 7



THIS INSTRUMENT WAS DRAFTED BY MARK D. NESGOOD, R.L.S.

SHEET 7 OF 7

508

**CITY OF PEWAUKEE
COMMON COUNCIL AGENDA ITEM 7.**

DATE: January 15, 2024

DEPARTMENT: Planning

PROVIDED BY: Nick Fuchs

SUBJECT:

Discussion and Possible Action for Junior Cup Golf, Inc. Related to the Conditional Use Permit for Property Located at W229 N2494 Redford Boulevard (PWC 0915-990-001) for the Purpose of Running a Non-Profit Junior Golf and Athletics Center [Fuchs]

BACKGROUND:

At their December 21, 2023, meeting, the Plan Commission unanimously recommended approval of the Conditional Use Permit for Junior Cup Golf, Inc.

FINANCIAL IMPACT:

RECOMMENDED MOTION:

A motion to approve the Conditional Use Permit for Junior Cup Golf Inc. for property located at W229N2494 Redford Boulevard.

ATTACHMENTS:

Description

Junior Cup Golf draft Conditional Use Permit

Junior Cup Golf staff report 12.21.23

Junior Cup Golf plans

**A COVENANT
REGARDING THE ISSUANCE OF A
CONDITIONAL USE PERMIT
BY THE
CITY OF PEWAUKEE**

**TAX KEY NUMBER(S)
OR PARCEL(S) INVOLVED:** **CONDITIONAL USE
PERMIT:**
NO. CUP-23-12-2

PWC 0915990001

LEGAL DESCRIPTION:

PARCEL A CERT SURV 1242 VOL 8/182 1.934 AC PT SW1/4 SEC
13 T7N R19E DOC# 3478036

**PERSON(S), AGENT(S) OR CORPORATION(S) PETITIONING
FOR PERMIT:**

Junior Cup Golf

Recording area

Name & Return Address

City of Pewaukee
W240N3065 Pewaukee Rd
Pewaukee, WI 53072

WHEREAS, It is understood by all parties to this covenant that Section 62.23 of WIS. Statutes prescribes the legal basis for the granting of a conditional use permit by a City and Chapter 17 of the City Codes and Ordinances provides for the issuance of such permits as well as the standards by which all such uses will be measured; and,

WHEREAS, The City Plan Commission has held a meeting on December 21, 2023; has reviewed the various elements of the petitioner's proposal; and has recommended that a Conditional Use Permit be granted to the above-named petitioner for the property/parcel identified above; and,

WHEREAS, The City Common Council, at their meeting held on January 15, 2024 concurred with the Plan Commission.

NOW, THEREFORE, let it be known that the City Common Council by its action on January 15, 2024 has, hereby, granted a Conditional Use Permit for the following use(s):

Running a non-profit junior golf and athletics center.

FURTHER, such approved use of the above designated parcel(s) are hereby allowed based on the following conditions being continually met:

No conditions.

The parties hereto, namely the City of Pewaukee and the Equitable Owner of the property for which this conditional use has been sought, set their signatures or the signatures of their representatives below, thereby agreeing to the provisions and conditions set forth in this covenant.

Attest:

Signature of equitable owner

Date

Kelly Tarczewski
City Clerk

Steve Bierce
Mayor, City of Pewaukee

Date

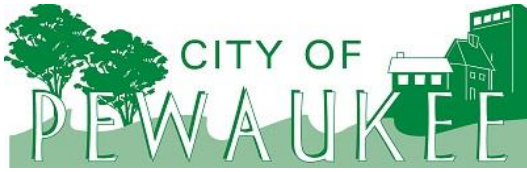
State of Wisconsin
County of Waukesha

Signed or attested before me on _____, 2024 by Steve Bierce, Mayor and Kelly Tarczewski, Clerk.

(Seal)

Ami Hurd
My Commission expires _____

This instrument was drafted by Ami Hurd, Deputy Clerk



Office of the Planner & Community Development Director
W240 N3065 Pewaukee Road
Pewaukee, Wisconsin 53072
Phone (262) 691-0770 Fax (262) 691-1798
fuchs@pewaukee.wi.us

REPORT TO THE PLAN COMMISSION

Meeting of December 21, 2023

Date: December 6, 2023

Project Name: Junior Cup Golf Conditional Use

Project Address/Tax Key No.: W229N2494 Redford Boulevard/PWC 0915990001

Applicant: Nathan Dosch

Property Owner: OHM REDFORD LLC

Current Zoning: M-2 Limited Industrial District

2050 Land Use Map Designation: Manufacturing/Fabrication/Warehousing

Use of Surrounding Properties: Industrial zoned properties to the north, south, east, and west and the Waukesha Gun Club to the west

Project Description

The applicant, Junior Cup Golf, filed a Conditional Use Permit requesting approval to locate within an existing 7,350 square foot tenant space at W229N2494 Redford Boulevard.

The property is zoned M-2 Limited Industrial District and designated as Manufacturing/Fabrication/Warehousing on the City's 2035 Future Land Use Map.

Junior Cup Golf offers team golf to educate, train, and develop young golfers as well as other instructional programming. Usage and hours of operations may vary throughout the year. The facility will generally be open during the day and host evening practices as well. Hours of operation are anticipated from 9:00 a.m. to 9:00 p.m. The facility is not open to the general public.

The M-2 District allows, as a Conditional Use, "Day care operations, wellness centers, physical fitness and training centers." As such the proposed use may be allowed within the M-2 District as a Conditional Use.

The applicant is not proposing any exterior site or building modifications.

Recommendation

Although the proposed use is not industrial in nature, it is allowed as a Conditional Use per the M-2 District, and the use does not have any anticipated adverse impacts on the surrounding properties. Therefore, approval of the Conditional Use Applications is recommended.

Project Narrative and Use Plan for Junior Cup Golf, Inc. Space
at
W229 N2494 Redford Blvd, Unit 300

Junior Cup Golf, Inc. (“JCG”) has entered into a lease agreement with OHM Redford, LLC (“Property Owner”) commencing on October 4, 2023 for approximately 7,350 square feet of commercial space, known as Unit 300, (“Leased Space”) in the property described as:

PARCEL “A” CERTIFIED SURVEY MAP NO. 1242 RECORDED ON AUGUST 26, 1970 IN VOLUME 8 OF CERTIFIED SURVEY MAPS, ON PAGES 182, 183 AND 184, AS DOCUMENT NO. 767649, BEING PART OF THE SOUTHWEST ¼ OF SECTION 13, TOWNSHIP 7 NORTH, RANGE 19 EAST, TOWN OF PEWAUKEE, COUNTY OF WAUKESHA, STATE OF WISCONSIN.

Proposed Use:

According to the property tax records the property is currently zoned as COMMERCIAL / MERCANTILE. JCG intends to use the Leased Space to run a junior golf and athletics training center in furtherance of its non-profit mission, which is to provide opportunities for young athletes to play team golf in a fun environment that promotes education and development. Junior Cup Golf is a 501(c)(3) nonprofit organization that provides opportunities to young athletes aspiring to play competitive golf in high school and fosters fun environments that promote education and development in the game of golf. While the sport of golf is traditionally an individual sport, our organization forms community-based teams of junior golfers to learn, train, and compete against other community-based teams. We currently have teams in the following districts: Sussex Hamilton, Menomonee Falls, Brookfield East, Germantown and Kettle Moraine. Demand has been outstanding and we are exploring opportunities to add additional school districts, as early as next year, including, but hopefully not limited to, Pewaukee.

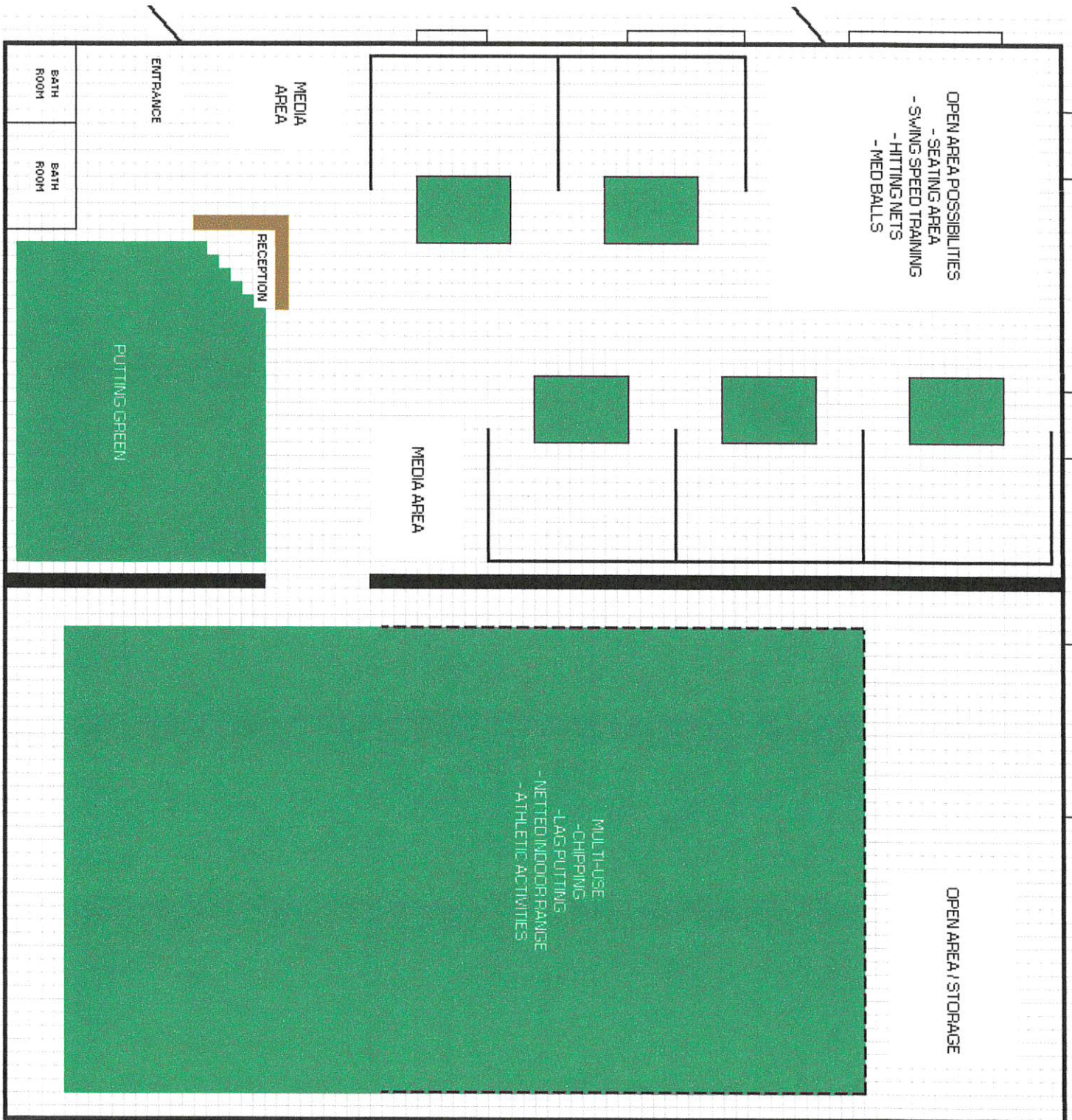
JCG not only educates junior golfers on the fundamentals of golf, but also promotes core values such as patience, respect, integrity, teamwork and sportsmanship. The organization is entirely volunteer organized, funded and run. While for profit, indoor golf facilities have gained in popularity over the last few years, no other non-profit organization or junior focused facility exists like it in the greater metro area or throughout the entire state or region.

JCG will primarily use the facility during the winter months, January to April, for junior golf instruction and practice facilities in the evening hours (5pm to 9pm). The facility is not open to the public and will not be available for walk-in usage. We will not be selling or allowing alcoholic beverages and we will not be providing any food service. The only items contemplated for sale to the participant might be water, soda and small snacks. The facility will be accessible during the day (9am-5pm) and year-round for usage by invitees and other permitted users including PGA professionals conducting instructional programming. The location provides for 10-12 parking spots after 4pm on weeknights and an abundance of parking over the weekend.

With our enrollment of 3rd – 8th graders, the participants are all dropped off and picked up by parents so there will be no demand for excess parking. The landlord and other tenants will continue to operate the remainder of the building in the fashion they do now in accordance with existing zoning. Our request relates simply to the approximately 7,350 square feet located on the north side of the building and known as Unit 300.

Project Plan:

The plan for the space does not involve any structural or mechanical changes to the building. JCG will be using the interior space as depicted on the attached layout and renderings to install 5 golf simulators, a turf putting green and a multipurpose turf area. None of these items will be permanent improvements to the building or Leased Space. JCG estimates the equipment and furnishings to be \$200,000. Many of those projects have already been completed, with the multipurpose turf area completed on December 9, 2023. The entire facility is scheduled to be operational by December 15, 2023, however, we will not launch for our instructional season until January 4, 2024.











**CITY OF PEWAUKEE
COMMON COUNCIL AGENDA ITEM 8.**

DATE: January 15, 2024

DEPARTMENT: Planning

PROVIDED BY: Nick Fuchs

SUBJECT:

PUBLIC HEARING, Discussion and Possible Action for a Comprehensive Master Plan Amendment to Change the Year 2050 Land Use/Transportation Plan Use Designation for the City of Pewaukee for Fox Run Development Partners, LLC for Property Located at N15 W22261 Watertown Road (PWC 0960-987) from Office / Commercial and Floodplains, Lowland and Upland Conservancy, and Other Natural Areas to Manufacturing / Fabrication / Warehousing and Floodplains, Lowland and Upland Conservancy, and Other Natural Areas Which Include the Adoption of **Resolution PC 24-01-03** and **Ordinance 24-01** if approved [Fuchs]

BACKGROUND:

At their December 21, 2023, meeting, the Plan Commission unanimously recommended approval of the subject Comprehensive Master Plan Amendment for property located at N15W22261 Watertown Road.

FINANCIAL IMPACT:

RECOMMENDED MOTION:

A motion to approve the proposed Comprehensive Master Plan Amendment to Change the Year 2050 Land Use/Transportation Plan Use Designation for the City of Pewaukee for Fox Run Development Partners, LLC for Property Located at N15W22261 Watertown Road from Office/Commercial and Floodplains, Lowland and Upland Conservancy, and Other Natural Areas to Manufacturing/Fabrication/Warehousing and Floodplains, Lowland and Upland Conservancy, and Other Natural Areas.

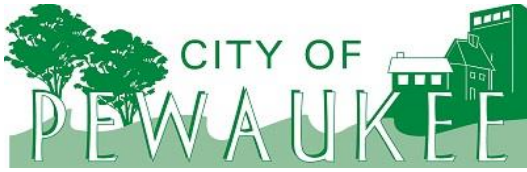
ATTACHMENTS:

Description

Fox Run staff report 12.21.23

Ordinance 24-01

Resolution 24-01-03



Office of the Planner & Community Development Director
W240 N3065 Pewaukee Road
Pewaukee, Wisconsin 53072
Phone (262) 691-0770 Fax (262) 691-1798
fuchs@pewaukee.wi.us

REPORT TO THE PLAN COMMISSION

Meeting of December 21, 2023

Date: December 11, 2023

Project Name: Fox Run Rezoning, Comprehensive Master Plan Amendment, and Site & Building Plan Review

Project Address/Tax Key No.: N15W22261 Watertown Rd./PWC 0960987

Applicant: Irgens Development, LLC

Property Owner: FOX RUN DEVELOPMENT PARTNERS LLC

Current Zoning: Rs-1 Single Family Residential & LC Lowland Conservancy

2050 Land Use Map Designation: Office/Commercial

Use of Surrounding Properties: Watertown Plank Rd to the north

Introduction

The applicant has filed a Rezoning Petition, Comprehensive Master Plan Amendment, and Site & Building Plan Review Application for the development of an industrial flex building located at N15W22261 Watertown Road. The parcel has an area of about 5.3 acres with the northern portion bordering Watertown Road and the southern portion bordering a large parcel zoned A-2 Agricultural District. Currently, the parcel is vacant.

The applicant, Irgens Development, originally filed a Rezoning Petition and Comprehensive Master Plan Amendment that went before the Plan Commission at their November 17th, 2022, meeting. Both applications were tabled at the meeting as the Plan Commission did not want to move forward without detailed site and building plans. The applicant has since filed a Site & Building Plan Review Application, which can be considered in conjunction with the previously submitted Rezoning Petition and Comprehensive Plan Amendment.

Comprehensive Master Plan

The applicant seeks to change the Comprehensive Master Plan Year 2050 Land Use/Transportation Plan map from Office/Commercial and Flood Plains, Lowland & Upland Conservancy and Other Natural Areas to Manufacturing/Fabrication/Warehousing. This is required to provide consistency between the requested zoning and the Comprehensive Master Plan.

As part of the proposed Comprehensive Master Plan Amendment, staff recommends that wetland areas be designated as Flood Plains, Lowland & Upland Conservancy and Other Natural Areas.

Rezoning

The applicant is requesting to rezone the property from Rs-1 Single Family Residential District and LC Lowland Conservancy District to M-2 Limited Industrial District and LC Lowland Conservancy District. Proposed uses in the future will be subject to either a Business Plan of Operation or a Conditional Use Permit, depending upon the allowance of the specific use in the M-2 District.

As part of this rezoning request, staff recommends that the existing LC Lowland Conservancy District on the property be revised to match the field delineated boundaries of the wetlands onsite.

Site and Building Plan Review

The proposed building and associated parking, landscaping, and lighting are further described below.

Project Description/Analysis

Site Plan

The proposed building is approximately 59,500 square feet and includes four loading docks and four future dock positions. Two 12' x 14' overhead doors are also shown on the rear or south elevation. The site includes about 2.36 acres of greenspace, which is nearly 44% of the site.

The access drive from Watertown Road is directly adjacent to the neighbor's existing gravel driveway. The applicant included the note below on the site plan indicating that their driveway would be eliminated if a shared access agreement could be agreed upon in the future with the adjacent landowner.

DRIVEWAY ONTO WATERTOWN ROAD TO BE ABANDONED PROVIDED
ADJACENT LANDOWNER GRANTS PERPETUAL VEHICULAR ACCESS RIGHTS
OVER A TO-BE-CONSTRUCTED SHARED DRIVEWAY. THE SHARED DRIVEWAY
MUST MEET CITY STANDARDS WITH RESPECT TO RIGHT-OF-WAY AND BE IN
SHAPE AND FORM TO HANDLE VEHICULAR AND TRUCK TRAFFIC NEEDS OF
BOTH PARTIES.

Natural Resources

A wetland delineation was completed on September 22, 2022, by the Heartland Ecological Group Inc. Two wetlands and associated wetland setback areas were identified. One wetland is in front of the proposed building and to the southeast of the parking lot. This wetland has an area of approximately 4,800 square feet. The second wetland is located along the south property line. This wetland has an area of about 435 square feet.

Note these areas are part of larger wetland complexes that are primarily located on the adjacent parcel and extend onto the subject property.

The applicant has revised and designed the site plan so not to disturb or impact these protected natural resources.

Parking

The site plan includes 73 proposed car parking spaces, of which 4 are ADA accessible. Additionally, 13 truck trailer stalls are being proposed to the rear of the building.

Lighting

The Lighting Plan consists of building and ground lighting. Light levels are between 0.0 and 0.5 footcandles at the property lines. According to the applicant, all fixtures have the capability to be dimmed.

Note all building and ground lighting is shown with a peak height or mounting height of 20-feet or less, in compliance with the City's Zoning Code.

Landscaping

The Landscape Plan consists of 15 Deciduous Trees, 28 Evergreen Trees, 4 Decorative Trees, and 169 shrubs, plus other grasses and perennials.

Staff suggests plantings be added along the access drive and to the east of the parking lot within the wetland setback area.

Architecture

The building exterior primarily consists of painted precast concrete wall panels of different colors. The building entrance is primarily glass windows with aluminum framing. The building also includes three metal accent canopies.

Signage

A monument sign is shown near the entrance of the site. Signage must comply with standards set forth in Section 17.0700 of the City's Zoning Code and will require separate review and approval by the City Planner as well as a Sign Permit from the Building Services Department, prior to installation.

Utilities/Stormwater management

A storm water management pond is located on the south end of the property. Staff recommends that final grading, erosion control, utilities, and storm water management plans be approved by the Engineering Department prior to any land disturbance.

Recommendation

A motion to recommend approval of the Comprehensive Master Plan Amendment, Rezoning, and Site & Building Plan Review Application, subject to the conditions within this report.

ORDINANCE 24-01

AN ORDINANCE TO AMEND THE CITY OF PEWAUKEE 2035 COMPREHENSIVE MASTER PLAN AND NEIGHBORHOOD PLANS 2015-2050 TO CHANGE THE YEAR 2050 LAND USE/TRANSPORTATION PLAN OF A PROPERTY LOCATED AT N15W22261 WATERTOWN ROAD BEARING TAX KEY NUMBER PWC 0960-987 FROM OFFICE COMMERCIAL AND FLOODPLAINS, LOWLAND & UPLAND CONSERVANCY AND OTHER NATURAL AREAS TO MANUFACTURING/FABRICATION/WAREHOUSING AND FLOODPLAINS, LOWLAND & UPLAND CONSERVANCY AND OTHER NATURAL AREAS

WHEREAS, pursuant to Wis. Stat. §§ 62.23(2) and (3) and 66.1001(4), the City of Pewaukee is authorized to prepare and adopt and to amend a comprehensive plan as defined in Wis. Stat. §§ 66.1001(1)(a) and 66.1001(2); and

WHEREAS, The Applicant, Fox Run Development Partners LLC has applied for an amendment to the Comprehensive Master Plan to change the Year 2050 Land Use/Transportation Plan of a property located at N15 W22261 Watertown Road bearing Tax Key Number PWC 0960-987 from Office Commercial and Floodplains, Lowland & Upland Conservancy and Other Natural Areas to Manufacturing/Fabrication/Warehousing and Floodplains, Lowland & Upland Conservancy and Other Natural Areas; and

WHEREAS, the Plan Commission of the City of Pewaukee by a majority vote of the entire Commission on December 21, 2023, recorded in its minutes, has adopted a resolution recommending to the Common Council the adoption of the Ordinance to Amend the City of Pewaukee Comprehensive Master Plan to change the Year 2050 Land Use/Transportation Plan of a property located at N15 W22261 Watertown Road bearing Tax Key Number PWC 0960-987 from Office Commercial and Floodplains, Lowland & Upland Conservancy and Other Natural Areas to Manufacturing/Fabrication/Warehousing and Floodplains, Lowland & Upland Conservancy and Other Natural Areas; and

WHEREAS, the City of Pewaukee held a public hearing upon this proposed Ordinance, in compliance with the requirements of Wis. Stat. § 66.1001(4)(d); the Common Council having received input from the public at a duly noticed public hearing on January 15, 2024; and

NOW THEREFORE, BE IT RESOLVED, by the Mayor and Common Council of the City of Pewaukee, Wisconsin, do ordain as follows:

SECTION 1: Adoption

The City of Pewaukee Comprehensive Master Plan is hereby amended to change the Year 2050 Land Use/Transportation Plan of a property located at N15W22261 Watertown Road bearing Tax Key Number PWC 0960-987 from Office Commercial and Floodplains, Lowland & Upland Conservancy and Other Natural Areas to Manufacturing/Fabrication/Warehousing and Floodplains, Lowland & Upland Conservancy and Other Natural Areas.

SECTION 2: Document Transmittal

The City Common Council hereby directs the City Clerk/Treasurer to transmit a copy of the amendment as well as a signed copy of both the **Resolution PC 24-01-03** and this **Ordinance 24-01** to the Wisconsin Department of Administration, the Southeastern Wisconsin Regional Planning Commission, the Waukesha Park and Land Use Department, the Pewaukee Public Library and to each town, village and city that abuts the City of Pewaukee.

SECTION 3: Severability

The several sections and portions of this ordinance are declared to be severable. If any section or portion thereof shall be declared by a decision of a court of competent jurisdiction to be invalid, unlawful, or unenforceable, such decision shall apply only to the specific section or portion thereof directly specified in the decision, and shall not affect the validity of all other provisions, sections, or portions thereof of the ordinance. The remainder of the ordinance shall remain in full force and effect. Any other ordinances whose terms are in conflict with the provisions of this ordinance are hereby repealed as to those terms that conflict.

SECTION 4: Effective Date

This ordinance shall take effect immediately upon passage and posting of publication as provided by law.

Passed and adopted this 15th day of January, 2024.

FOR THE COMMON COUNCIL OF THE CITY OF
PEWAUKEE, WAUKESHA COUNTY, WISCONSIN

ATTEST:

Steve Bierce, Mayor

Kelly Tarczewski, Clerk/Treasurer

RESOLUTION NO. PC-24-01-03

**A RESOLUTION RECOMMENDING THE ADOPTION OF AN ORDINANCE TO
AMEND THE CITY OF PEWAUKEE 2035 COMPREHENSIVE MASTER PLAN AND
NEIGHBORHOOD PLANS 2015-2050 TO CHANGE THE YEAR 2050 LAND
USE/TRANSPORTATION PLAN OF A PROPERTY LOCATED AT N15 W22261
WATERTOWN ROAD BEARING TAX KEY NUMBER PWC 0960-987 FROM OFFICE
COMMERCIAL AND FLOODPLAINS, LOWLAND & UPLAND CONSERVANCY AND
OTHER NATURAL AREAS TO
MANUFACTURING/FABRICATION/WAREHOUSING AND FLOODPLAINS,
LOWLAND & UPLAND CONSERVANCY AND OTHER NATURAL AREAS,
PURSUANT TO WIS. STAT. § 66.1001(4)(b)**

WHEREAS, pursuant to Wis. Stat. §§ 62.23(2) and (3) and 66.1001(4), the City of Pewaukee is authorized to prepare and adopt and to amend a comprehensive plan as defined in Wis. Stat. §§ 66.1001(1)(a) and 66.1001(2); and

WHEREAS, pursuant to Wis. Stat. § 66.1001(4)(b), the Plan Commission may recommend the amendment of the Comprehensive Master Plan to the Common Council by adopting a resolution by a majority vote of the entire Commission, which vote shall be recorded in the official minutes of the Plan Commission; and

WHEREAS, The Applicant, Fox Run Development Partners LLC has applied for an amendment to the Comprehensive Master Plan to change the Year 2050 Land Use/Transportation Plan of a property located at N15 W22261 Watertown Road bearing Tax Key Number PWC 0960-987 from Office Commercial and Floodplains, Lowland & Upland Conservancy and Other Natural Areas to Manufacturing/Fabrication/Warehousing and Floodplains, Lowland & Upland Conservancy and Other Natural Areas, more particularly described as follows:

PT SE1/4 SEC 24 T7N R19E; COM E1/4 COR; N88°10'W 1340.88 FT; S 786.45 FT THE BGN; S 363.21 FT; S70°03'W 642.89 FT; N19°57'W 17.38 FT; N87°55'W 136.08 FT; N04°30'E 13.76 FT; N43°56'E 656.69 FT; N34°01'E 219.87 FT; S57°15'E 197.98 FT TO BGN :: DOC# 3535686

WHEREAS, the Plan Commission having determined that the proposed amendment in form and content as presented to the Commission on December 21, 2023, is consistent with the Comprehensive Master Plan's goals, objectives and policies and in proper form and content for adoption by the Common Council as an amendment to the Comprehensive Master Plan, subject to such modifications the Common Council may consider reasonable and necessary, following public hearing, in order to protect and promote the health, safety and welfare of the City of Pewaukee.

NOW, THEREFORE, BE IT RESOLVED, by the Plan Commission of the City of Pewaukee, Wisconsin, that the application for and the proposed ordinance to amend the City of Pewaukee Comprehensive Master Plan to change the Year 2050 Land Use/Transportation Plan of a property located at N15 W22261 Watertown Road from Office Commercial and Floodplains, Lowland & Upland Conservancy and Other Natural Areas to Manufacturing/Fabrication/Warehousing and Floodplains, Lowland & Upland Conservancy and Other Natural Areas, such property bearing Tax Key Number PWC 0960-987, be and the same is hereby recommended for adoption and incorporation into the Comprehensive Master Plan by the Common Council:

Passed and adopted this 21st day of December, 2023.

FOR THE PLAN COMMISSION OF THE CITY OF
PEWAUKEE, WAUKESHA COUNTY, WISCONSIN

ATTEST:

Steve Bierce, Mayor

Colleen Brown, Plan Commission Secretary

**CITY OF PEWAUKEE
COMMON COUNCIL AGENDA ITEM 9.**

DATE: January 15, 2024

DEPARTMENT: Planning

PROVIDED BY: Nick Fuchs

SUBJECT:

Discussion and Possible Action Regarding **Ordinance 24-02** Rezoning the Property Located at N15 W22261 Watertown Road (PWC 0960-987) from Rs-1 Single-Family Residential and LC Lowland Conservancy to M-2 Limited Industrial and LC Lowland Conservancy for the Purpose of Developing an Industrial Flex Building as Requested by Fox Run Development Partners, LLC [Fuchs]

BACKGROUND:

At their December 21, 2023, meeting, the Plan Commission unanimously recommended approval of the subject rezoning for property located at N15W22261 Watertown Road.

FINANCIAL IMPACT:

RECOMMENDED MOTION:

A motion to approve the proposed rezoning of property located at N15W22261 Watertown Road from Rs-1 Single-Family Residential and LC Lowland Conservancy to M-2 Limited Industrial and LC Lowland Conservancy.

ATTACHMENTS:

Description

Ordinance 24-02 Fox Run Rezone

Rezoning Map

Fox Run Site & Building Plans

STATE OF WISCONSIN

CITY OF PEWAUKEE

WAUKESHA COUNTY

ORDINANCE 24-02

**TO AMEND THE ZONING MAP OF
THE CITY OF PEWAUKEE, WAUKESHA COUNTY, WISCONSIN**

The Common Council of the City of Pewaukee, Waukesha County, Wisconsin do ordain that the Zoning Map of the City of Pewaukee, Wisconsin is hereby amended to change the zoning classification of the property described below as follows:

FROM: **RS-1 Single Family Residential & LC Lowland Conservancy**

TO: **M-2 Limited Industrial & LC Lowland Conservancy**

SECTION 1 - DESCRIPTION

Common Description:

PT SE1/4 SEC 24 T7N R19E; COM E1/4 COR; N88°10'W 1340.88 FT; S 786.45 FT THE BGN; S 363.21 FT; S70°03'W 642.89 FT; N19°57'W 17.38 FT; N87°55'W 136.08 FT; N04°30'E 13.76 FT; N43°56'E 656.69 FT; N34°01'E 219.87 FT; S57°15'E 197.98 FT TO BGN:: DOC# 3535686

Tax Key Number: PWC 0960-987

Property Address: N15 W22261 Watertown Road

This Amendment to the Zoning Map (Change in zoning) is being proposed for the purpose of: developing a light industrial/flex building.

SECTION 2 –SEVERABILITY

The several sections of this Ordinance are declared to be severable. If any section shall be declared by a decision of a court of competent jurisdiction to be invalid, such decision shall not affect the validity of the other provisions of the Ordinance.

SECTION 3 - ACTION

This Ordinance shall take effect upon passage and posting.

Dated this 15th day of January, 2024.

COMMON COUNCIL OF THE CITY OF PEWAUKEE

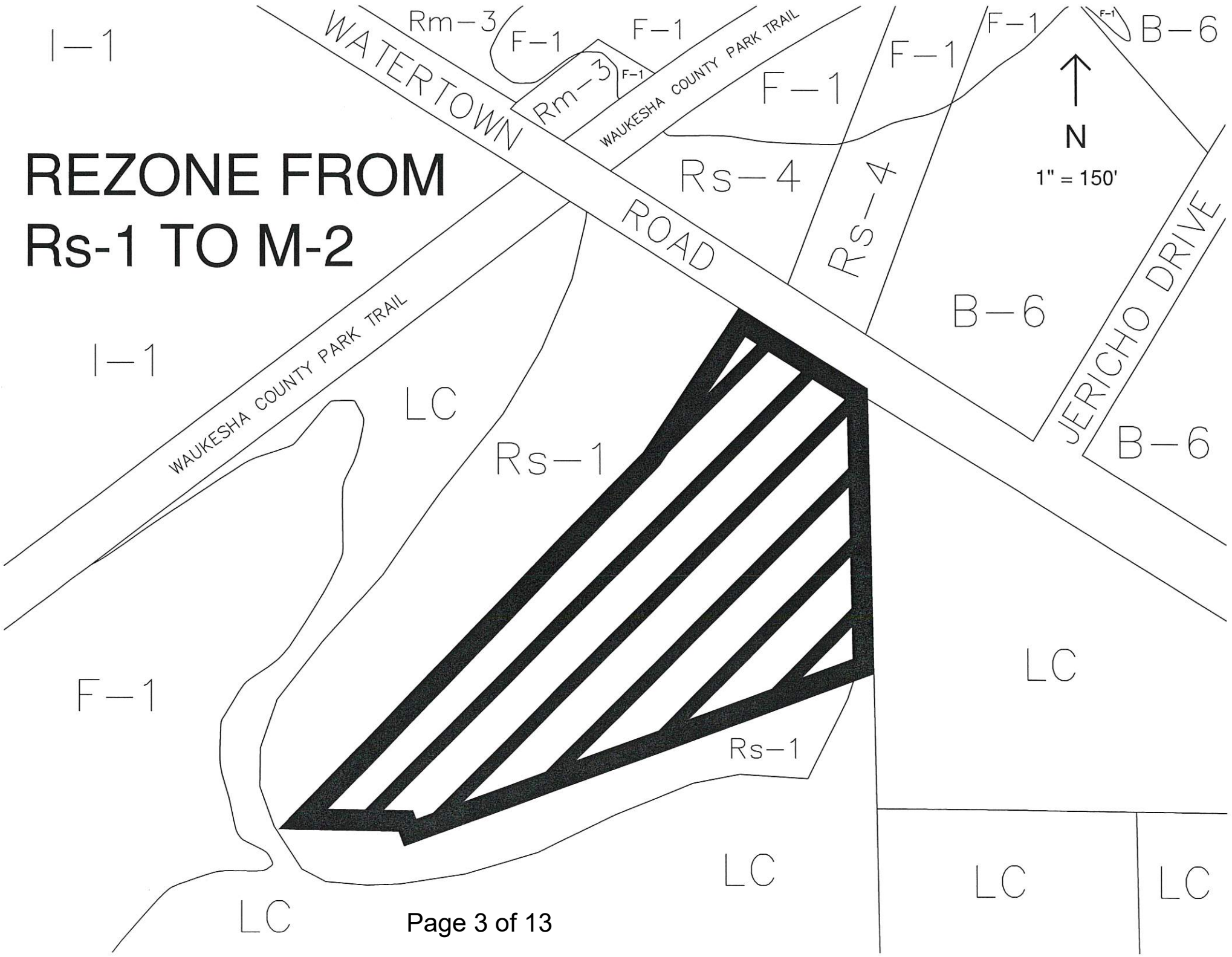
WAUKESHA COUNTY, WISCONSIN

Attest:

Steve Bierce, Mayor

Kelly Tarczewski, Clerk/Treasurer

REZONE FROM Rs-1 TO M-2





**PETITION FOR A REZONING DISTRICT MAP AMENDMENT
AND
AMENDMENT TO THE COMPREHENSIVE MASTER PLAN
NARRATIVE**

Subject Property: 5.3 Acre Vacant site at N15 W22261 Watertown Road (Tax Key 0960.987)
Current Zoning: RS-1, Single Family Residential
Proposed Zoning: M-2, Limited Industrial District

The property owner, Fox Run Development Partners, LLC, an affiliate of Irgens Partners, LLC (“Irgens”), seeks to rezone the site from RS-1, Single Family Residential to M-2 Limited Industrial District. Irgens plans to develop and improve the site with a single-story, approximately 60,000 square foot, flexible light industrial center with either a build-to-suit single user or a multi-tenant building with 2 compatible users. We believe that there is a market for light manufacturing and fabrication, with related warehouse and office space at this location due to its proximity to interstate 94 and with a limited supply of available sites to serve the market for small to mid-size users.

The current zoning for single-family would restrict development of the site. It is highly unlikely that a single-family developer would find it financially feasible to pay the market rate for the site and speculate on the appeal of the site for home ownership between the GE training center, the multi-family use across Watertown Road, and the nearby industrial and distribution users to the east.

The M-2 Limited Industrial District provides the flexibility for Irgens to attract a suitable user for this location and create property values that are commensurate with the location and proximity to the interstate. It is our intention, should we be granted the rezoning request, to aggressively market the site and submit an appealing set of final site development and building plans for review by the City Plan Commission and Council in the first half of 2023.

The Comprehensive Master Plan for the City of Pewaukee calls for office development at this site. The site has remained vacant for many years without an office user coming forward for this location despite marketing efforts and listings. To our knowledge, adjacent locations have not attracted much interest in office use either.

It is our intention that the façade of the building to be developed facing Watertown Road will have architectural attributes of office use, while providing the practical efficiencies of light industrial in a balanced way. The receiving and shipping functions will be located to the south of the building.





IRGENS
PEWAUKEE, WISCONSIN

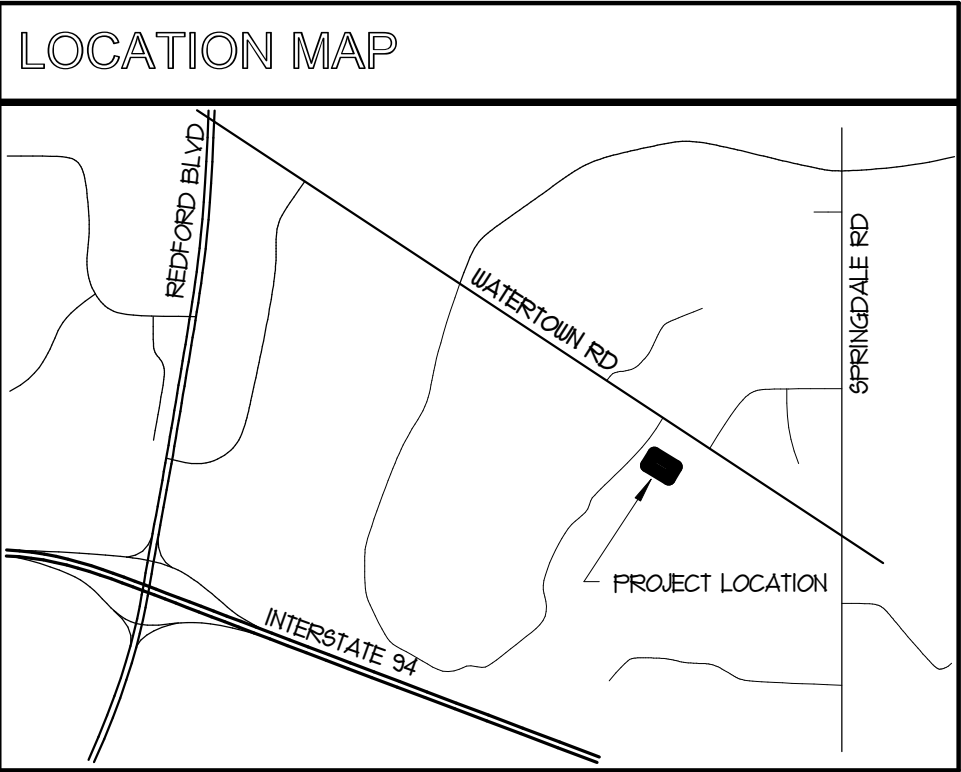
FOX RUN CENTER

ABBREVIATIONS			
ALT	ALTERNATE	HM	HOLLOW METAL
A/E	ARCHITECT/ENGINEER	HT	HEIGHT
AF	ABOVE FINISH FLOOR	HW	HOT WATER
ALUM	ALUMINUM	INT	INTERIOR
BD	BOARD	INSUL	INSULATION
B/O	BOTTOM OF	JT	JOINT
CPT	CARPET	LAV	LAVATORY
CL	CENTER LINE	MFR	MANUFACTURER
CLG	CEILING	MIN	MINIMUM
CJ	CONTROL JOINT	MO	MASONRY OPENING
CMU	CONCRETE MASONRY UNIT	MTL	METAL
CONC	CONCRETE	NIC	NOT IN CONTRACT
CONT	CONTINUOUS	NTS	NOT TO SCALE
CO	CLEAN OUT	OC	ON CENTER
CW	COLD WATER	OFI	OWNER FURNISHED, CONTRACTOR TO INSTALL
CT	CERAMIC TILE	OPP	OPPOSITE
DEBL	DOUBLE	PLYUD	PLYWOOD
DF	DRINKING FOUNTAIN	FL	PLASTIC LAMINATE
DIA	DIAMETER	PL	PLATE
DIM	DIMENSION	PT	PORCELAIN TILE
DN	DOWN	QT	QUARRY TILE
DR	DOOR	REQ'D	REQUIRED
DS	DOWNSPOUT	RO	ROUGH OPENING
DTL	DETAIL	SHT	SHEET
DWG	DRAWING	SV	SHEET VINYL
EA	EACH	SH	SIMILAR
EIPS	EXTERIOR INSULATION AND FINISH SYSTEM	SS	SOLID SURFACE
EL	ELEVATION	ST	STAINLESS STEEL
EJ	EXPANSION JOINT	STD	STANDARD
EQ	EQUAL	SAT	SUSPENDED ACOUSTIC TILE
EQUIP	EQUIPMENT	CEILING	CEILING
EXIST	EXISTING	TBD	TO BE DETERMINED
EXT	EXTERIOR	TEMP	TEMPORARY
EWC	ELECTRIC WATER COOLER	T/O	TOP OF
FEC	FIRE EXTINGUISHER CABINET	T&G	TONGUE AND GROOVE
FIN	FINISH	TYP	TYPICAL
FD	FLOOR DRAIN	UNO	UNLESS NOTED OTHERWISE
FLR	FLOOR	VCT	VINYL COMPOSITION TILE
FRP	FIBERGLASS REINFORCED PLASTIC	VB	VINYL BASE
GA	GAUGE	VF	VERIFY IN FIELD
GALV	GALVANIZED	WD	WOOD
GYP BD	GYP SUM BOARD	WUF	WELDED WIRE FABRIC

PROJECT DATA
APPLICABLE CODES: 2015 IBC - INTERNATIONAL BUILDING CODE WITH WISCONSIN AMENDMENTS 2015 IBC - INTERNATIONAL EXISTING BUILDING CODE
BUILDING CLASSIFICATION:
CONSTRUCTION CLASSIFICATION:
TENANT IMPROVEMENTS:
EGRESS:
BUILDING DATA:

GENERAL NOTES
1. DO NOT SCALE DRAWINGS. 2. CONTRACTOR SHALL FIELD VERIFY AND BECOME THOROUGHLY FAMILIAR WITH ALL CONDITIONS AND DIMENSIONS. 3. EACH CONTRACTOR SHALL REVIEW AND COMPLETE PLANS FOR RELATED WORK. 4. ALL WORK SHALL BE IN COMPLIANCE WITH STATE AND LOCAL CODES FOR RESPECTIVE TRADES.

SHEET INDEX
GENERAL TI TITLE SHEET
ARCHITECTURAL A11 SITE PLAN A31 OVERALL FLOOR PLAN A41 EXTERIOR ELEVATIONS A42 PERSPECTIVE VIEWS



CLIENT:

IRGENS
833 EAST MICHIGAN STREET, SUITE 400
MILWAUKEE, WISCONSIN 53202
PHONE: 414.443.2594
ATTN: TOM IRGENS

ARCHITECT:

PARTNERS IN DESIGN ARCHITECTS
2610 LAKE COOK ROAD, SUITE 280
RIVERWOODS, ILLINOIS 60075
PHONE: 847.340.0300
ATTN: WERNER BRISKE, AIA
OR ERIC ORTEGA

CIVIL ENGINEER

PINNACLE ENGINEERING GROUP
15550 W. BLUEMOUND ROAD, SUITE 210
BROOKFIELD, WISCONSIN 53005
PHONE: 262.754.8888
ATTN: MATTHEW CAREY

IRGENS

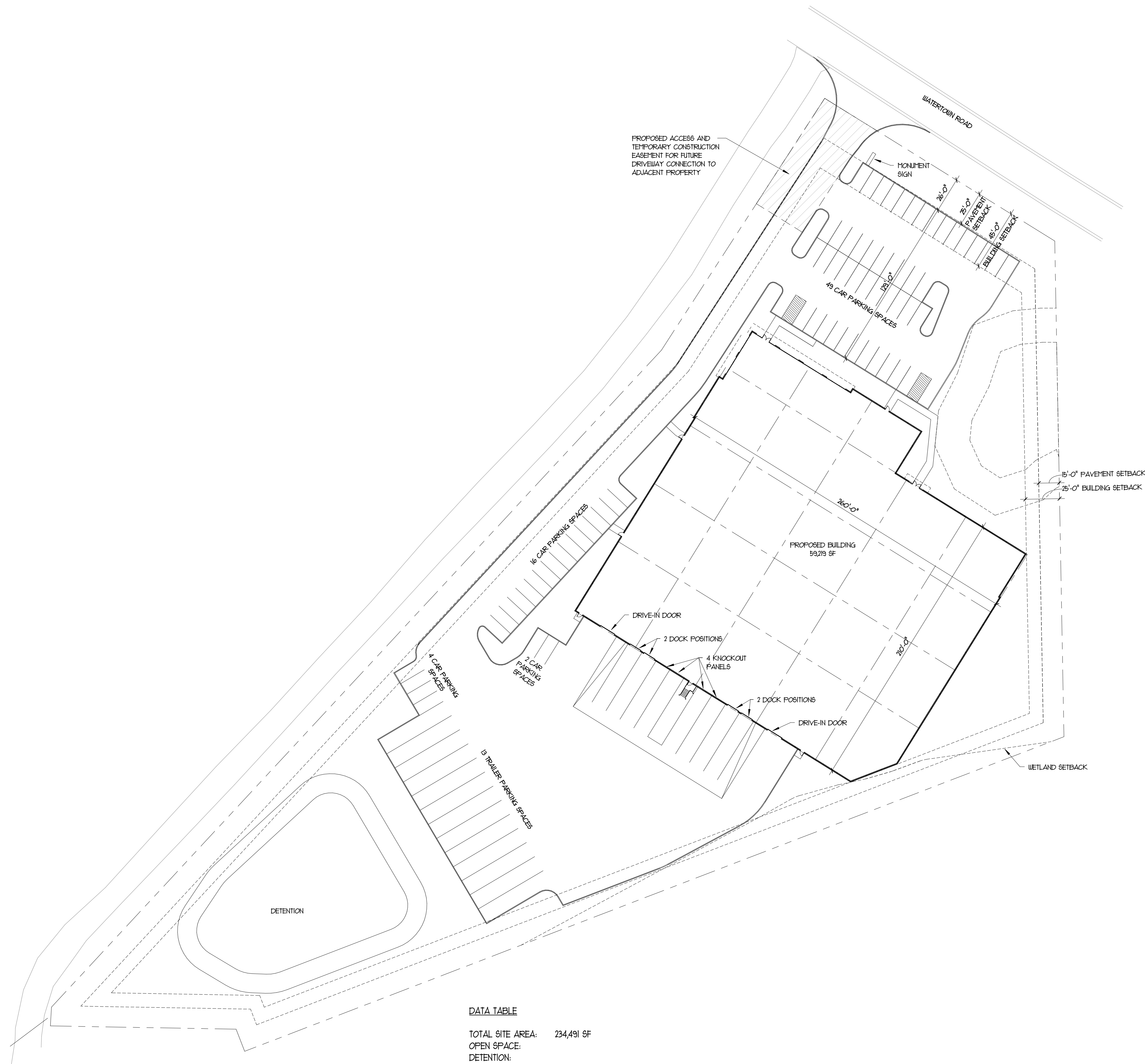
Partners in Design ARCHITECTS

PINNACLE ENGINEERING GROUP

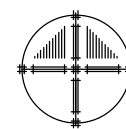


FOX RUN CENTER
PROJECT NUMBER:
636.22.113

SITE PLAN SUBMITTAL
ISSUE DATE: DECEMBER 8, 2023



1 SITE PLAN
A1.1 1" = 40'-0"



DATA TABLE

TOTAL SITE AREA: 234,491 SF
OPEN SPACE:
DETENTION:

BUILDING AREA: 59,219 SF
TRUCK DOCKS: 4 DOCK POSITIONS
(FUTURE): 4 DOCK POSITIONS
TRAILER STALLS: 17 SPACES
CAR PARKING: 71 SPACES

REVISIONS:

FOX RUN CENTER
N15 W 22261 Watertown Road, Pewaukee, WI

SITE PLAN

2610 Lake Cook Road
Suite 280
Riverwoods, IL 60015
Ph.: (847)940-0300

Partners in Design
ARCHITECTS



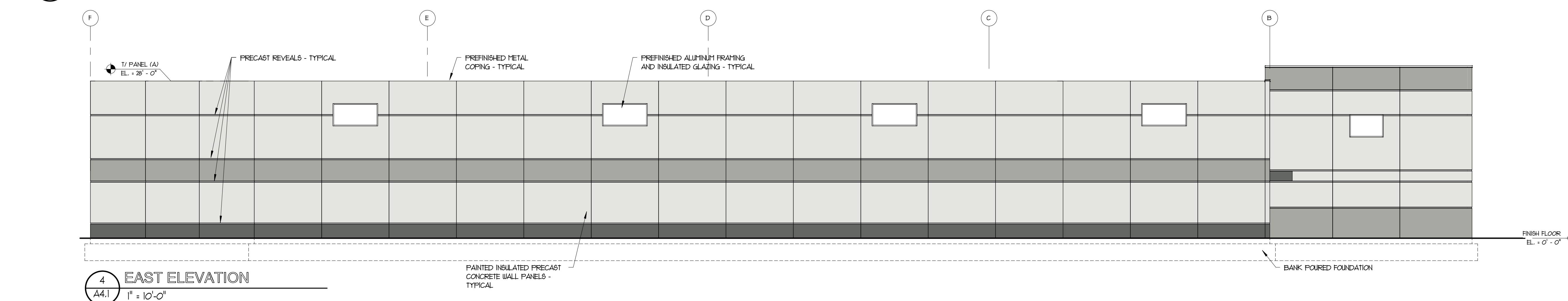
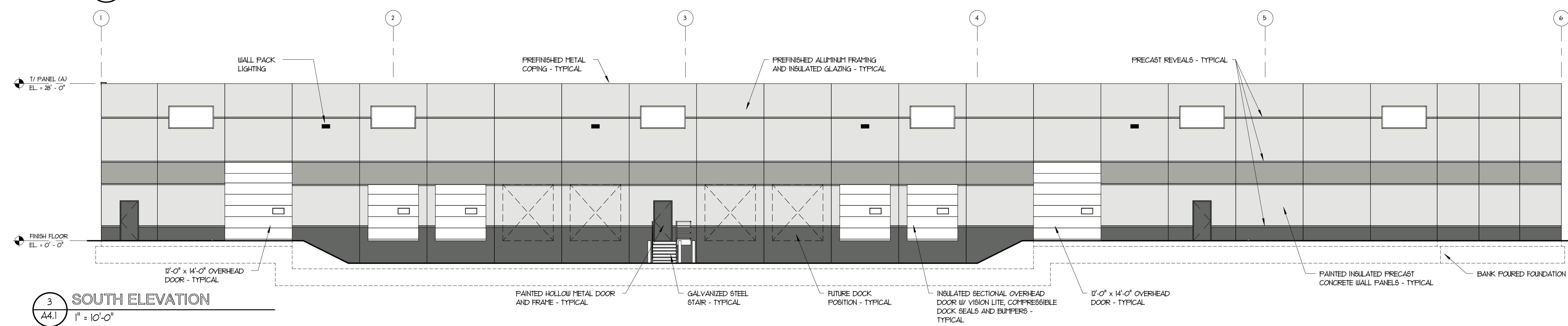
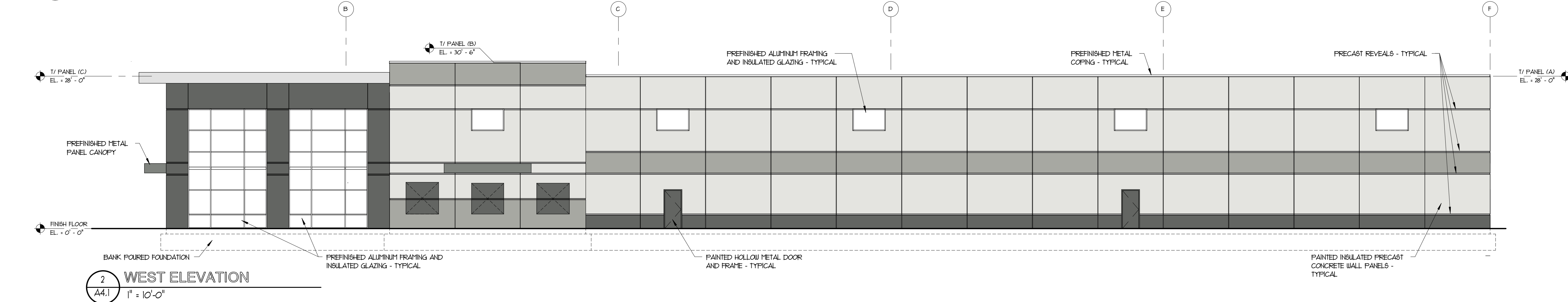
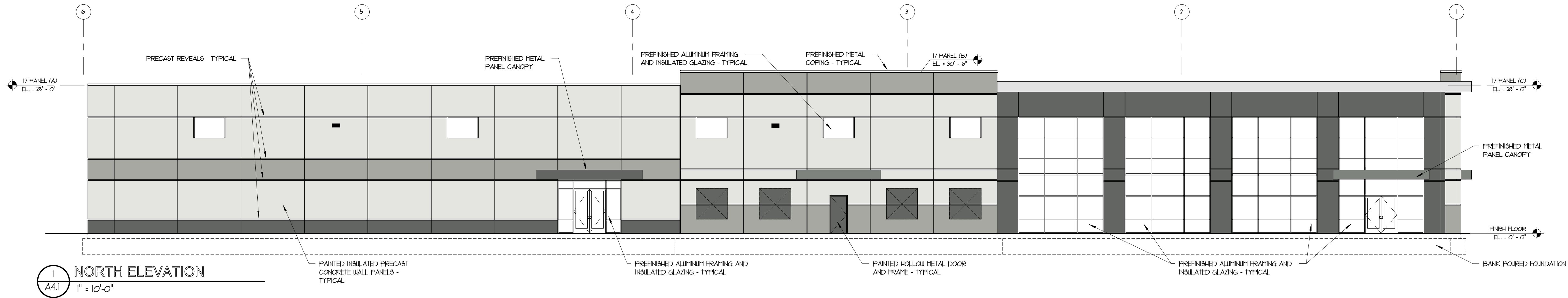
PROJECT NO.:
636.22.113

DRAWN BY: EEO CHECKED BY: WJB

DATE:
12.08.23

SHEET NO.:

A1.1



COLOR LEGEND:	
	ACCENT COLOR 1: SHERWIN WILLIAMS T.B.D.
	ACCENT COLOR 2: SHERWIN WILLIAMS T.B.D.
	ACCENT COLOR 3: SHERWIN WILLIAMS T.B.D.
	METAL PANEL CANOPY COLOR: T.B.D.
	METAL COPING COLOR: PAC-CLAD T.B.D.





PERSPECTIVE 1



PERSPECTIVE 2



PERSPECTIVE 3

REVISIONS:

FOX RUN CENTER
N15 W 22261 Watertown Road, Pewaukee, WI
PERSPECTIVE VIEWS

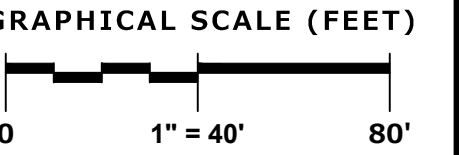
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Partners in Design
ARCHITECTS



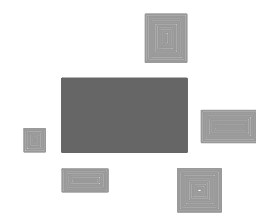
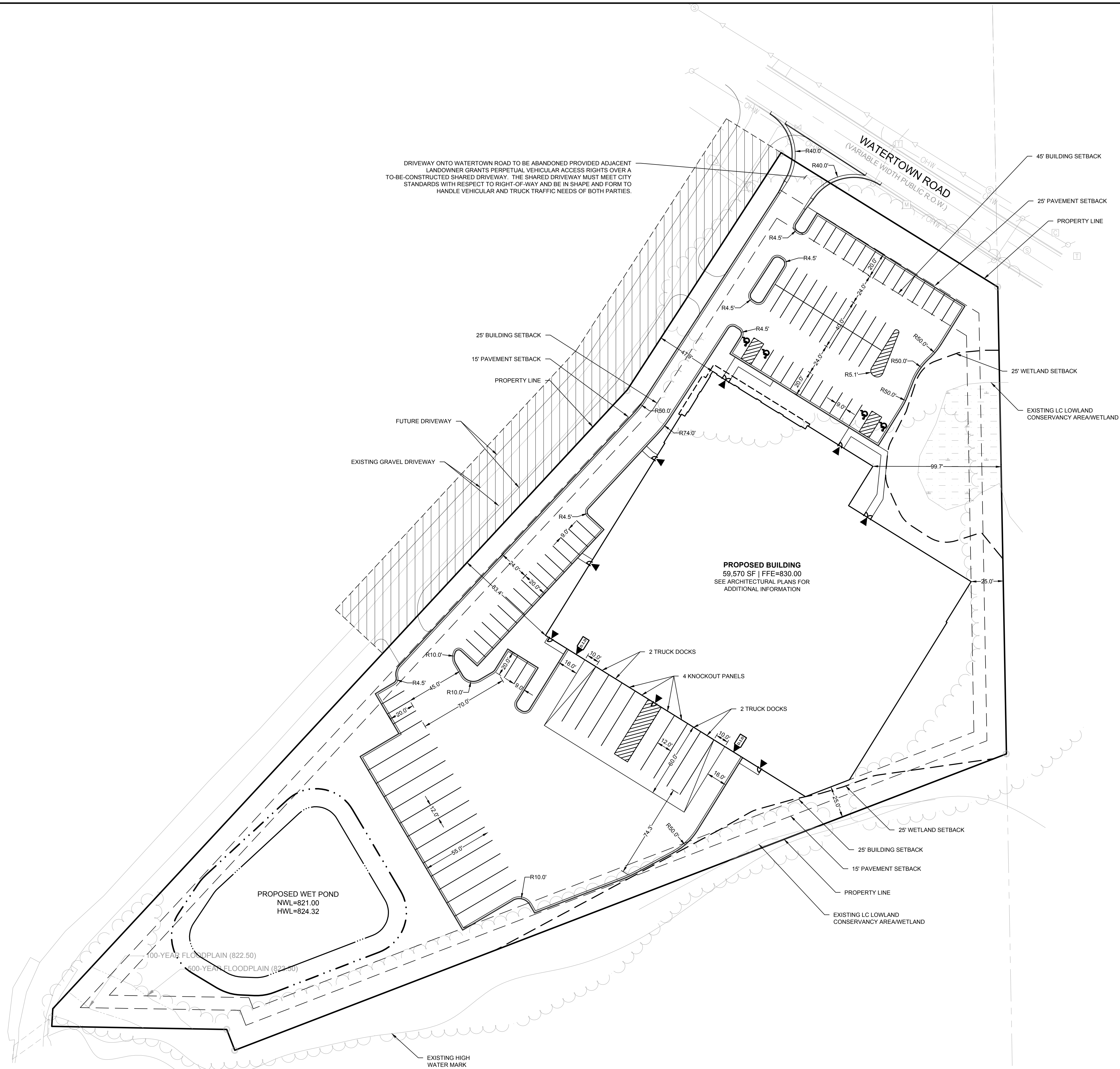
PROJECT NO.:
636.22.113
DRAWN BY: EEO CHECKED BY: WJB
DATE: 12.08.23
SHEET NO.:

A4.2



C-1	REZONING & MASTER PLAN AMENDMENT EXHIBIT
C-2	GRADING PLAN

EXISTING ZONING:	RS-1 SINGLE-FAMILY RESIDENTIAL DISTRICT
PROPOSED ZONING:	M-2 LIMITED INDUSTRIAL DISTRICT
TOTAL SITE AREA:	234,491 SF (5.38 AC)
OPEN SPACE AREA:	102,599 SF (2.36 AC) 43.9%
PROPOSED CAR PARKING:	73 SPACES (4 A.D.A.)
PROPOSED TRAILER PARKING:	13 SPACES
PROPOSED TRUCK DOCKS:	4 DOCKS, 4 KNOCKOUT PANELS
REQUIRED PARKING:	1 SPACE / 2 EMPLOYEES IN A 12 HOUR PERIOD



PINNACLE ENGINEERING GROUP
ENGINEERING | NATURAL RESOURCES | SURVEYING

WISCONSIN OFFICE:
20725 WATERTOWN ROAD, SUITE 100
BROOKFIELD, WI 53186
(262) 754-8888
CHICAGO | MILWAUKEE : NATIONWIDE

PEWAUKEE, WI

REVISIONS

_____	_____	_____	_____	_____	_____
_____	_____	_____	_____	_____	_____
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_____	_____	_____	_____	_____	_____

EG JOB NO:	MAC
EG RM	MAC

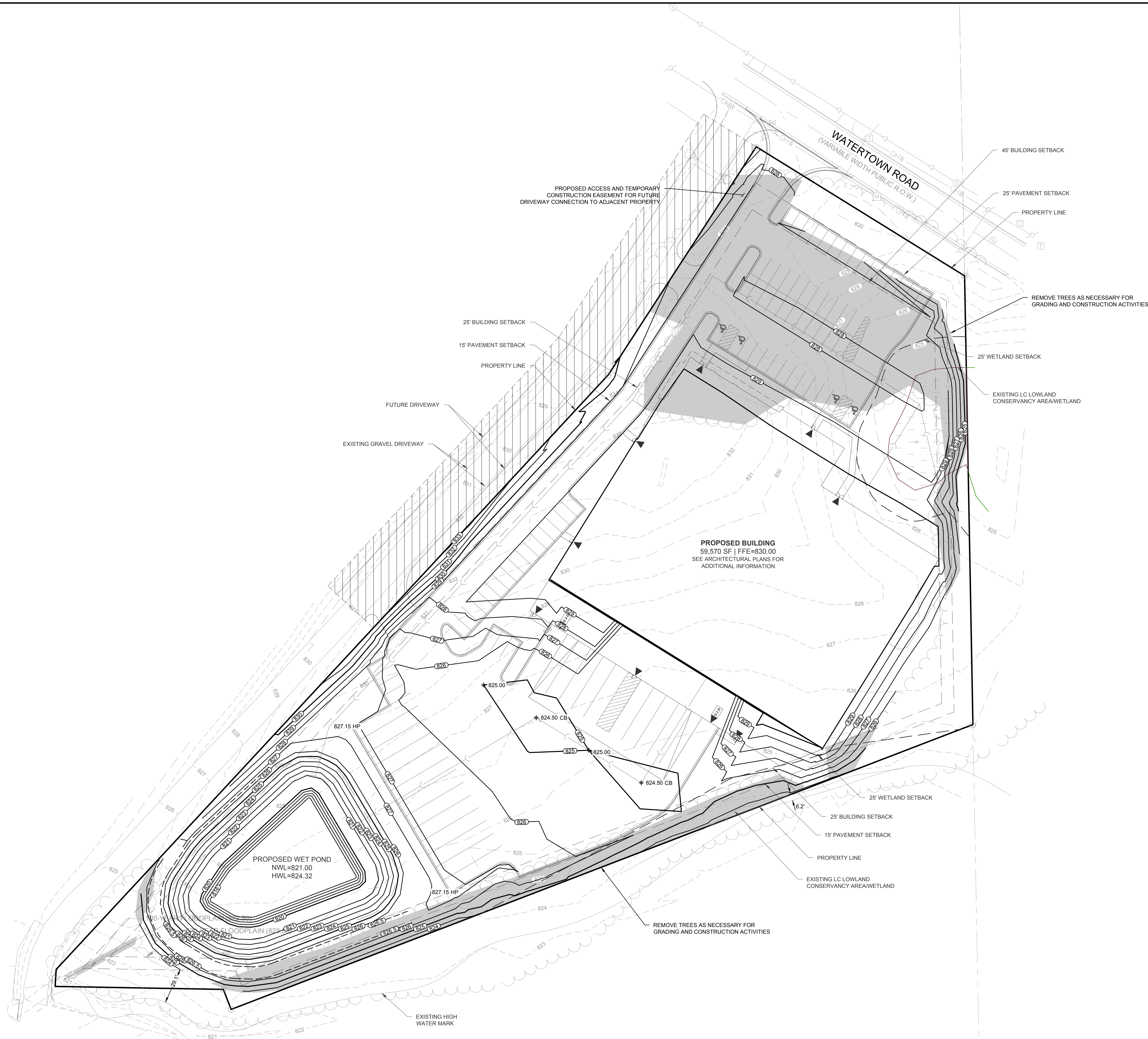
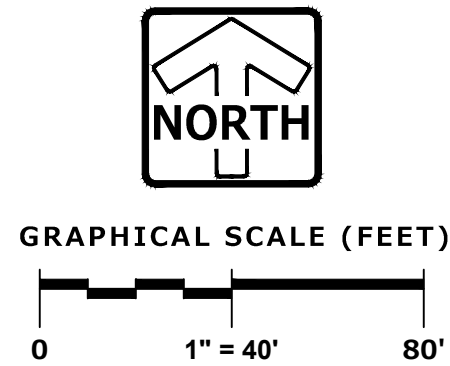
LOG IN: _____

TEST DATE 12/11/33

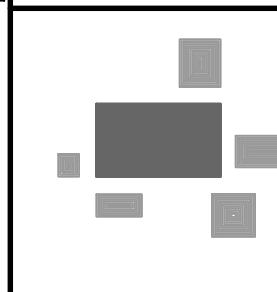
EXPIRATION DATE 12/11/23

SHEET
C-1
C-2

THESE PLANS AND DESIGNS ARE COPYRIGHT PROTECTED AND MAY NOT BE USED IN WHOLE OR IN PART WITHOUT THE WRITTEN CONSENT OF PINNACLE ENGINEERING GROUP, LLC
DESIGNED: DCC
DRAFTED: DCC
REVIEWED: MAC



LEGEND	
	STORM SEWER MANHOLE
	STORM SEWER CATCH BASIN- ROUND CASTING
	STORM SEWER CATCH BASIN- RECTANGULAR CASTING
	PROPOSED CONCRETE FLARED END SECTION
	PROPOSED CONTOUR
	SPOT ELEVATION
	DIRECTION OF SURFACE FLOW
	DITCH OR SWALE
	DIVERSION SWALE
	OVERFLOW RELIEF ROUTING
	CONCRETE SIDEWALK
	CURB AND GUTTER
	DEPRESSED CURB
	REVERSE PITCH CURB & GUTTER
	EDGE OF PAVEMENT
	FINISHED FLOOR ELEVATION
	TOP OF SIDEWALK
	YARD GRADE



PINNACLE ENGINEERING GROUP

ENGINEERING | NATURAL RESOURCES | SURVEYING

PLAN | DESIGN | DELIVER

www.pinnacle-engr.com

WISCONSIN OFFICE:

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BROOKFIELD, WI 53186

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CHICAGO | MILWAUKEE | NATIONWIDE

FOX RUN CENTER

PEWAUKEE, WI













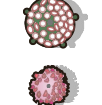















GRADING PLAN

REVISIONS

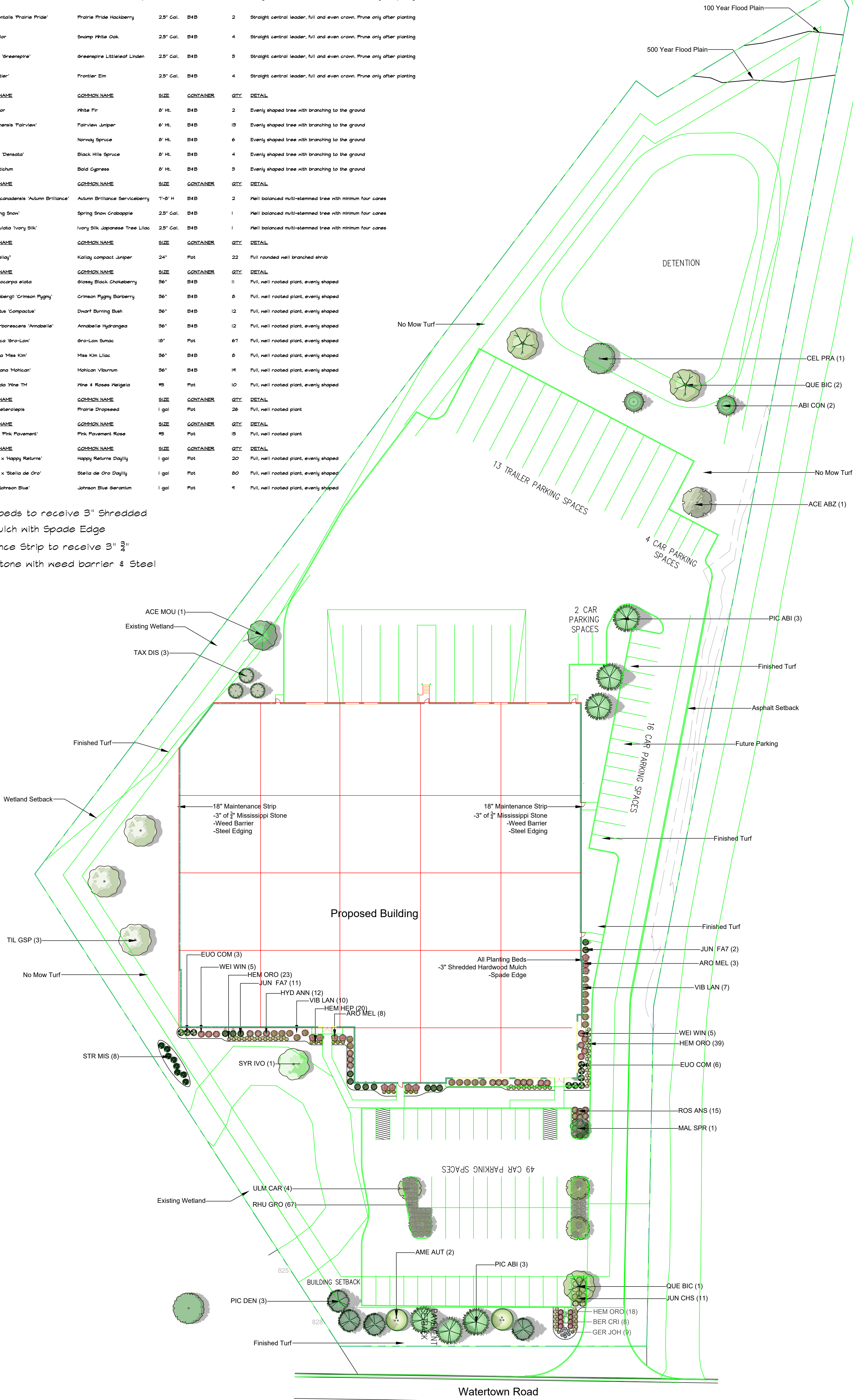
REG JOB NO.	2757.00
MAC	
REG PM	
START DATE	12/11/23
SCALE	

SHEET
C-2
C-2

PLANT SCHEDULE

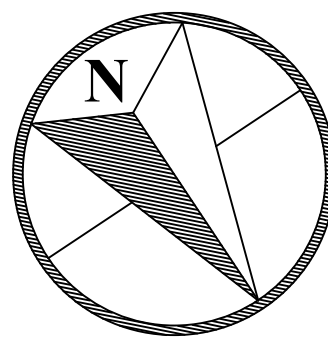
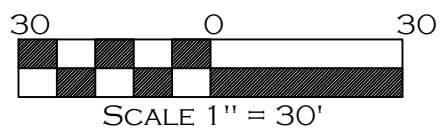
DECIDUOUS TREES	CODE	BOTANICAL NAME	COMMON NAME	SIZE	CONTAINER	QTY	DETAIL
	ACE MOU	Acer saccharum 'PNI G285'	Green Mountain Sugar Maple	2.5' Cal.	B4B	1	Straight central leader, full and even crown. Prune only after planting
	ACE ABZ	Acer x freemanii 'Jeffersred'	Autumn Blaze® Freeman Maple	2.5' Cal.	B4B	1	Straight central leader, full and even crown. Prune only after planting
	CEL PRA	Celtis occidentalis 'Prairie Pride'	Prairie Pride Hackberry	2.5' Cal.	B4B	2	Straight central leader, full and even crown. Prune only after planting
	QUE BIC	Quercus bicolor	Swamp White Oak	2.5' Cal.	B4B	4	Straight central leader, full and even crown. Prune only after planting
	TIL GSP	Tilia cordata 'Greenspire'	Greenspire Littleleaf Linden	2.5' Cal.	B4B	3	Straight central leader, full and even crown. Prune only after planting
	ULM CAR	Ulmus x 'Frontier'	Frontier Elm	2.5' Cal.	B4B	4	Straight central leader, full and even crown. Prune only after planting
EVERGREEN TREES	CODE	BOTANICAL NAME	COMMON NAME	SIZE	CONTAINER	QTY	DETAIL
	ABI CON	Abies concolor	White Fir	8' Ht.	B4B	2	Evenly shaped tree with branching to the ground
	JUN FA7	Juniperus chinensis 'Fairview'	Fairview Juniper	6' Ht.	B4B	13	Evenly shaped tree with branching to the ground
	PIC ABI	Picea abies	Norway Spruce	8' Ht.	B4B	6	Evenly shaped tree with branching to the ground
	PIC DEN	Picea glauca 'Densata'	Black Hills Spruce	8' Ht.	B4B	4	Evenly shaped tree with branching to the ground
	TAX DIS	Taxodium distichum	Bald Cypress	8' Ht.	B4B	3	Evenly shaped tree with branching to the ground
FLOWERING TREES	CODE	BOTANICAL NAME	COMMON NAME	SIZE	CONTAINER	QTY	DETAIL
	AME AUT	Amelanchier canadensis 'Autumn Brilliance'	Autumn Brilliance Serviceberry	7'-8' H	B4B	2	Well balanced multi-stemmed tree with minimum four canes
	MAL SPR	Malus x 'Spring Snow'	Spring Snow Crabapple	2.5' Cal.	B4B	1	Well balanced multi-stemmed tree with minimum four canes
	SYR IVO	Syringa reticulata 'Ivory Silk'	Ivory Silk Japanese Tree Lilac	2.5' Cal.	B4B	1	Well balanced multi-stemmed tree with minimum four canes
CONIFERS	CODE	BOTANICAL NAME	COMMON NAME	SIZE	CONTAINER	QTY	DETAIL
	JUN CHS	Juniperus 'Kallay'	Kallay compact Juniper	24"	Pot	22	Full rounded well branched shrub
SHRUBS	CODE	BOTANICAL NAME	COMMON NAME	SIZE	CONTAINER	QTY	DETAIL
	ARO MEL	Aronia melanocarpa 'elata'	Glossy Black Chokeberry	36"	B4B	11	Full, well rooted plant, evenly shaped
	BER CRI	Berberis thunbergii 'Crimson Pigmy'	Crimson Pigmy Barberry	36"	B4B	6	Full, well rooted plant, evenly shaped
	EYO COM	Euonymus alatus 'Compactus'	Dwarf Burning Bush	36"	B4B	12	Full, well rooted plant, evenly shaped
	HYD ANN	Hydrangea arborescens 'Annabelle'	Annabelle Hydrangea	36"	B4B	12	Full, well rooted plant, evenly shaped
	RHU GRO	Rhus aromatica 'Smo-Low'	Smo-Low Sumac	18"	Pot	67	Full, well rooted plant, evenly shaped
	SYR MIS	Syringa patula 'Miss Kim'	Miss Kim Lilac	36"	B4B	6	Full, well rooted plant, evenly shaped
	VIB LAN	Viburnum lantana 'Mohican'	Mohican Viburnum	36"	B4B	14	Full, well rooted plant, evenly shaped
	WEI WIN	Weigela florida 'Wine TH'	Wine & Roses Weigela	#5	Pot	10	Full, well rooted plant, evenly shaped
GRASSES	CODE	BOTANICAL NAME	COMMON NAME	SIZE	CONTAINER	QTY	DETAIL
	SPO HET	Sporobolus heterolepis	Prairie Dropseed	1 gal	Pot	26	Full, well rooted plant
ROSES	CODE	BOTANICAL NAME	COMMON NAME	SIZE	CONTAINER	QTY	DETAIL
	ROS ANS	Rosa rugosa 'Pink Pavement'	Pink Pavement Rose	#5	Pot	15	Full, well rooted plant
ANNUALS/PERENNIALS	CODE	BOTANICAL NAME	COMMON NAME	SIZE	CONTAINER	QTY	DETAIL
	HEM HAP	Hemerocallis x 'Happy Returns'	Happy Returns Daylily	1 gal	Pot	20	Full, well rooted plant, evenly shaped
	HEM ORO	Hemerocallis x 'Stella de Oro'	Stella de Oro Daylily	1 gal	Pot	80	Full, well rooted plant, evenly shaped
	GER JOH	Geranium x 'Johnson Blue'	Johnson Blue Geranium	1 gal	Pot	4	Full, well rooted plant, evenly shaped

- * All Planting beds to receive 3" Shredded Hardwood Mulch with Spade Edge
- * All Maintenance Strip to receive 3" $\frac{3}{4}$ " Mississippi Stone with weed barrier & Steel edging.



Fox Run Industrial Center

Watertown Road, Pewaukee, WI



Sheet
L1

Designed By Mike Schierl
Drawn By Kathy Feucht
Date January 27th, 2023
Revised December 10th, 2023-kf
Project #005855

**CITY OF PEWAUKEE
COMMON COUNCIL AGENDA ITEM 10.**

DATE: January 15, 2024

DEPARTMENT: Planning

PROVIDED BY: Nick Fuchs

SUBJECT:

Discussion and Possible Action Regarding a Certified Survey Map for Yench, LLC for Property Located on the West Side of Yench Road (PWC 0885-996-006) for the Purpose of Subdividing the Existing Property Into Three Lots and One Outlot [Fuchs]

BACKGROUND:

At their December 21, 2023, meeting, the Plan Commission unanimously recommended approval of the proposed Certified Survey Map.

FINANCIAL IMPACT:

RECOMMENDED MOTION:

Considering the zoning and storm water management concerns, staff is recommending a motion to table the certified survey map to allow the applicant to submit a Rezoning Application and provide a storm water management plan in compliance with City regulations.

Alternatively, if the Common Council wishes to approve the CSM, the recommended motion is to approve the Certified Survey Map for Yench, LLC for property located on the west side of Yench Road for the purpose of subdividing the existing property into three lots and one outlot.

ATTACHMENTS:

Description

Yench LLC staff report 12.21.23

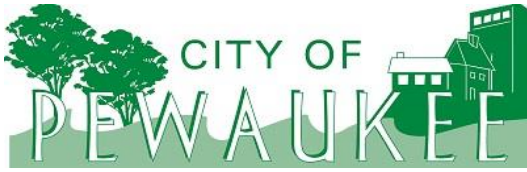
Yench LLC CSM narrative

Yench LLC CSM #1

Yench LLC staff comment responses

Yench LLC civil plans

Engineering memo



Office of the Planner & Community Development Director
W240 N3065 Pewaukee Road
Pewaukee, Wisconsin 53072
Phone (262) 691-0770 Fax (262) 691-1798
fuchs@pewaukee.wi.us

REPORT TO THE PLAN COMMISSION

Meeting of December 21, 2023

Date: December 8, 2023

Project Name: Yench LLC Certified Survey Map

Project Address/Tax Key No.: Not Assigned/PWC 0885996006

Applicant: YENCH LLC

Property Owner: YENCH LLC

Current Zoning: Rs-2 Single-Family Residential District, A-2 Agricultural District, F-1 Floodplain District, and LC Lowland Conservancy District

2050 Land Use Map Designation: Low Density Residential (> 2 Ac. / D.U.) and Floodplains, Lowland & Upland Conservancy and Other Natural Areas

Use of Surrounding Properties: Single-family residential to the north and east, Highway 16 and Yench Road Wildlife Area to the south and single-family residential zoned A-2 to the west

Project Description/Analysis

The applicant filed two Certified Survey Map Applications requesting to subdivide the existing lot located along Yench Road into a total of four lots and one outlot.

The subject property is approximately 30.7-acres and contains split zoning of Rs-2 Single-Family Residential District and A-2 Agricultural District. The split zoning was a result of a previous Rezoning and Certified Survey Map (CSM) submittal. That CSM, however, did not move forward and was not recorded. Note the property also contains F-1 Floodplain District and LC Lowland Conservancy District.

The property is designated as Low Density Residential and Floodplains, Lowland & Upland Conservancy and Other Natural Areas on the City's Year 2050 Land Use/Transportation Plan.

CSM #1: 3 Lots and 1 Outlot

The first CSM that would be recorded consists of three lots and one outlot. Lot 1 has an area of 2.48-acres, Lot 2 has an area of 2.31-acres, and Lot 3 has an area of 25.21-acres. The area of Outlot 1 is about 0.69-acres.

CSM #2: 2 Lots

The second CSM provided is a redivision of Lot 3 noted above. That 25.21-acre lot is proposed to be subdivided into two separate lots. Lot 1 has an area of 2.31-acres and Lot 2 has an area of 22.90-acres.

All lots are planned for single-family residential development. The applicant would like to retain the A-2 District zoning of the larger parcel for the potential of agricultural uses in addition to a single-family residence.

All properties will be served by private utilities and City approval of the access location for each lot is required.

Staff also recommends that Lot 2 of the 2-Lot CSM shall have a minimum width of 220 feet at the setback line in accordance with the Rs-2 District minimum lot size requirements.

Storm Water Management

The applicant has provided a storm water management plan; however, it does not include lands that were previously subdivided as part of this development. It is staff's contention that this is a continuation of development or a common plan of development, and as such, the storm water management plan needs to consider all development that has been completed within the original 56.5-acre property.

Staff recommends resubmittal of a storm water management plan in compliance with Chapter 19 of the City's Municipal Code. Final grading and storm water management plans, including maintenance agreements, shall be reviewed and approved by the Engineering Department, prior to recording the CSM with the Waukesha County Register of Deeds.

Zoning

The current split zoning of Rs-2 and A-2 does not reflect the boundaries of the newly proposed lots. As such, staff recommends submittal of a Rezoning Application to eliminate any split zoning that would be created by the recording of the subject certified survey maps. Staff recommends that all lots and the outlot be zoned Rs-2 and only the boundary of Lot 2 of the 2-Lot CSM include A-2 zoning. Note the F-1 District and LC District zoning must match the field delineated boundaries of the floodplain and wetland areas onsite.

In addition, confirmation is needed that the wetland delineations are still valid. If not, the site shall be re-delineated, and the boundary revised on the certified survey maps if necessary.

Recommendation

Considering the zoning and storm water management concerns, staff is recommending a motion to table both certified survey maps to allow the applicant to submit a Rezoning Application and provide a storm water management plan in compliance with City regulations.

November 8, 2023

Re: "Yench Road Land Division"
Proposed 4 Lots and Storm water Outlot

Dear City of Pewaukee,

Yench, LLC appreciates the opportunity to bring forward for your approval a series of **two CSM's that will create a total of four (4) lots and one (1) Outlot along the west side of Yench Road, south of Lynndale Road (CTH JK)**. The property is currently zoned Rs-2 with the southern area zoned A-2; the proposed land divisions will conform to the existing zoning. The first CSM will create the two northern Rs-2 lots, an Outlot for storm water management, with the remaining south land as one large lot. The second CSM will further divide the first CSM's southern lot into an Rs-2 lot and a large A-2 lot.

You may recall that this property was before you in 2021, at which time you approved it for a four (4) lot CSM land division. Through this new application we are maintaining the same 4 lot concept, but have added an Outlot for storm water management.

This project has been designed to provide storm water management meeting City and Wisconsin DNR requirements within the extents of the two CSMs. A proposed biofiltration basin situated in the Outlot manages runoff from the proposed lots; this basin discharges through an easement to a large wetland adjacent to STH 16 to the south. The stormwater management plan demonstrates that this design meets City and DNR requirements.

The stormwater management plan for this project does not include other nearby properties or offsite areas that are downhill or not tributary to this site, as is appropriate and consistent with DNR policy. Pete Wood, a lead Water Resources Engineer at the DNR, has stated via email dated July 7, 2023, that this project is not considered a common plan of development with other homes built by others on lots adjacent to this property. Therefore, this project should not be burdened with retroactively creating storm water management facilities for downhill or offsite areas, and is not able to do so.

We herein request that the City Plan Commission and Common Council, as part of our desired approval, note that this project needs to provide stormwater management specific for the subject property.

The enclosures with this CSM application include the two (2) CSMs, site civil plans, and a storm water management plan, all designed per City of Pewaukee requirements.

We thank you for your time and consideration in this minor land division approval, and welcome any questions or discussion.

Sincerely,

Jeff Mierow
Yench, LLC

CERTIFIED SURVEY MAP NO.

BEING A REDIVISION OF LOT 3 OF CERTIFIED SURVEY MAP NUMBER 11464 LOCATED IN THE NORTHWEST 1/4 AND THE SOUTHWEST 1/4 OF THE NORTHEAST 1/4 OF SECTION 6, TOWN 7 NORTH, RANGE 19 EAST, IN THE CITY OF PEWAUKEE, WAUKESHA COUNTY, WISCONSIN.

PREPARED FOR:

YENCH, LLC
3055 N. BROOKFIELD RD.
BROOKFIELD, WI 53005
PHONE: 414-254-0731

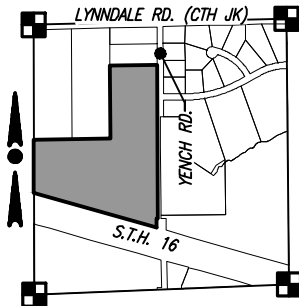
PREPARED BY:

TRIO ENGINEERING, LLC
4100 N. CALHOUN RD. SUITE 300
BROOKFIELD, WI 53005 30' WIDE PRIVATE STORM WATER DRAINAGE EASEMENT
PHONE: 262-790-1480
FAX: 262-790-1481

SETBACKS PER RS-2 ZONING

LOTS 1-2

FRONT = 45' WETLANDS = 25'
SIDE = 25' 100-YEAR FLOOD = 75'
REAR = 35' EASEMENTS = 5'



LOCALITY MAP:
N.E. 1/4 SEC. 6,
T. 7 N., R. 19 E.
SCALE: 1"=2000'

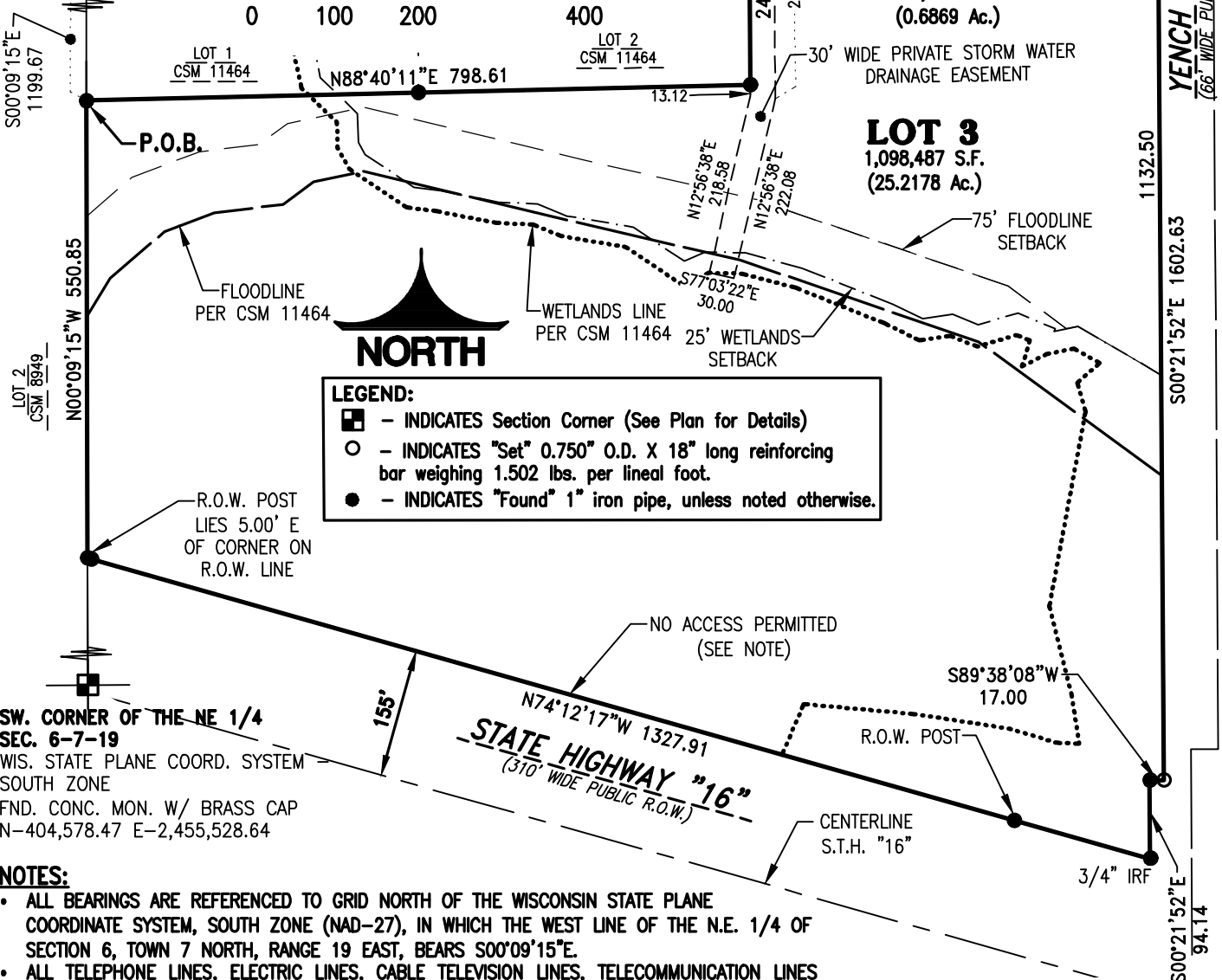
(P.O.C.)

NW. CORNER OF THE NE 1/4

SEC. 6-7-19

WIS. STATE PLANE COORD.
SYSTEM - SOUTH ZONE
FND. CONC. MON.
W/ BRASS CAP
N-407,391.66 E-2,455,521.07

SCALE: 1"=200'



- LEGEND:**
- - INDICATES Section Corner (See Plan for Details)
 - - INDICATES "Set" 0.750" O.D. X 18" long reinforcing bar weighing 1.502 lbs. per lineal foot.
 - - INDICATES "Found" 1" iron pipe, unless noted otherwise.

SW. CORNER OF THE NE 1/4

SEC. 6-7-19

WIS. STATE PLANE COORD. SYSTEM
SOUTH ZONE
FND. CONC. MON. W/ BRASS CAP
N-404,578.47 E-2,455,528.64

NOTES:

- ALL BEARINGS ARE REFERENCED TO GRID NORTH OF THE WISCONSIN STATE PLANE COORDINATE SYSTEM, SOUTH ZONE (NAD-27), IN WHICH THE WEST LINE OF THE N.E. 1/4 OF SECTION 6, TOWN 7 NORTH, RANGE 19 EAST, BEARS S00°09'15"E.
- ALL TELEPHONE LINES, ELECTRIC LINES, CABLE TELEVISION LINES, TELECOMMUNICATION LINES AND OTHER SIMILAR UTILITY SERVICES SHALL BE PLACED UNDERGROUND UNLESS THE APPLICANT OR UTILITY CAN DEMONSTRATE THAT UNDERGROUNDING IS NOT FEASIBLE.
- WETLANDS AND FLOODLINE SHOWN HEREON ARE FROM CERTIFIED SURVEY MAP NUMBER 11464.
- THERE SHALL BE NO DIRECT VEHICULAR INGRESS OR EGRESS TO STATE HIGHWAY "16" FROM LOT 3. IT BEING EXPRESSLY INTENDED THAT THIS RESTRICTION SHALL CONSTITUTE A RESTRICTION FOR THE BENEFIT OF THE PUBLIC ACCORDING TO S.236.293 OF THE WISCONSIN STATUTES AND SHALL BE ENFORCED BY THE WISCONSIN DEPARTMENT OF TRANSPORTATION AND WAUKESHA COUNTY.
- OUTLOT 1 IS TO BE MAINTAINED BY THE LOT OWNERS OF THIS LAND DIVISION AND ANY FUTURE LAND DIVISIONS. WAUKESHA COUNTY SHALL NOT BE LIABLE FOR FEES OR SPECIAL CHARGES IN THE EVENT THEY BECOME THE OWNER OF ANY LOT WITHIN THIS LAND DIVISION.
- LOT 3 TO BE RETAINED BY DEVELOPER FOR FUTURE DIVISION.

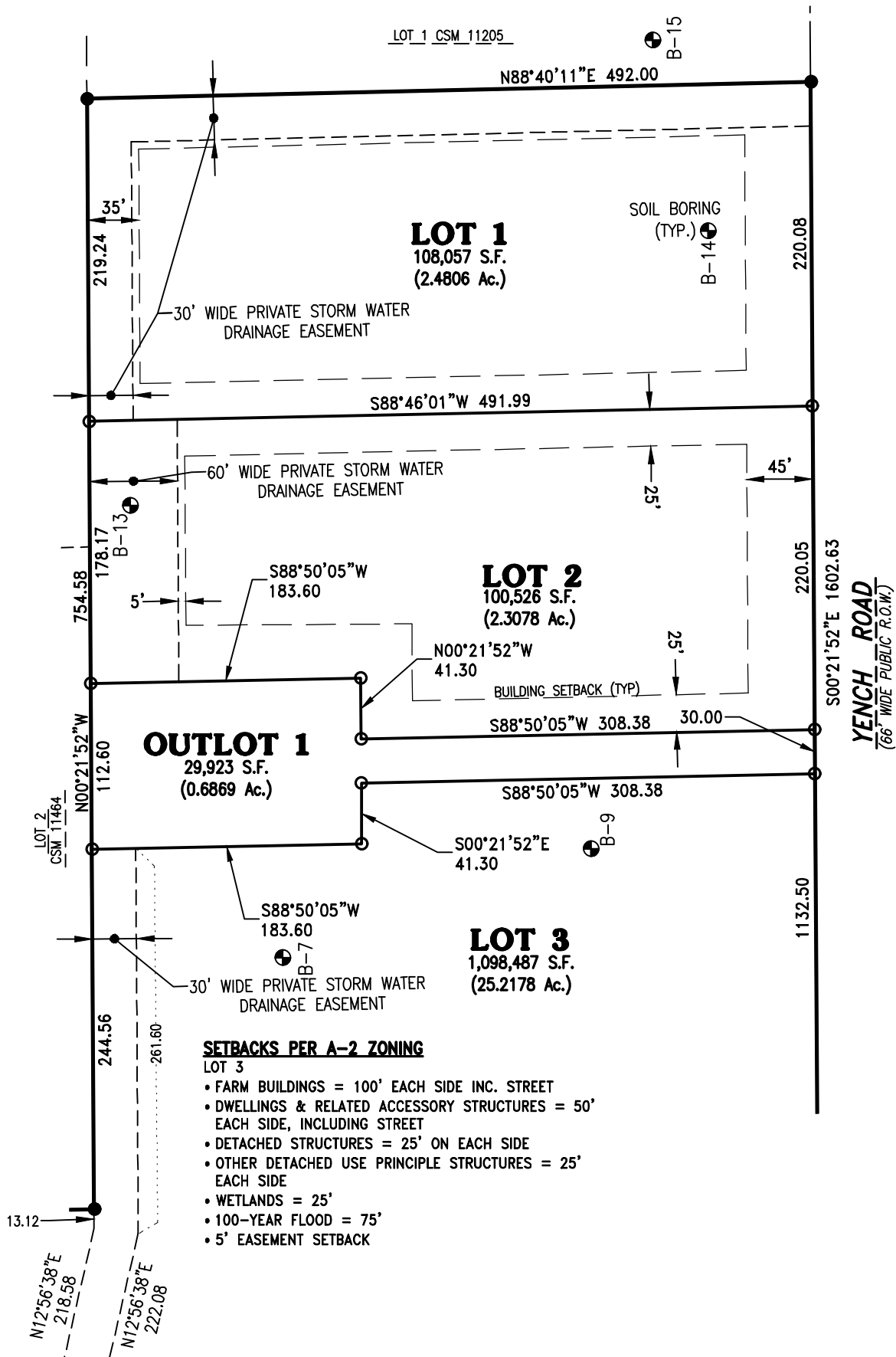
DRAFTED THIS 3RD DAY OF NOVEMBER, 2023. REVISED 11-22-23.

THIS INSTRUMENT WAS DRAFTED BY TED R. INDERMUEHLE, S-3119

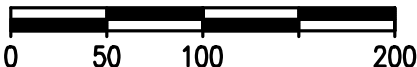
SHEET 1 OF 5

CERTIFIED SURVEY MAP NO.

BEING A REDIVISION OF LOT 3 OF CERTIFIED SURVEY MAP NUMBER 11464 LOCATED IN THE NORTHWEST 1/4 AND THE SOUTHWEST 1/4 OF THE NORTHEAST 1/4 OF SECTION 6, TOWN 7 NORTH, RANGE 19 EAST, IN THE CITY OF PEWAUKEE, WAUKESHA COUNTY, WISCONSIN.



NORTH
SCALE: 1"=100'



H:\C400\467\16015-01-YENCH ROAD\Survey\CSM\530CSM01.dwg

BEING A REDIVISION OF LOT 3 OF CERTIFIED SURVEY MAP NUMBER 11464 LOCATED IN THE NORTHWEST 1/4 AND THE SOUTHWEST 1/4 OF THE NORTHEAST 1/4 OF SECTION 6, TOWN 7 NORTH, RANGE 19 EAST, IN THE CITY OF PEWAUKEE, WAUKESHA COUNTY, WISCONSIN.

[illegible]

That I have surveyed, divided, and mapped a redivision of Lot 3 of Certified Survey Map Number 11464, recorded in the office of the Register of Deeds, Waukesha County on June 23rd, 2016 in Book 114 Pages 82-85 of Certified Survey Maps, inclusive as document number 4214210, located in the Northwest 1/4 and the Southwest 1/4 of the Northeast 1/4 of Section 6, Township 7 North, Range 19 East, in the City of Pewaukee, Waukesha County, Wisconsin.

That I have made such survey, land division and map by the direction of **Yench, LLC**, owner of said lands.

That such map is a correct representation of all the exterior boundaries of the land surveyed and the land division thereof made.

That I have fully complied with the provisions of Chapter 236 of the Wisconsin Statutes and the Subdivision Regulations of the City of Pewaukee, in surveying, dividing and the mapping the same.

Dated this _____ day of _____, 20_____.

Ted R. Indermuehle, P.L.S.
Professional Land Surveyor, S-3119
TRIO ENGINEERING, LLC
4100 N. Calhoun Rd. Suite 300
Brookfield, WI 53005
Phone: (262)790-1480 Fax: (262)790-1481

CERTIFIED SURVEY MAP NO._____

**BEING A REDIVISION OF LOT 3 OF CERTIFIED SURVEY MAP NUMBER 11464 LOCATED IN THE
NORTHWEST 1/4 AND THE SOUTHWEST 1/4 OF THE NORTHEAST 1/4 OF SECTION 6, TOWN 7 NORTH,
RANGE 19 EAST, IN THE CITY OF PEWAUKEE, WAUKESHA COUNTY, WISCONSIN.**

CORPORATE OWNER'S CERTIFICATE OF DEDICATION:

Yench, LLC, a Limited Liability Company duly organized and existing under and by virtue of the laws of the State of Wisconsin, as owner, does hereby certify that said Limited Liability Company has caused the land described on this map to be surveyed, divided, mapped and dedicated as represented on this map in accordance with the provisions of Chapter 236 of the Wisconsin Statutes and the Subdivision Regulations of the City of Pewaukee, this _____ day of _____, 20 ____.

Yench, LLC

Jeff Mierow, Owner

David Mierow, Owner

STATE OF WISCONSIN)
) ss
COUNTY OF)

Personally came before me this _____ day of _____, 20____, Jeff Mierow, Member of the above named limited liability company, to me known to be the person who executed the foregoing instrument, and to me known to be such Member of said limited liability company, and acknowledged that he executed the foregoing instrument as such officer as the deed of said limited liability company, by its authority.

Print Name: _____
Notary Public, _____ County, WI
My commission expires: _____

WETLANDS AND FLOODPLAIN PRESERVATION RESTRICTIONS:

Those areas identified as Wetlands and Floodplain on this Plat shall be subject to the following restrictions:

- 1.Grading, filling and removal of topsoil or other earthen materials are prohibited except in connection with the construction of a proposed gravel path, unless specifically authorized by the Municipality in which this land is located and, if applicable, the Waukesha County Department of Parks and Land Use, the Wisconsin Department of Natural Resources and the Army Corps of Engineers.
- 2.The removal or destruction of any vegetative cover, i.e., trees, shrubs, grasses, etc., is prohibited, with the exception that dead, diseased, or dying vegetation may be removed, at the discretion of the landowner and with approval from the Municipality in which this land is located and, if applicable, the Waukesha County Department of Parks and Land Use-Planning and Zoning Division. Silvicultural thinning upon the recommendation of a forester or naturalist and with approval from the Municipality in which this land is located and, if applicable, the Waukesha County Department of Parks and land Use-Planning and Zoning Division shall also be permitted.
- 3.Grazing by domesticated animals, i.e., horses, cows, etc, is prohibited.
- 4.The introduction of plant material not indigenous to the existing environment is prohibited.
- 5.Ponds may be permitted subject to the approval of the Municipality in which they are located and, if applicable, the Waukesha County Department of Parks and Land Use, the Wisconsin Department of Natural Resources and the Army Corps of Engineers.
- 6.The construction of buildings is prohibited.

Drafted this 3rd Day of November, 2023. REVISED 11-22-23.

THIS INSTRUMENT WAS DRAFTED BY TED R. INDERMUEHLE, S-3119

Job. No. 16-015-467-01

SHEET 4 OF 5

L:\LOBBYS\WPDOCS\DOCUMENT\467\16015-01_YENCH ROAD\530-Certified Survey Map\Yench CSM-1.doc

CERTIFIED SURVEY MAP NO._____

**BEING A REDIVISION OF LOT 3 OF CERTIFIED SURVEY MAP NUMBER 11464 LOCATED IN THE
NORTHWEST 1/4 AND THE SOUTHWEST 1/4 OF THE NORTHEAST 1/4 OF SECTION 6, TOWN 7 NORTH,
RANGE 19 EAST, IN THE CITY OF PEWAUKEE, WAUKESHA COUNTY, WISCONSIN.**

PLAN COMMISSION APPROVAL:

APPROVED by the Planning Commission of the City of Pewaukee on this ____ day of _____, 20__.

Steve Bierce, Mayor

Colleen Brown, Plan Commission Secretary

COMMON COUCIL APPROVAL:

Resolved that the Certified Survey map, in the City of Pewaukee, **Yench, LLC**, owner, is hereby approved and dedication accepted by the City Board of the City of Pewaukee by Resolution No._____, on this ____ day of _____, 20__.

All conditions have been met as of this _____ day of _____, 20__.

Date: _____

Steve Bierce, Mayor

Date: _____

Kelly Tarczewski, Clerk/Treasurer

November 29, 2023

Nick Fuchs,
Planner and Community
Development Director
City of Pewaukee
W240 N3065 Pewaukee Rd.
Pewaukee, WI 53072



RE: Staff Comments – Yench LLC Certified Survey Maps

Dear Mr. Fuchs:

Enclosed you will find our responses (**BOLD**) to your comments regarding Certified Survey Maps dated November 03, 2023, Civil Plans dated November 03, 2023, and Preliminary SWMP dated October 31 2023 for the proposed Lynndale Road CSM in the City of Pewaukee.

Comments and Recommendations:

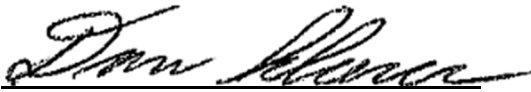
Below are comments and recommendations for the proposed development application for property located at Tax Key No. PWC 0885996006.

1. When were the wetlands delineated?
 - **Wetlands were delineated in 2015 by NES**
2. Note that all on-site soil absorption sanitary disposal systems must be approved by the Waukesha County Health and Human Services Department prior to installation.
 - **Noted**
3. Please revise both CSMs to list the Plan Commission secretary as Colleen Brown, not Brandon Bergman.
 - **Name has been revised.**
4. On both CSMs, please list the Plan Commission approval first and then Common Council approval.
 - **Document has been revised.**
5. Note that access for these lots will require City approval.
6. Below are comments related to the 3 Lot/1Outlot CSM:
 - a) Include A-2 setback information in addition to the Rs-2 setbacks.
 - **Setback information has been included.**
 - b) Please show a 5' setback from all public easements, including the drainage easement.
 - **5' setback has been included on CSMs and plans.**
 - c) Please show the location of all Soil Boring and Soil Percolation Tests in accordance with Section 18.0603b.8. of the City's Land Division Ordinance.
 - **Soil borings included on CSMs.**
7. Below are comments related to the 2 lot CSM:
 - a) Lot 1 requires a minimum width of 220 feet under the Rs-2 District.
 - i. **Lot widths have been revised to comply with zoning ordinances.**
 - b) As the A-2 District setbacks depend on an interpretation of the type of building proposed, it is recommended to remove the building setback line. The note indicating required setbacks will suffice.
 - i. **Setback lines have been removed.**

- c) Please show a 5' setback from all public easements, including the drainage easement.
 - i. **5' setback has been included on CSMs and plans.**
- d) Please show the location of all Soil Boring and Soil Percolation Tests in accordance with Section 18.0603b.8. of the City's Land Division Ordinance.
 - i. **Soil borings included on CSMs.**

Please feel free to contact me with any questions, comments or to further discuss the updated plans.

Sincerely,

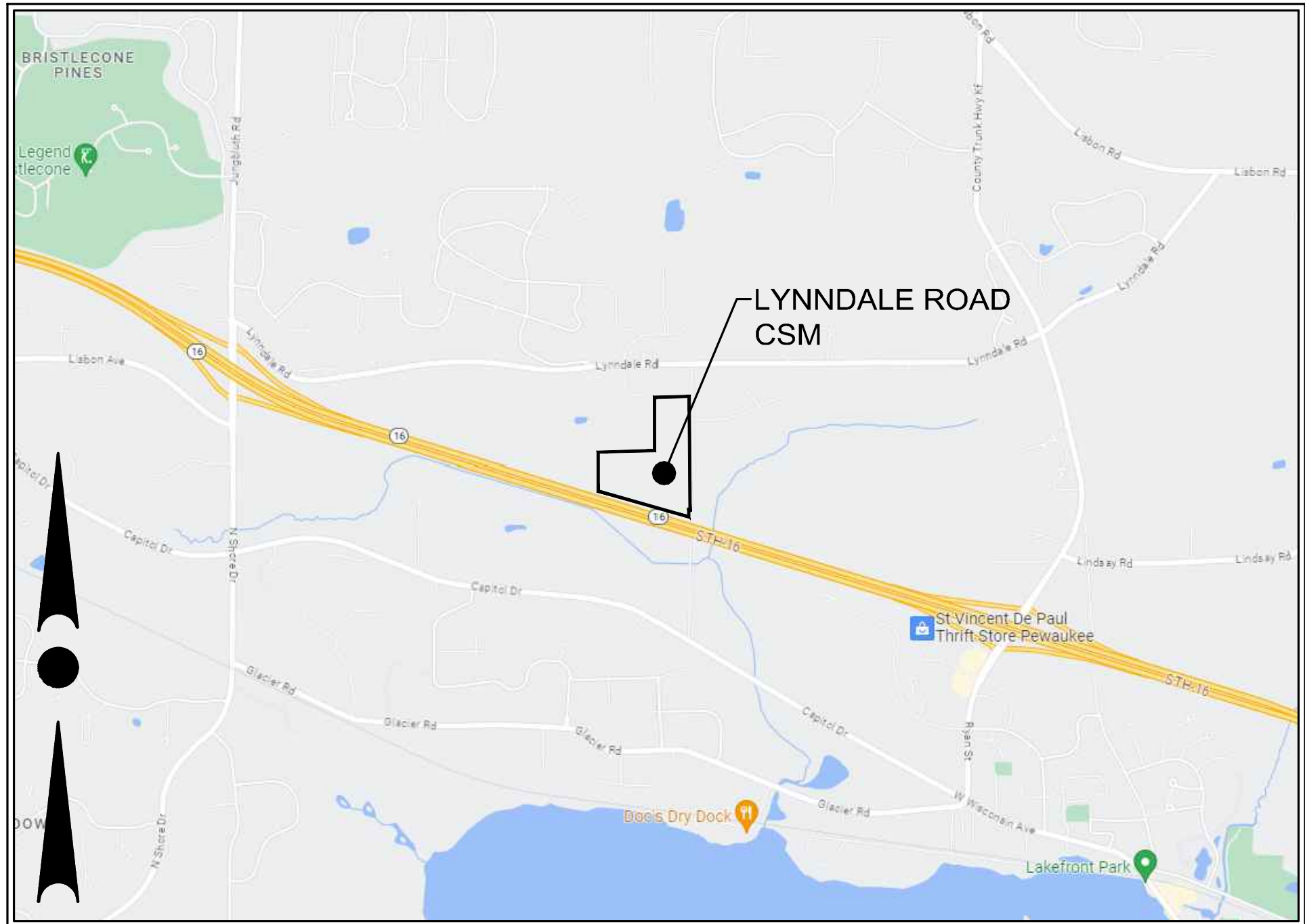


Dan Scherer, P.E.
Project Engineer
Trio Engineering
(262) 790-1480 ext.108
dscherer@trioeng.com

H:\C400\467\16015-01-YENCH ROAD\CONSTRUCTION PLANS\YENCH ROAD_CIVIL PLAN SET_22X34.DWG

GENERAL NOTES

1. THE LATEST EDITIONS OF THE FOLLOWING DOCUMENTS AND ANY SUPPLEMENTS THERETO, SHALL GOVERN ALL CONSTRUCTION ITEMS ON THIS PLAN UNLESS OTHERWISE NOTED.
-STANDARD SPECIFICATIONS FOR SEWER AND WATER CONSTRUCTION IN WISCONSIN, 6TH EDITION (SSSWCW)
-THE WISCONSIN D.O.T. STANDARD SPECIFICATIONS FOR HIGHWAY AND STRUCTURE CONSTRUCTION, LATEST EDITION
-WDNR STORMWATER RUNOFF TECHNICAL STANDARDS.
-WISDOT PAL APPROVED EROSION CONTROL MEASURES LIST, LATEST EDITION.
-CITY OF PEWAUKEE TECHNICAL STANDARDS.
2. THE CONTRACTOR SHALL TAKE ALL MEASURES NECESSARY TO MINIMIZE EROSION, WATER POLLUTION AND SILTATION CAUSED BY CONSTRUCTION OF THIS PROJECT. EROSION CONTROL MEASURES SHALL BE IN ACCORDANCE WITH WISCONSIN DEPARTMENT OF NATURAL RESOURCES TECHNICAL STANDARDS.
3. EROSION CONTROL PLAN: PRIOR TO BEGINNING WORK, AN APPROVED EROSION CONTROL PLAN WILL BE PROVIDED BY THE ENGINEER. THE CONTRACTOR IS RESPONSIBLE FOR PROPERLY IMPLEMENTING THE APPROVED PLAN.
4. THE CONTRACTOR SHALL FIELD VERIFY THE ELEVATIONS OF THE BENCHMARKS PRIOR TO COMMENCING WORK. THE CONTRACTOR SHALL ALSO FIELD VERIFY LOCATION, ELEVATION AND SIZE OF EXISTING UTILITIES, AND VERIFY FLOOR, CURB OR PAVEMENT ELEVATIONS WHERE MATCHING INTO EXISTING WORK. THE CONTRACTOR SHALL FIELD VERIFY HORIZONTAL CONTROL BY REFERENCING SHOWN COORDINATES TO KNOWN PROPERTY LINES. NOTIFY ENGINEER OF DISCREPANCIES IN EITHER VERTICAL OR HORIZONTAL CONTROL PRIOR TO PROCEEDING WITH WORK.
5. THE CONTRACTOR SHALL VERIFY THE EXACT LOCATION OF ALL UNDERGROUND UTILITIES PRIOR TO BEGINNING CONSTRUCTION (CALL DIGGERS HOTLINE AT 800-242-8511). COST OF REPLACEMENT OR REPAIR OF EXISTING UTILITIES DAMAGED AS A RESULT OF THE CONTRACTOR'S OPERATION SHALL BE THE CONTRACTOR'S RESPONSIBILITY.
6. EXISTING UTILITY INFORMATION IS SHOWN FROM SURVEY WORK, FIELD OBSERVATIONS, AVAILABLE PUBLIC RECORDS, AND AS-BUILT DRAWINGS. EXACT LOCATIONS AND ELEVATIONS OF UTILITIES SHALL BE DETERMINED PRIOR TO INSTALLING NEW WORK. EXCAVATE TEST PITS AS REQUIRED.
7. PROPERTY CORNERS SHALL BE CAREFULLY PROTECTED UNTIL THEY HAVE BEEN REFERENCED BY A PROFESSIONAL LAND SURVEYOR. PROPERTY MONUMENTS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
8. ENGINEER SHALL BE NOTIFIED 48 HOURS IN ADVANCE OF PERFORMING ANY CONSTRUCTION.
9. ALL TRENCHING SHALL BE PERFORMED ACCORDING TO OSHA STANDARDS.
10. ALL ITEMS SHALL INCLUDE ALL THE NECESSARY MATERIALS AND LABOR TO COMPLETE THE ITEM IN PLACE.
11. THE CONTRACTOR SHALL CLEAN ALL ADJACENT STREETS OF ANY SEDIMENT OR DEBRIS BY SWEEPING BEFORE THE END OF THE WORKING DAY OR AS REQUIRED BY THE CITY OF PEWAUKEE.
12. THE CONTRACTOR MUST AND IS REQUIRED TO NOTIFY THE CITY OF PEWAUKEE SEWER AND WATER UTILITY A MINIMUM OF 48 HOURS IN ADVANCE OF WATER MAIN FILLING, WATER MAIN FLUSHING, WATER MAIN TESTING AND WATER MAIN CONNECTIONS. NO CONNECTION SHALL BE MADE TO THE EXISTING WATER SYSTEM UNTIL SAFE SAMPLES FROM THE NEW MAIN HAVE BEEN OBTAINED.



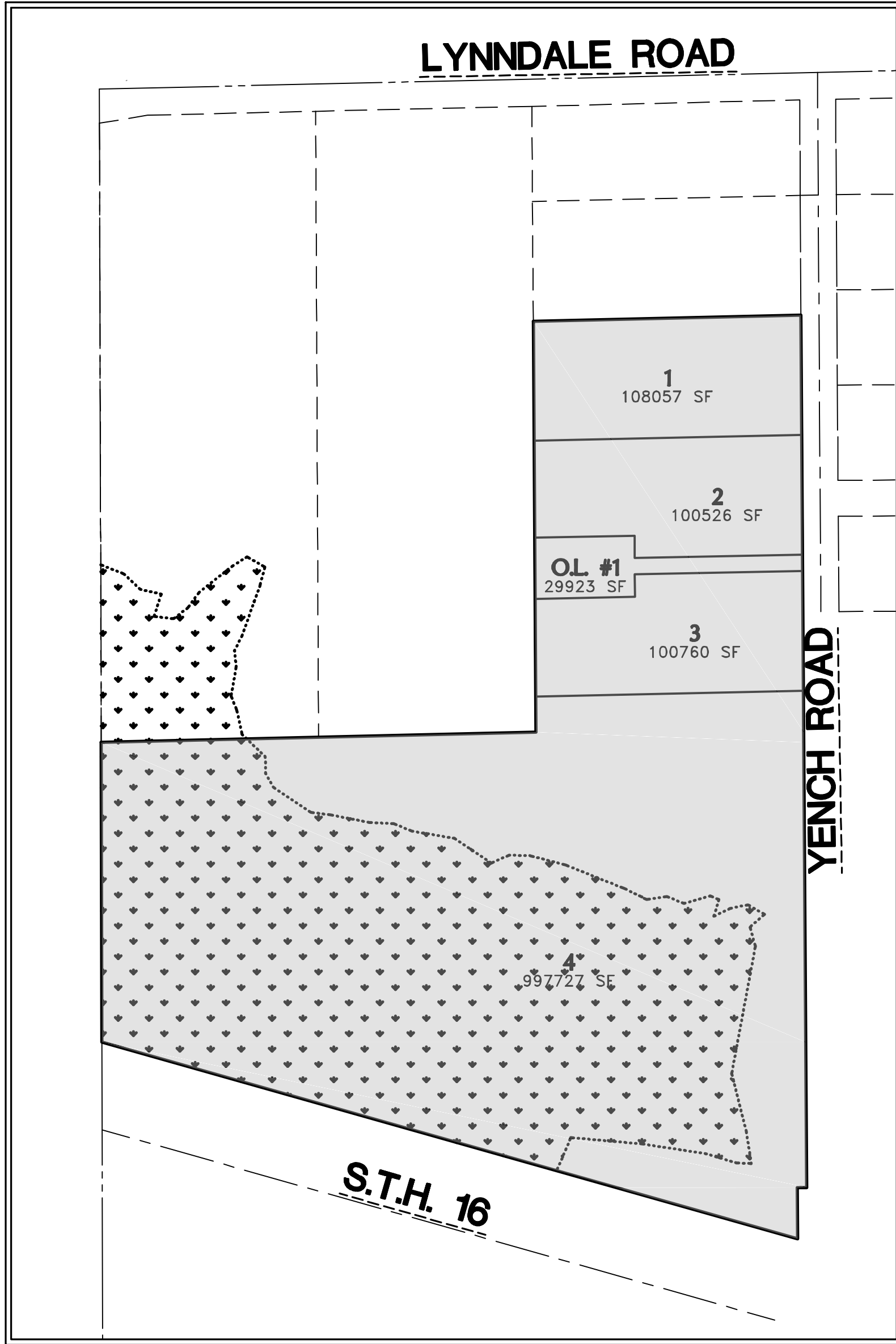
LOCATION MAP
NOT TO SCALE

LYNNDALE ROAD CSM

SINGLE-FAMILY SUBDIVISION

SITE DEVELOPMENT PLANS

CITY OF PEWAUKEE, WISCONSIN



SITE PLAN
SCALE: 1" = 200'

ENGINEER:
TRIO ENGINEERING, LLC
4100 N. CALHOUN ROAD, SUITE 300
BROOKFIELD, WI 53005
CONTACT: MR. DAN SCHERER, P.E.
PHONE: 262-790-1480
FAX: 262-790-1481
EMAIL: dscherer@trioeng.com

SHEET INDEX

CIVIL	
T1	- COVER SHEET
C1.0	- PROPOSED DEVELOPMENT PLAN
C1.1	- EXISTING SITE PLAN
C2.0	- OVERALL GRADING PLAN
C2.1	- GRADING AND DRAINAGE PLAN
C2.2	- POND PLANS AND DETAILS
C3.0	- EROSION CONTROL PLAN
C4.0	- CONSTRUCTION NOTES & DETAILS



PROJECT:
LYNNDALE ROAD CSM
CITY OF PEWAUKEE, WISCONSIN

BY: YENCH LLC
3055 N. BROOKFIELD RD.
BROOKFIELD, WI 53045

REVISION HISTORY

DATE	DESCRIPTION
11/03/2023	PLAN COMMISSION
11/29/2023	PER REVIEW COMMENTS

DATE:
NOVEMBER 29, 2023

JOB NUMBER:
16015

DESCRIPTION:
COVER SHEET

SHEET

T1

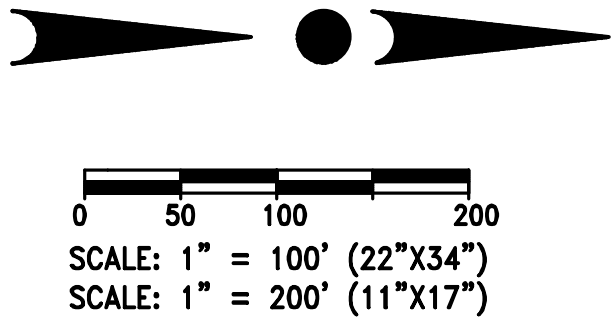
APPLICABLE ZONING SETBACKS

<p><u>A-2 ZONING SETBACKS (LOT 4)</u></p> <ul style="list-style-type: none">- FARM BUILDINGS = 100' EACH SIDE INCLUDING STREET- DWELLINGS & RELATED ACCESSORY STRUCTURES = 50' EACH SIDE INCLUDING STREET- DETACHED STRUCTURES = 25' ON EACH SIDE- OTHER DETACHED USE PRINCIPLE STRUCTURES = 25' EACH SIDE- WETLANDS = 25'- 100 YEAR FLOODPLAIN = 75'	<p><u>RS-2 ZONING SETBACKS (LOTS 1-3)</u></p> <ul style="list-style-type: none">- FRONT = 45'- SIDE = 25'- REAR = 35'- WETLANDS 25'- 100 YEAR FLOODPLAIN = 75'
<p><u>NOTES:</u></p> <ul style="list-style-type: none">• BUILDING PADS SHOWN ARE APPROXIMATE AND MAY DIFFER FROM LOCATION SHOWN WHEN LOTS ARE DEVELOPED.• CONTRACTOR SHALL CONDUCT INFILTRATION TESTING AT THE LOCATION OF THE PROPOSED STORMWATER BASIN. IF RESULTS ARE LESS THEN X.XX IN/HR. THE PROPOSED BASIN NOTION AND THE DESIGN SHALL BE ADJUSTED ACCORDINGLY.• SEPTIC AREAS SHOWN ARE APPROXIMATE AND MAY VARY WHEN LOTS ARE DEVELOPED, BUT SHALL BE PROTECTED DURING SITE GRADING OPERATIONS ASSOCIATED WITH THE STORM WATER DESIGN.	

- FARM BUILDINGS = 100' EACH SIDE INCLUDING STREET
- DWELLINGS & RELATED ACCESSORY STRUCTURES = 50' EACH SIDE INCLUDING STREET
- DETACHED STRUCTURES = 25' ON EACH SIDE
- OTHER DETACHED USE PRINCIPLE STRUCTURES = 25' EACH SIDE
- WETLANDS = 25'
- 100 YEAR FLOODPLAIN = 75'

- FRONT = 45'
- SIDE = 25'
- REAR = 35'
- WETLANDS 25'
- 100 YEAR FLOODPLAIN = 75'

- * BUILDING PADDS SHOWN ARE APPROXIMATE AND MAY DIFFER FROM LOCATION SHOWN WHEN LOTS ARE DEVELOPED.
- * CONTRACTOR SHALL CONDUCT INFILTRATION TESTING AT THE LOCATION OF THE PROPOSED STORMWATER BASIN. IF RESULTS ARE LESS THEN X.XX IN/HR, THE DESIGNER SHALL BE NOTIFIED AND THE DESIGN SHALL BE ADJUSTED ACCORDINGLY.
- * SEPTIC AREAS SHOWN ARE APPROXIMATE AND MAY VARY WHEN LOTS ARE DEVELOPED BUT SHALL BE PROTECTED DURING SITE GRADING OPERATIONS ASSOCIATED WITH THE STORM WATER DESIGN.



4100 N. CALHOUN RD., SUITE 300
BROOKFIELD, WI 53005
PHONE: (262) 790-1480
FAX: (262) 790-1481
EMAIL: jpudelko@trioeng.com

**BY: YENCH LLC
3055 N. BROOKFIELD RD.
BROOKFIELD, WI 53045**

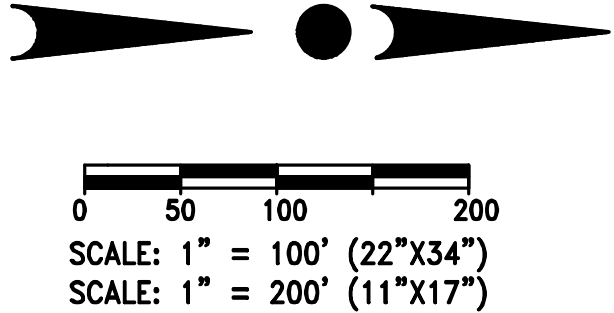
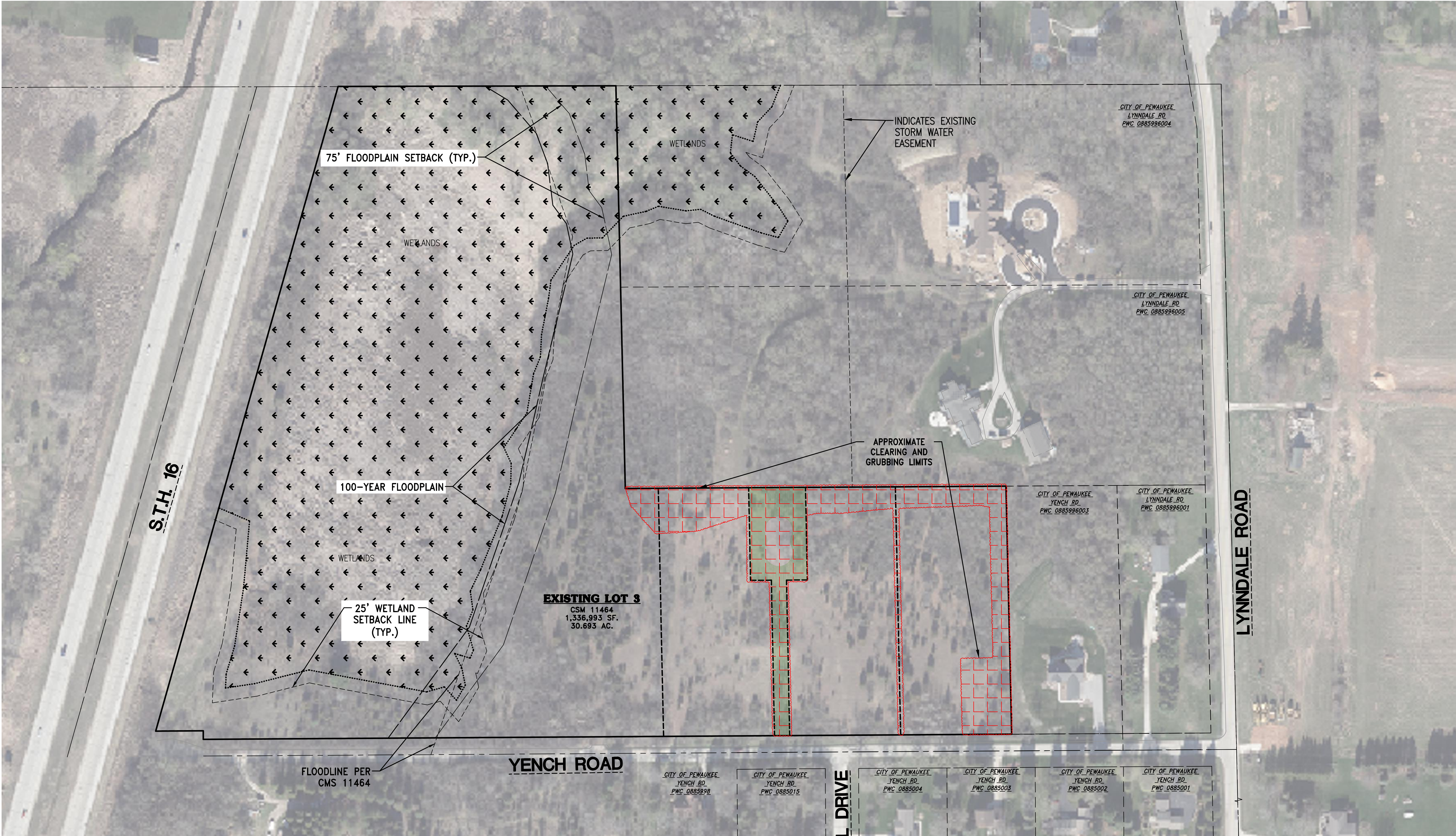
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11/29/2023	PER REVIEW COMMENTS

JOB NUMBER:
16015

SHEET

C1.0

H:\C400\467\16015-01-YENCH ROAD\CONSTRUCTION PLANS\YENCH ROAD_CIVIL PLAN SET_22X34.DWG



DANIEL A. SCHELL
E-4820-6
FORT ATKINSON, WI

TRIO

4100 N. CALHOUN RD., SUITE 300
BROOKFIELD, WI 53005
PHONE: (262) 790-1480
FAX: (262) 790-1481
EMAIL: jpudetto@trioeng.com

PROJECT:
LYNNDALE ROAD CSM
CITY OF PEWAUKEE, WISCONSIN

BY: YENCH LLC
3055 N. BROOKFIELD RD.
BROOKFIELD, WI 53045

REVISION HISTORY	
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11/03/2023	PLAN COMMISSION
11/29/2023	PER REVIEW COMMENTS

DATE:
NOVEMBER 29, 2023

JOB NUMBER:
16015

DESCRIPTION:
EXISTING SITE PLAN

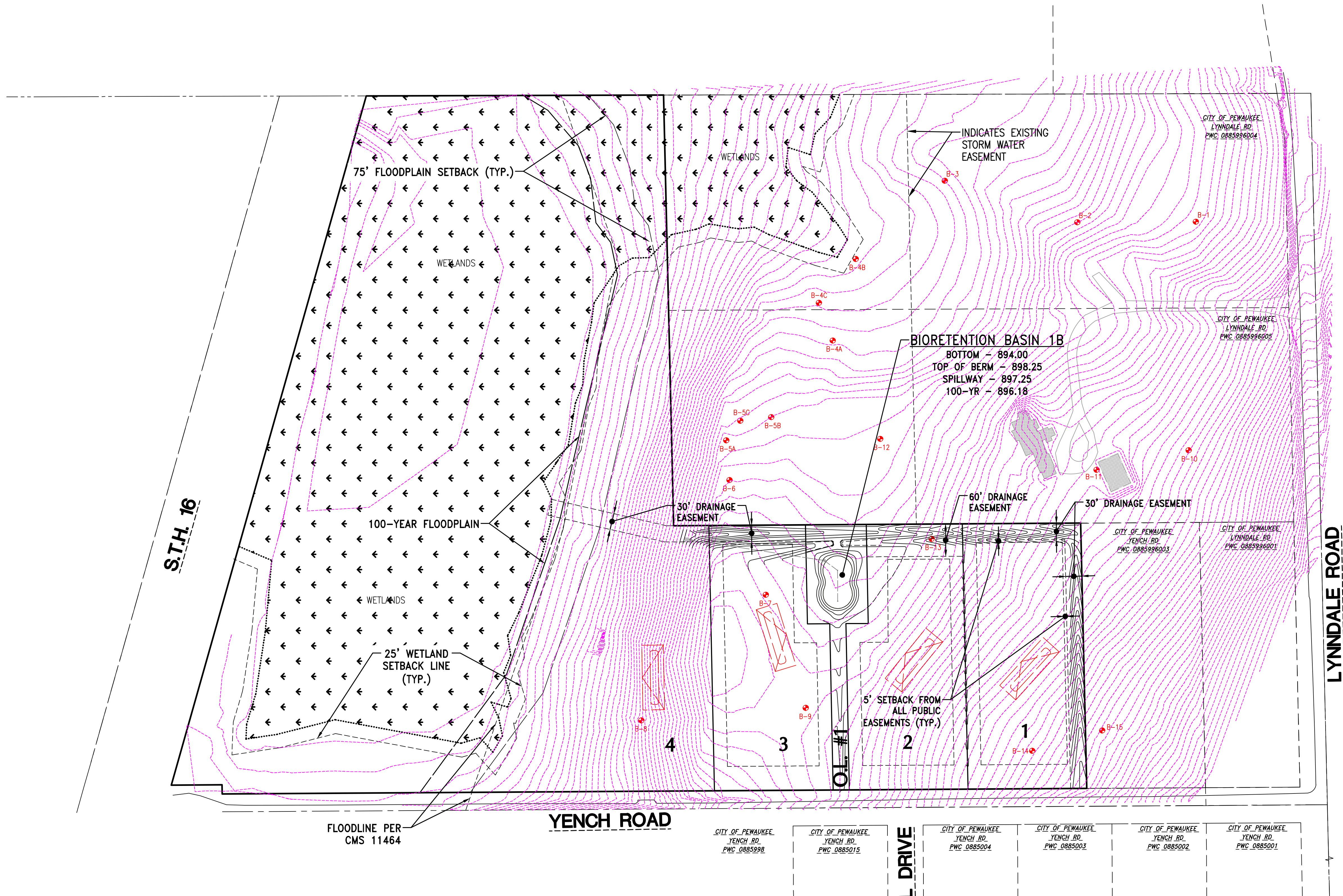
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CONTRACTOR IS REQUIRED TO CONTACT DIGGERS HOTLINE TOLL FREE TO OBTAIN LOCATION OF UNDERGROUND UTILITIES PRIOR TO COMMENCING THE WORK. WISCONSIN STATUTE 182.0715 REQUIRES MIN. OF 3 WORK DAYS NOTICE BEFORE YOU EXCAVATE.
CALL DIGGERS HOTLINE 1-800-242-8511

NOTE:
EXISTING UNDERGROUND UTILITY INFORMATION WAS OBTAINED FROM AVAILABLE RECORDS. THE ENGINEER MAKES NO GUARANTEE AS TO THE ACCURACY OF THIS INFORMATION. VERIFICATION TO THE SATISFACTION OF THE CONTRACTOR OF ALL UNDERGROUND UTILITIES, WHETHER OR NOT SHOWN ON THE PLANS, SHALL BE ASSUMED AS A CONDITION OF THE CONTRACT. THE CONTRACTOR SHALL NOTIFY THE ENGINEER OF ANY DISCREPANCIES BETWEEN LOCATION OF UTILITIES IN THE FIELD AND LOCATIONS SHOWN ON THE PLANS.



0 50 100 200
SCALE: 1" = 100' (22"x34")
SCALE: 1" = 200' (11"x17")



PROJECT:
LYNNDALE ROAD CSM
CITY OF PEWAUKEE, WISCONSIN

BY:
YENCH LLC
3055 N. BROOKFIELD RD.
BROOKFIELD, WI 53045

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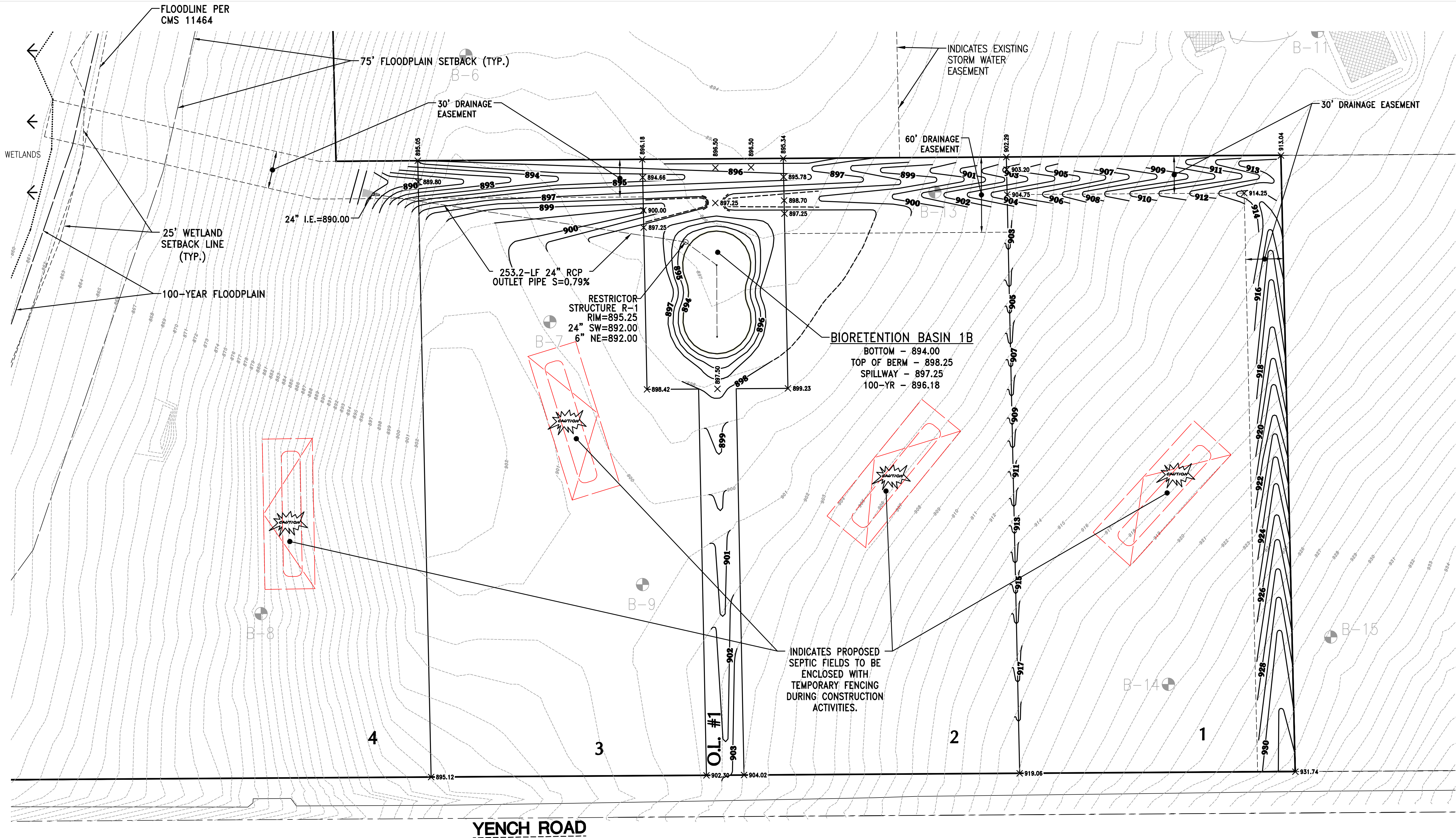
DATE:
NOVEMBER 29, 2023

JOB NUMBER:
16015

DESCRIPTION:
**MASTER GRADING
& DRAINAGE PLAN**

SHEET
C2.0

HN\C400\467\16015-01-YENCH ROAD\CONSTRUCTION PLANS\YENCH ROAD\CIVIL PLAN SET_22X34.DWG



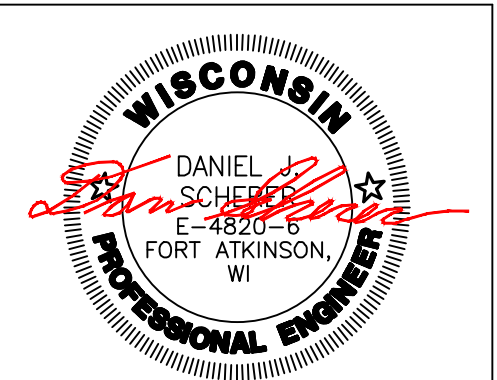
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0 20 40 80

SCALE: 1" = 40' (22"x34")
SCALE: 1" = 80' (11"x17")



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BROOKFIELD, WI 53005
PHONE: (262) 790-1480
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EMAIL: jpudefio@trioeng.com

PROJECT:
LYNNDALE ROAD CSM
CITY OF PEWAUKEE, WISCONSIN

BY: YENCH LLC
3055 N. BROOKFIELD RD.
BROOKFIELD, WI 53045

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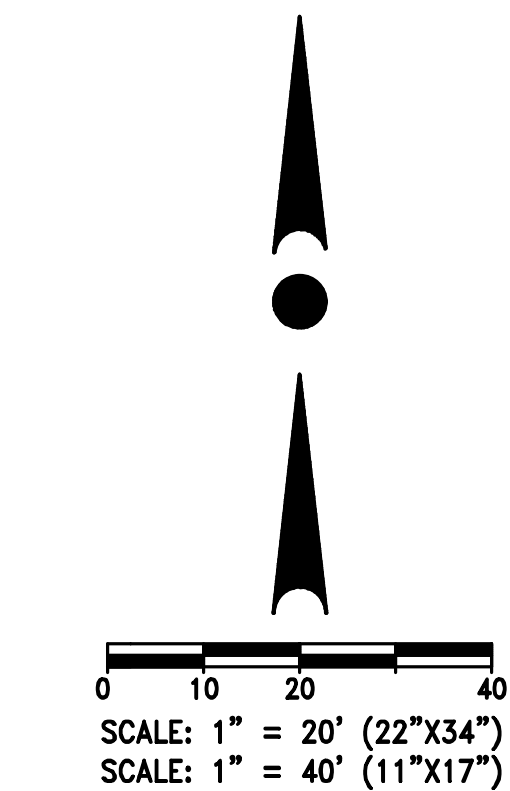
JOB NUMBER:
16015

DESCRIPTION:
**MASTER GRADING
& DRAINAGE PLAN**

SHEET

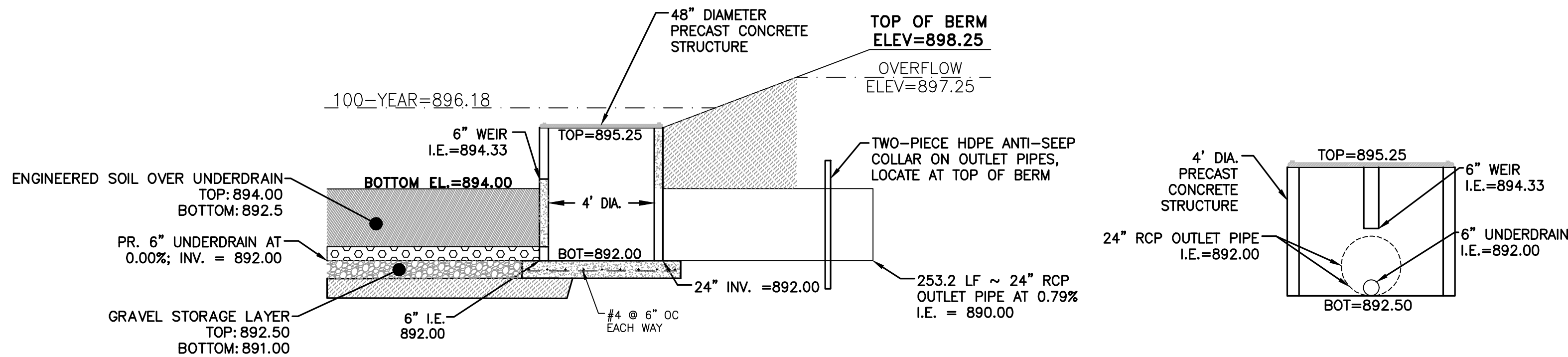
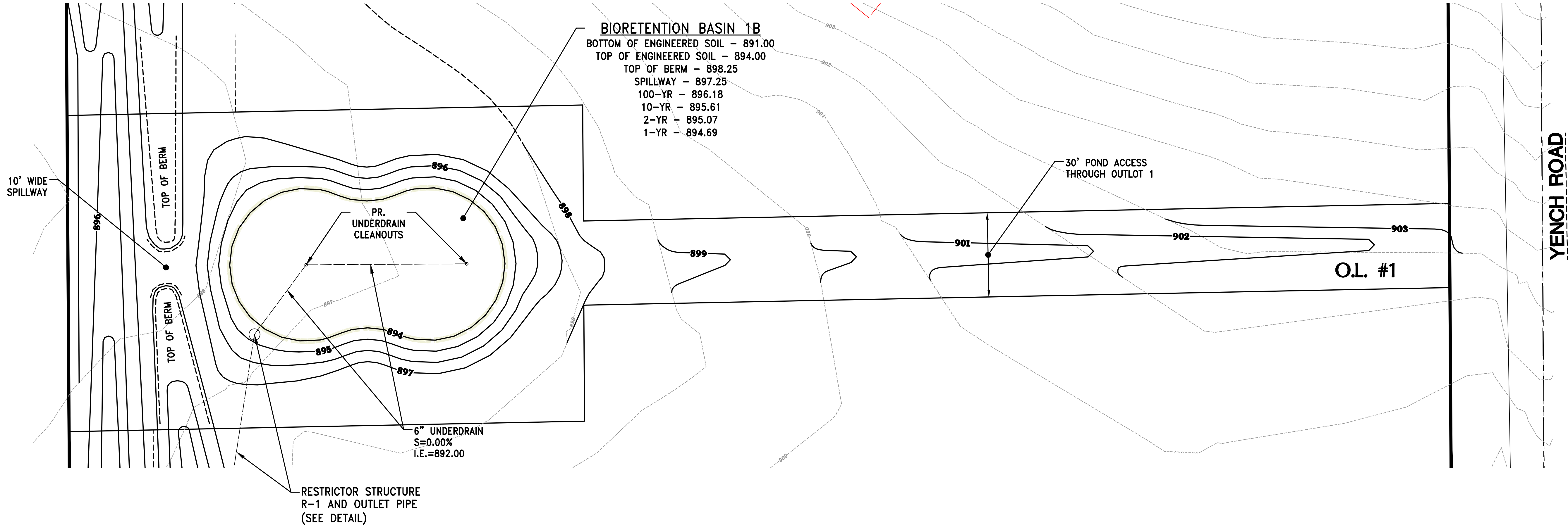
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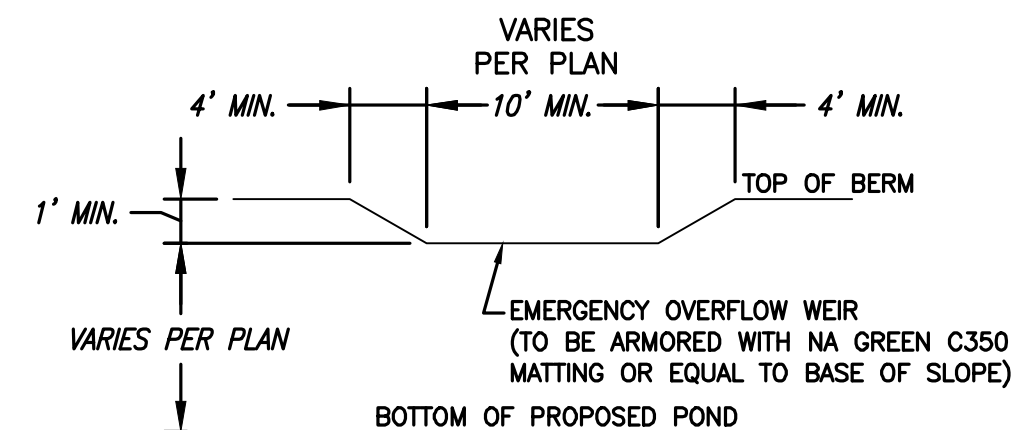
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BIORETENTION BASIN 1B & RESTRICTOR STRUCTURE R-1 DETAIL
NOT TO SCALE

TWO-PIECE ANTI-SEEP COLLAR NOTES:
- ANTI-SEEP COLLAR MATERIAL SHALL MATCH OUTLET PIPE MATERIAL.
- ANTI-SEEP COLLARS SHALL BE 1/4" POLYETHYLENE SHEETS WITH COLLAR FOR PIPE; DIMENSIONS SHALL BE AS NOTED ON THE DETAIL.
- INSTALL ANTI-SEEP COLLARS PER MANUFACTURER SPECIFICATIONS.



TYPICAL EMERGENCY OVERFLOW WEIR DETAIL
NOT TO SCALE
NOTE: GRADES FOR TOP OF BERM, OVERFLOW SPILLWAY, AND BOTTOM OF POND SHALL BE PER PLAN.



PROJECT:
LYNNDALE ROAD CSM
CITY OF PEWAUKEE, WISCONSIN

BY:
YENCH LLC
3055 N. BROOKFIELD RD.
BROOKFIELD, WI 53045

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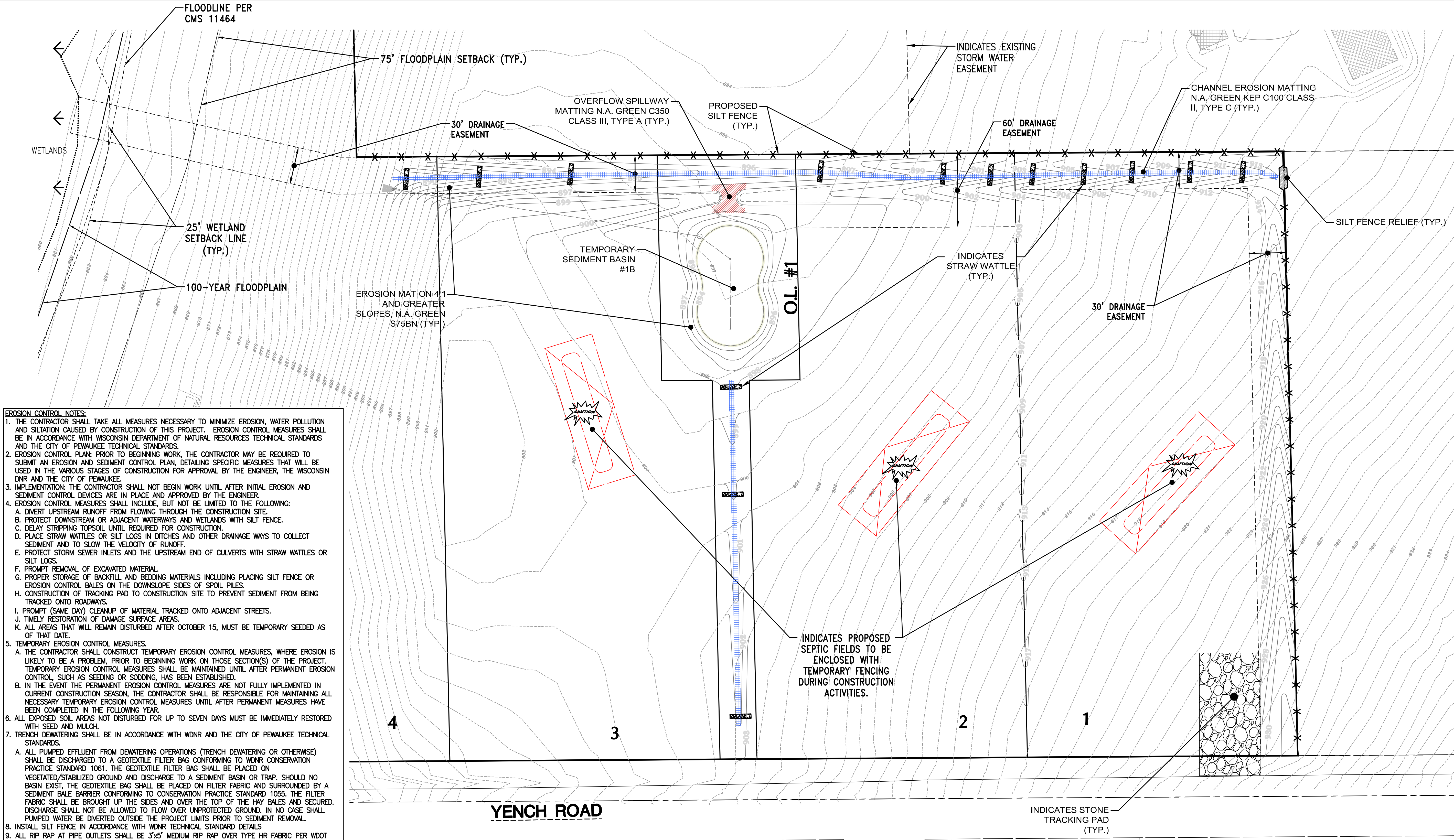
JOB NUMBER:
16015

DESCRIPTION:
POND DETAIL

SHEET

C2.2

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- EROSION CONTROL NOTES:**
1. THE CONTRACTOR SHALL TAKE ALL MEASURES NECESSARY TO MINIMIZE EROSION, WATER POLLUTION AND SILTATION CAUSED BY CONSTRUCTION OF THIS PROJECT. EROSION CONTROL MEASURES SHALL BE IN ACCORDANCE WITH WISCONSIN DEPARTMENT OF NATURAL RESOURCES TECHNICAL STANDARDS AND THE CITY OF PEWAUKEE TECHNICAL STANDARDS.
 2. EROSION CONTROL PLAN: PRIOR TO BEGINNING WORK, THE CONTRACTOR MAY BE REQUIRED TO SUBMIT AN EROSION AND SEDIMENT CONTROL PLAN, DETAILING SPECIFIC MEASURES THAT WILL BE USED IN THE VARIOUS STAGES OF CONSTRUCTION FOR APPROVAL BY THE ENGINEER, THE WISCONSIN DNR AND THE CITY OF PEWAUKEE.
 3. IMPLEMENTATION: THE CONTRACTOR SHALL NOT BEGIN WORK UNTIL AFTER INITIAL EROSION AND SEDIMENT CONTROL DEVICES ARE IN PLACE AND APPROVED BY THE ENGINEER.
 4. EROSION CONTROL MEASURES SHALL INCLUDE, BUT NOT BE LIMITED TO THE FOLLOWING:
 - A. DIVERT UPSTREAM RUNOFF FROM FLOWING THROUGH THE CONSTRUCTION SITE.
 - B. PROTECT DOWNSTREAM OR ADJACENT WATERWAYS AND WETLANDS WITH SILT FENCE.
 - C. DELAY STRIPPING TOPSOIL UNTIL REQUIRED FOR CONSTRUCTION.
 - D. PLACE STRAW WATTLES OR SILT LOGS IN DITCHES AND OTHER DRAINAGE WAYS TO COLLECT SEDIMENT AND TO SLOW THE VELOCITY OF RUNOFF.
 - E. PROTECT STORM SEWER INLETS AND THE UPSTREAM END OF CULVERTS WITH STRAW WATTLES OR SILT LOGS.
 - F. PROMPT REMOVAL OF EXCAVATED MATERIAL.
 - G. PROPER STORAGE OF BACKFILL AND BEDDING MATERIALS INCLUDING PLACING SILT FENCE OR EROSION CONTROL BALES ON THE DOWNSLOPE SIDES OF SPOIL PILES.
 - H. CONSTRUCTION OF TRACKING PAD TO CONSTRUCTION SITE TO PREVENT SEDIMENT FROM BEING TRACKED ONTO ROADWAYS.
 - I. PROMPT (SAME DAY) CLEANUP OF MATERIAL TRACKED ONTO ADJACENT STREETS.
 - J. TIMELY RESTORATION OF DAMAGE SURFACE AREAS.
 - K. ALL AREAS THAT WILL REMAIN DISTURBED AFTER OCTOBER 15, MUST BE TEMPORARY SEEDING AS OF THAT DATE.
 5. TEMPORARY EROSION CONTROL MEASURES.
 - A. THE CONTRACTOR SHALL CONSTRUCT TEMPORARY EROSION CONTROL MEASURES, WHERE EROSION IS LIKELY TO BE A PROBLEM, PRIOR TO BEGINNING WORK ON THOSE SECTION(S) OF THE PROJECT. TEMPORARY EROSION CONTROL MEASURES SHALL BE MAINTAINED UNTIL AFTER PERMANENT EROSION CONTROL, SUCH AS SEEDING OR SODDING, HAS BEEN ESTABLISHED.
 - B. IN THE EVENT THE PERMANENT EROSION CONTROL MEASURES ARE NOT FULLY IMPLEMENTED IN CURRENT CONSTRUCTION SEASON, THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING ALL NECESSARY TEMPORARY EROSION CONTROL MEASURES UNTIL AFTER PERMANENT MEASURES HAVE BEEN COMPLETED IN THE FOLLOWING YEAR.
 6. ALL EXPOSED SOIL AREAS NOT DISTURBED FOR UP TO SEVEN DAYS MUST BE IMMEDIATELY RESTORED WITH SEED AND MULCH.
 7. TRENCH DEWATERING SHALL BE IN ACCORDANCE WITH WDNR AND THE CITY OF PEWAUKEE TECHNICAL STANDARDS.
 - A. ALL PUMPED EFFLUENT FROM DEWATERING OPERATIONS (TRENCH DEWATERING OR OTHERWISE) SHALL BE DISCHARGED TO A GEOTEXTILE FILTER BAG CONFORMING TO WDNR CONSERVATION PRACTICE STANDARD 1061. THE GEOTEXTILE FILTER BAG SHALL BE PLACED ON VEGETATED/STABILIZED GROUND AND DISCHARGE TO A SEDIMENT BASIN OR TRAP. SHOULD NO BASIN EXIST, THE GEOTEXTILE BAG SHALL BE PLACED ON FILTER FABRIC AND SURROUNDED BY A SEDIMENT BALE BARRIER CONFORMING TO CONSERVATION PRACTICE STANDARD 1055. THE FILTER FABRIC SHALL BE BROUGHT UP THE SIDES AND OVER THE TOP OF THE HAY BALES AND SECURED. DISCHARGE SHALL NOT BE ALLOWED TO FLOW OVER UNPROTECTED GROUND. IN NO CASE SHALL PUMPED WATER BE DIVERTED OUTSIDE THE PROJECT LIMITS PRIOR TO SEDIMENT REMOVAL.
 8. INSTALL SILT FENCE IN ACCORDANCE WITH WDNR TECHNICAL STANDARD DETAILS.
 9. ALL RIP RAP AT PIPE OUTLETS SHALL BE 3"x5" MEDIUM RIP RAP OVER TYPE HR FABRIC PER WDOT SPECIFICATIONS UNLESS OTHERWISE DESIGNATED.
 10. EROSION CONTROL INSPECTION AND MAINTENANCE.
 - A. INSPECT EROSION CONTROL MEASURES WEEKLY AND AFTER SITE RECEIVES 0.5" OF RAIN IN A 24-HOUR PERIOD DURING CONSTRUCTION.
 - B. REPAIR OR REPLACE EROSION CONTROLS THAT ARE DAMAGED OR FAILING.
 11. ALL AREAS TO BE SEEDING AND MULCHED SHALL USE WDOT SEED MIXTURE NO. 20. MULCH SHALL APPLIED PER WDOT SPECIFICATIONS USING METHOD B FOR PLACEMENT.



CONTRACTOR IS REQUIRED TO CONTACT DIGGERS HOTLINE TOLL FREE TO OBTAIN LOCATION OF UNDERGROUND UTILITIES PRIOR TO COMMENCING THE WORK. WISCONSIN STATUTE 182.0715 REQUIRES MIN. OF 3 WORK DAYS NOTICE BEFORE YOU EXCAVATE.
CALL DIGGERS HOTLINE 1-800-242-8511

NOTE:
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TEMPORARY DEWATERING GEOTEXTILE BAG

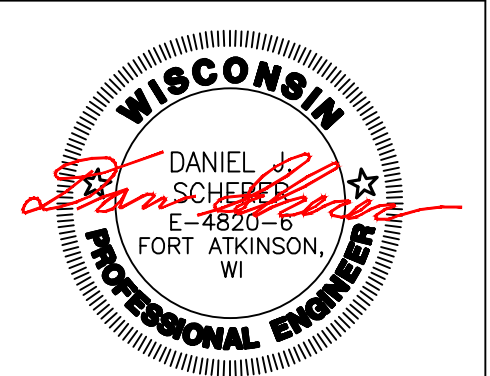
1. IF DEWATERING IS REQUIRED, A GEOTEXTILE DEWATERING BAG MEETING THE SPECIFICATIONS IN THE TABLE TO THE RIGHT SHALL BE UTILIZED AS STATED ON THE EROSION CONTROL PLAN.
2. REFER TO WDNR TECHNICAL STANDARDS 1061 AND MANUFACTURERS REQUIREMENTS FOR INSTALLATION AND MAINTENANCE REQUIREMENTS.
3. CONTRACTOR MAY USE THRACE-LINO, INC 275EX GEOTEXTILE BAG, OR FUNCTIONAL EQUIVALENT TO MEET TECHNICAL STANDARD REQUIREMENTS.
4. CONTRACTOR MAY INSTALL HAY BALES AROUND DEWATERING BAG FOR STABILIZATION AS DICTATED BY SITE CONDITIONS.
5. MAX FLOW RATE SHALL NOT EXCEED 100GPM

Property	Test Method	Type I Value
Maximum Apparent Opening Sizes	ASTM D-4751	0.212 mm
Grab Tensile Strength	ASTM D-4632	200 lbs.
Mullen Burst	ASTM D-3786	350 psi
Permeability	ASTM D-4491	0.28 cm/sec
Fabric	Nominal Representative Weight	8 oz

WISCONSIN DEPARTMENT OF NATURAL RESOURCES.
CONSERVATION PRACTICE STANDARD:
1052 - NON-CHANNEL EROSION MAT
1053 - CHANNEL EROSION MAT
1055 - SEDIMENT BALE BARRIER (NON-CHANNEL)
1056 - SILT FENCE
1062 - DITCH CHECK
1057 - STONE TRACKING PAD AND TIRE WASHING
1058 - MULCHING FOR CONSTRUCTION SITES
1059 - TEMPORARY SEEDING
1061 - DE-WATERING
1064 - SEDIMENT BASIN



0 20 40 80
SCALE: 1" = 40' (22"x34")
SCALE: 1" = 80' (11"x17")



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BROOKFIELD, WI 53005
PHONE: (262) 790-1480
FAX: (262) 790-1481
EMAIL: jpudefio@trioeng.com

PROJECT: LYNNDALE ROAD CSM CITY OF PEWAUKEE, WISCONSIN

BY: YENCH LLC
3055 N. BROOKFIELD RD.
BROOKFIELD, WI 53045

REVISION HISTORY

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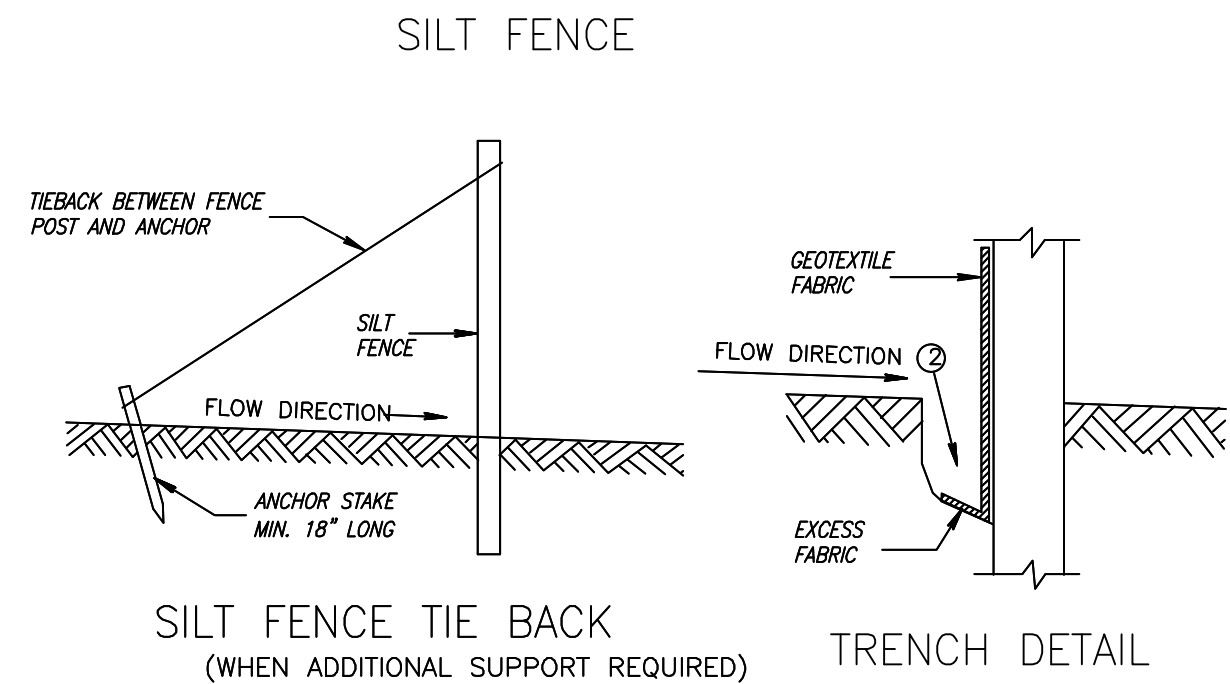
DATE:
NOVEMBER 29, 2023

JOB NUMBER:
16015

DESCRIPTION:
**EROSION
CONTROL PLAN**

SHEET

C3.0



NO SCALE

JOINING TWO LENGTHS OF SILT FENCE ⁽⁵⁾

- ① HORIZONTAL BRACE REQUIRED WITH 2" x 4" WOODEN FRAME OR EQUIVALENT AT TOP OF POSTS.
- ② TRENCH SHALL BE A MINIMUM OF 4" WIDE & 6" DEEP TO BURY AND ANCHOR THE GEOTEXTILE FABRIC. FOLD MATERIAL TO FIT TRENCH AND BACKFILL & COMPACT TRENCH WITH EXCAVATED SOIL.
- ③ WOOD POSTS SHALL BE A MINIMUM SIZE OF 12" x 12" OF OAK OR HICKORY.
- ④ SILT FENCE TO EXTEND ACROSS THE TOP OF THE PIPE.
- ⑤ CONSTRUCT SILT FENCE FROM A CONTINUOUS ROLL IF POSSIBLE BY CUTTING LENGTHS TO AVOID JOINTS. IF A JOINT IS NECESSARY USE ONE OF THE FOLLOWING TWO METHODS: A) OVERLAP THE END POSTS AND TWIST OR ROTATE, AT LEAST 180 DEGREES; B) HOOK THE END OF EACH SILT FENCE LENGTH.



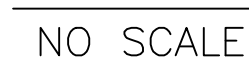
1. TEMPORARY SEED/STABILIZE BERMS AND DITCHES IMMEDIATELY AS ANY SECTION IS COMPLETED PER WisDOT STD. SPEC.
2. REFER TO WISCONSIN DNR TECHNICAL STANDARD 1066 FOR ADDITIONAL INFORMATION.

NO SCALE



1. ALL MATERIAL TO MEET MANUFACTURER'S SPECIFICATIONS
2. REFER TO MANUFACTURERS AND/OR SUPPLIERS.
SPECIFICATIONS FOR INSTALLATION RECOMMENDATIONS.
3. ACTUAL LOCATION MAY BE FIELD ADJUSTED TO MINIMIZE
DISTURBANCE TO EXISTING VEGETATION.
4. INSTALL EVERY 200' OR 2' GRADE DROP IN SWALES.

NO SCALE



**YENCH LLC
3055 N. BROOKFIELD RD.
BROOKFIELD, WI 53045**

REVISION HISTORY

DATE:

NOVEMBER 29, 2023

JOB NUMBER:
16015

DESCRIPTION:

**CONSTRUCTION
NOTES & DETAILS**

SHEET

C4.0



Department of Public Works

W240N3065 Pewaukee Road

Pewaukee, WI 53072

Phone: (262) 691-0804 • Fax: (262) 691-5729

Email: publicworks@pewaukee.wi.us

MEMORANDUM

TO: Common Council

FROM: Magdelene Wagner, Director of Public Works

DATE: January 12, 2024

RE: Yench Road LLC CSM

In 2014, Mr. Mierow received approval (PC 6/19/2014 & CC 7/7/2014) to split a 2.4ac parcel off a larger parcel by CSM 11205 creating PWC0885996002 and 0885996003. A single family home was constructed on the 2.4 ac parcel (Lot 1 of the CSM).

In 2016, Mr. Mierow again received approval to further split the lot, but with conditions if he further subdivides the parcels, storm water management would be required. There were discussions on this matter at several PC and CC meetings (see attached). CSM 11464 was ultimately approved for 2-10 ac lots and 1-30 ac lot, but I believe with conditions that any further split of the land must include stormwater management for all lots previously developed. In order to assist with this, the developer placed a storm water easement across the 2-10 ac parcels on the CSM. Unfortunately, he did not define on the CSM who owns the easement or what can be done on this easement.

The developer is now seeking to split the 30 ac lot into 4 lots. He has attempted and claims that he cannot get stormwater management to meet our Chapter 19 ordinances to meet the conditions of the previous CSM approval. The owners of the 2-10ac parcels with the easements are now refusing him access to their properties within the easement area. He has contacted the WisDNR which only requires water quality for the current development with the statement that they still need to meet City Ordinances (I've had discussions with Pete Wood, WisDNR representative, and he concurs his statements do not preclude them from meeting local ordinances).

The plan commission took action on 12/21/2023 to approve the CSM against Staff's concerns regarding the conditions we believe are in place from the previous approvals. We had a long discussion but didn't feel they could deny this further split based on actions from the previous PC/CC action.

I believe the intent was clear in the previous approvals of land division and that the developer would be required to include stormwater management for all these parcels developed post 2003 if he further subdivided the parcel. I would note that all of these land splits occurred under the same developer.

This development is creating 4 single family lots in addition to the 3 already created from this previously large undeveloped lot. Since all these splits are post 2003, the City will be responsible for all the impervious area on these parcels under our MS4 permits (and especially challenging with the TMDL's that are currently being developed). Our ordinance has very few exceptions for developments of parcels without stormwater management. The previous parcels were developed with the contingency that if further development occurs, they will need to meet storm water for all the previous developments as well to ensure compliance with our ordinances. I don't believe that because the drainage easement that the developer recorded was not clear that it eliminates the conditions placed on this further split.

If stormwater management is not required for this development, the City will be required to compensate for this development somewhere in the City to meet our MS4 permit requirements. The City ordinance was passed in 2003 requiring storm water management for all new developments to protect the existing community from paying for the costs of development. This is extremely important for the City as regulations of storm water management are only increasing and land available for cost effective measures to overcompensate for the non-treated development (ie. Ponds) are limited and becoming costly to purchase. I would further note that this area has substantial wetlands nearby which may not be used for water quality and quantity treatment.

If the Council determines to exempt the current development from providing storm water management from the previous land splits, it should be clearly noted that this current development must meet our storm water ordinances. In addition, any further land division must meet City Ordinances even if it is less than 1 acre to minimize any further impacts to the City by this development.

ARCTM
(262) 542-8200

4092512

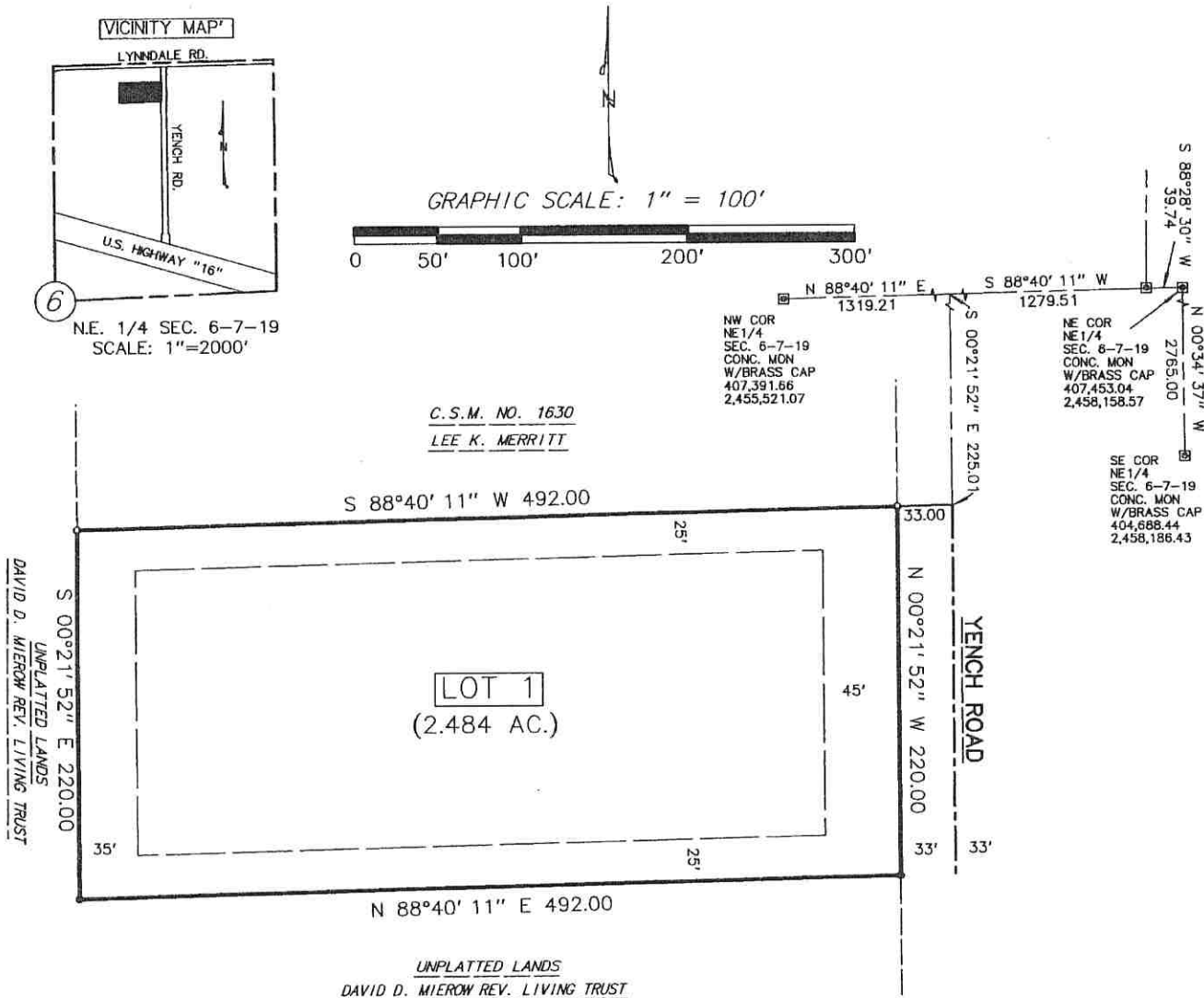


FORM ARC-101

PC # 140619-1

CERTIFIED SURVEY MAP NO. 11205

A PART OF THE NORTHWEST 1/4 OF THE NORTHEAST 1/4 OF SECTION 6, TOWNSHIP 7 NORTH
RANGE 19 EAST, IN THE CITY OF PEWAUKEE, WAUKESHA COUNTY WISCONSIN



LEGEND

- - DENOTES 1-INCH DIA. IRON PIPE FOUND
- - DENOTES 1-INCH DIA. IRON PIPE SET
1.13 LB./LINFT
18 INCHES IN LENGTH
- ⊠ - DENOTES CONC. MON.
W/ BRASS CAP END.

SURVEYED FOR:
DAVID D. MIEROW REVOCABLE TRUST
3055 N. BROOKFIELD RD.
BROOKFIELD, WI 53045

SURVEYED BY: MICHAEL W. BUECHL, INC.
233 OAKTON AVENUE
PEWAUKEE, WI 53072
262-691-4444

BEARINGS ARE REFERENCED TO THE WIS. STATE
PLANE COORDINATE SYSTEM, SOUTH ZONE (NAD 1927)
WITH THE WEST LINE OF THE NE 1/4 OF SECTION 6
T7N, R19E HAVING A BEARING OF NORTH 00°34' 37" WEST

THIS INSTRUMENT WAS DRAFTED BY MICHAEL W. BUECHL



Michael W Bueckel

PE-6211-W
SHEET 1 OF 3

CERTIFIED SURVEY MAP NO. 11205

A PART OF THE NORTHWEST 1/4 OF THE NORTHEAST 1/4 OF SECTION 6, TOWNSHIP 7
NORTH, RANGE 19 EAST, IN THE CITY OF PEWAUKEE, WAUKESHA COUNTY, WISCONSIN

SURVEYOR'S CERTIFICATE

State of Wisconsin)

Waukesha County)

I, Michael W. Buechl, a registered land surveyor, do hereby certify:

That I have surveyed, divided and mapped a parcel of land being a part of the Northwest 1/4 of the Northeast 1/4 of Section 6, Township 7 North, Range 19 East, in the City of Pewaukee, Waukesha County, Wisconsin, bounded and described as follows:

Commencing at the Northeast corner of said Northeast 1/4; thence South 88°28'30" West, along the North line of said Northeast 1/4, 39.74 feet to a point; thence North 88°40'11" West, along said North line 1279.51 feet to the centerline of Yench Road; thence South 00°21'52" East, along said centerline, 225.01 feet to a point; thence South 88°40'11" West, 33.00 feet to a point on the West line of Yench Road and the place of beginning of the lands to be described; continuing thence South 88°40'11" West, 492.00 feet; thence South 00°21'52" East, 220.00 feet; thence North 88°40'11" East, 492.00 feet to a point of the West line of Yench Road; thence North 00°21'52" West, along said West line, 220.00 feet to the place of beginning.

Containing 108,224 square feet (2.484472 acres) more or less.

That I have made this survey, land division and map by the direction of the owners of said land.

That such map is a correct representation of all exterior boundaries of the land surveyed and the division thereof made.

That I have fully complied with the provisions of Chapter 236 of the Wisconsin Statutes pertaining to Certified Survey Maps (Section 236.34 and the Regulations of the City of Pewaukee in surveying, dividing and mapping the same.

JUNE 3, 2014
Date

Michael W Buechl
Michael W. Buechl, Registered
Wisconsin Land Surveyor (S-1106)



ARCTM
(262) 542-8200

FORM ARC-101

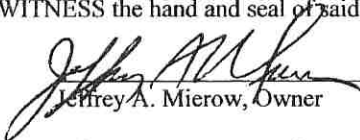
CERTIFIED SURVEY MAP NO. 11205

A PART OF THE NORTHWEST 1/4 OF THE NORTHEAST 1/4 OF SECTION 6, TOWNSHIP 7 NORTH, RANGE 19 EAST, IN THE CITY OF PEWAUKEE, WAUKESHA COUNTY, WISCONSIN

OWNER'S CERTIFICATE OF DEDICATION

AS OWNERS, David D Mierow Revocable Trust & Jeffrey A Mierow Et Al, We hereby certify that we have caused the land described in this document to be surveyed, divided, mapped and dedicated as represented on this map in accordance with the requirements of Chapter 236 of the Wisconsin Statutes pertaining to Certified Survey Maps (Section 236.34) and the Regulations of the City of Pewaukee

WITNESS the hand and seal of said owner this 4th day of June, 2014.


Jeffrey A. Mierow, Owner

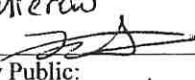

David D Mierow, Owner

State of Wisconsin)

Waukesha County)

PERSONALLY came before me this 4 day of June, 2014, Jeffrey A. Mierow known to be the person who executed the Owner's Certificate and acknowledged the same.
* and David D. Mierow

Notary Public:


Laura Schroeder


My commission expires: 4/28/17



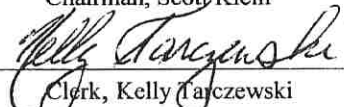
CERTIFICATE OF APPROVAL

RESOLVED that the above Certified Survey Map which has been filed for approval, be and is hereby approved as required by the Subdivision Regulations and Chapter 236 of Wisconsin Statutes relating to Certified Survey Maps. I hereby certify that the foregoing is a true and correct copy of a resolution adopted by the City Board of the City of Pewaukee on this 7th day of July, 2014.

Approved:


Chairman, Scott Klein

Signed:



Clerk, Kelly Tarczewski



PLANNING COMMISSION APPROVAL

APPROVED by the City of Pewaukee Planning Commission, this 19th day of June, 2014.

Approved:


Mayor, Scott Klein

Signed:


Secretary





This instrument was drafted by Michael W. Buechl.

4092512

REGISTER OF DEEDS
WAUKESHA COUNTY, WI
RECORDED ON

July 31, 2014 02:52 PM
James R Behrend
Register of Deeds
3 PGS
TOTAL FEE: \$30.00
TRANS FEE: \$0.00

Book 110 Page 136-138

Sheet 3 of 3



4214210

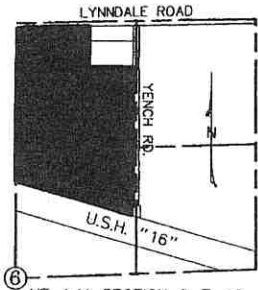
Book 114 Page 82-85



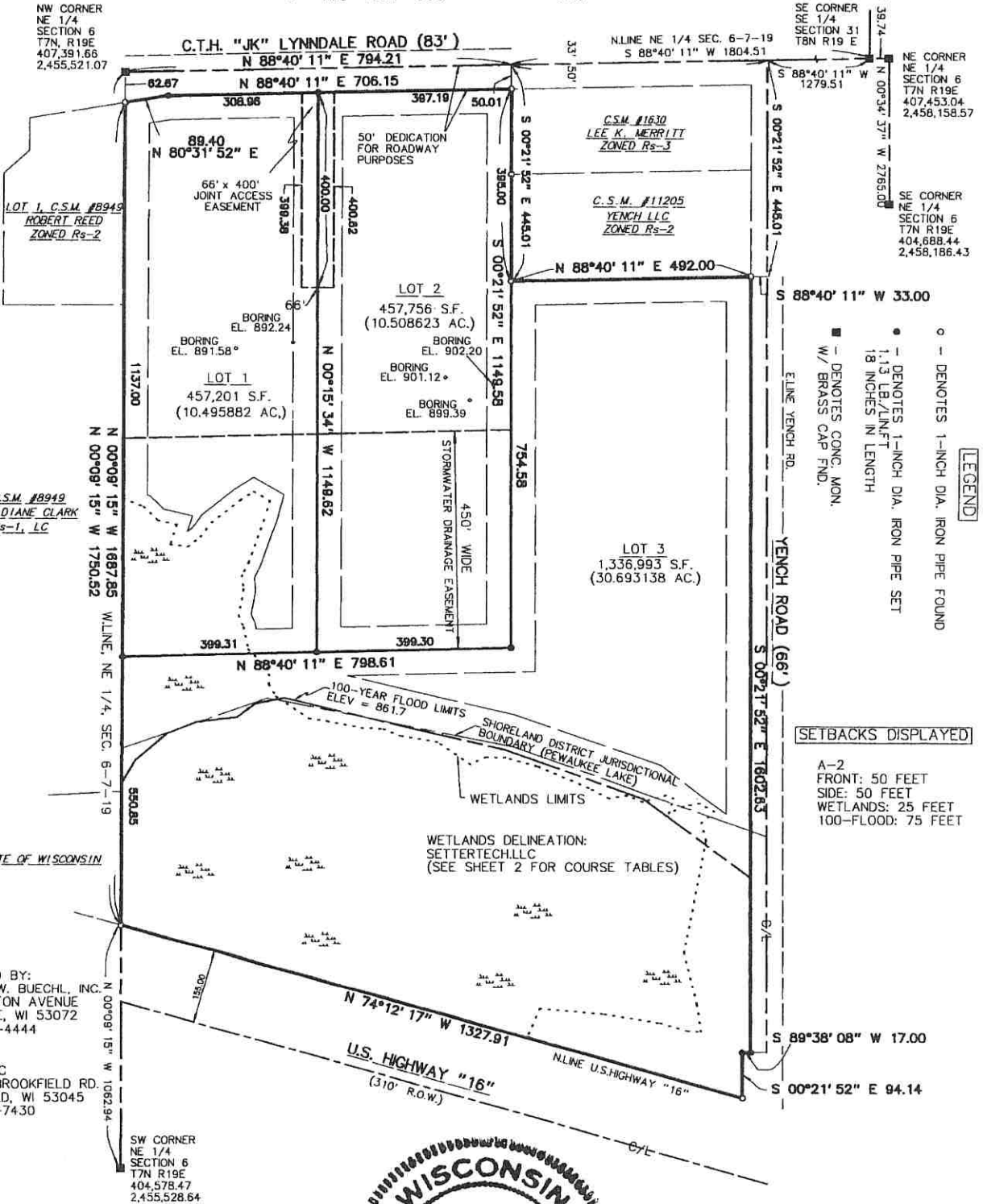
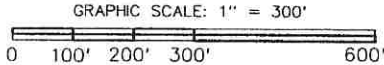
FORM ARC-101

CERTIFIED SURVEY MAP NO. 11404

A PART OF THE NORTHWEST 1/4 AND SOUTHWEST 1/4 OF
THE NORTHEAST 1/4 OF SECTION 6, TOWNSHIP 7 NORTH, RANGE 19 EAST
IN THE CITY OF PEWAUKEE, WAUKESHA COUNTY, WISCONSIN



NE 1/4 SECTION 6-7-19
SCALE: 1" = 2000'
VICINITY SKETCH



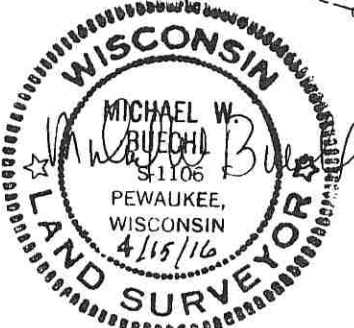
SURVEYED BY:
MICHAEL W. BUECHL, INC.
233 OAKTON AVENUE
PEWAUKEE, WI 53072
262-691-4444

OWNERS:
YENCH LLC
3055 N. BROOKFIELD RD.
BROOKFIELD, WI 53045
262-786-7430

BEARINGS ARE REFERENCED TO THE WIS. STATE
PLANE COORDINATE SYSTEM, SOUTH ZONE (NAD 1927)
WITH THE EAST LINE OF THE NE 1/4 OF SECTION 6
T7N, R19E HAVING A BEARING OF NORTH 00°34' 37" WEST

THIS INSTRUMENT WAS DRAFTED BY MICHAEL W. BUECHL

PE-6211-W



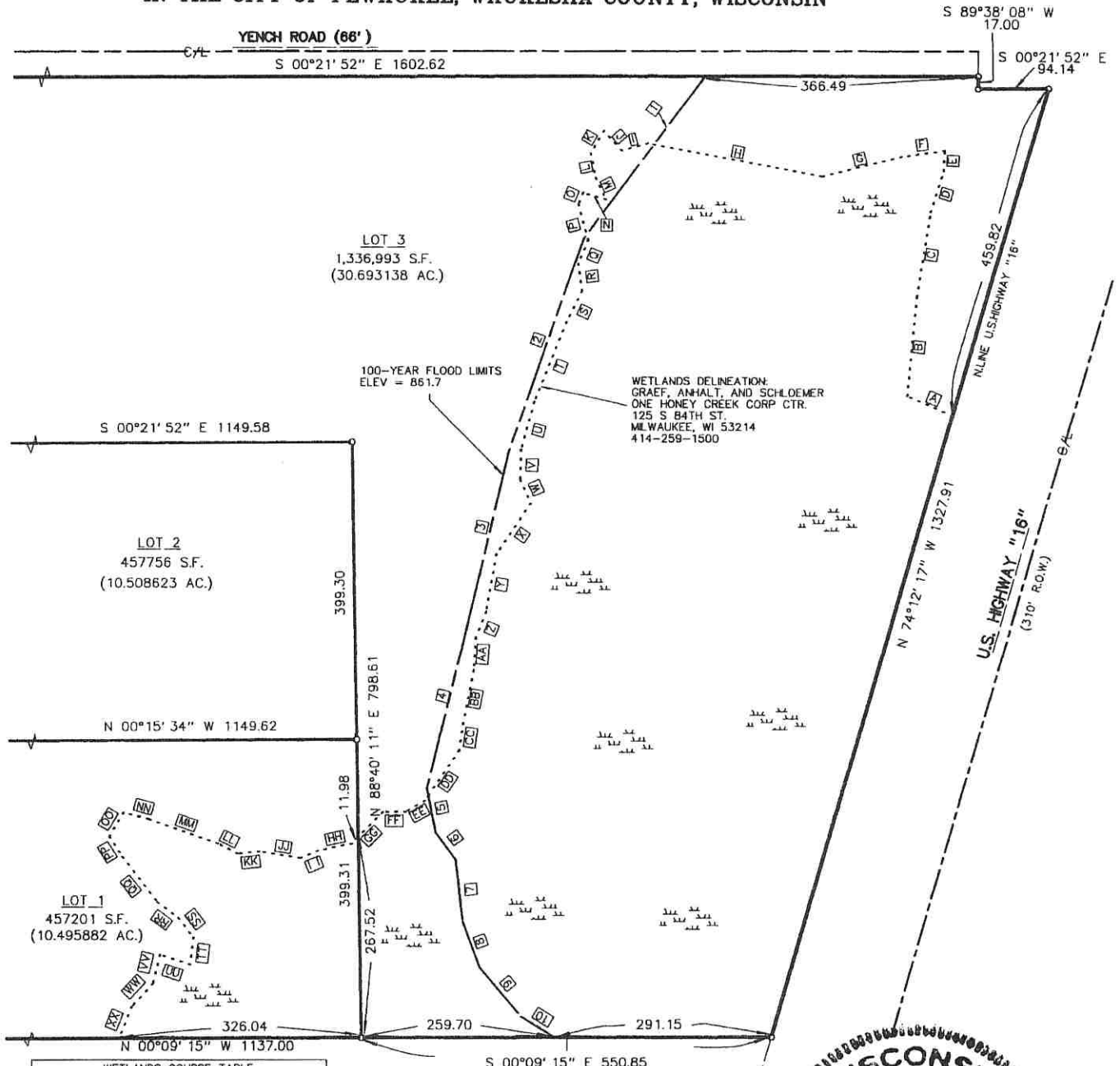
1. F-1 & LC ZONED LANDS SHALL NOT BE FILLED,
EXCAVATED, OR OTHERWISE DISTURBED
IN ANY MANNER.

2. VEHICULAR ACCESS TO C.T.H. "JK" PROHIBITED
EXCEPT THROUGH DESIGNATED JOINT ACCESS
EASEMENT.

SHEET 1 OF 4

CERTIFIED SURVEY MAP NO. 11464

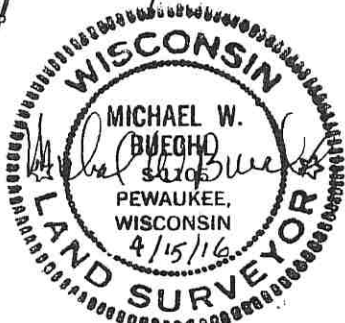
A PART OF THE NORTHWEST 1/4 AND SOUTHWEST 1/4 OF
THE NORTHEAST 1/4 OF SECTION 6, TOWNSHIP 7 NORTH, RANGE 19 EAST
IN THE CITY OF PEWAUKEE, WAUKESHA COUNTY, WISCONSIN



WETLANDS COURSE TABLE	
COURSE	BEARING & DISTANCE
A	N 23°08' 12" E 66.19
B	S 84°51' 33" E 133.64
C	S 81°15' 34" E 116.11
D	S 73°10' 58" E 58.12
E	S 87°33' 17" E 27.69
F	N 08°36' 08" W 58.32
G	N 14°09' 57" W 110.80
H	N 10°32' 38" E 237.87
I	N 16°00' 16" W 36.28
J	N 48°07' 29" E 35.80
K	N 59°52' 48" W 35.05
L	S 78°19' 44" W 34.08
M	S 63°51' 05" W 32.44
N	N 16°30' 08" E 32.09
O	N 67°40' 47" W 17.09
P	S 73°53' 58" W 49.87
Q	N 67°50' 10" W 34.40
R	S 81°36' 53" W 37.50
S	N 64°00' 41" W 46.73
T	N 68°09' 42" W 115.16
U	N 75°13' 52" W 66.50
V	N 88°35' 45" W 39.81
W	S 65°41' 48" W 35.28
X	N 56°11' 23" W 82.10
Y	N 80°25' 53" W 78.54
Z	N 67°53' 38" W 36.40
AA	N 87°07' 23" W 44.61
BB	N 79°00' 05" W 72.01
CC	N 82°14' 50" W 37.36
DD	N 56°14' 12" W 82.65
EE	N 29°29' 12" W 29.00

WETLANDS COURSE TABLE	
COURSE	BEARING & DISTANCE
FF	N 01°06' 33" W 30.85
GG	N 46°03' 03" W 59.32
HH	N 12°19' 14" W 46.55
II	N 22°22' 27" W 26.29
JJ	N 09°27' 06" E 55.46
KK	N 06°07' 28" W 29.10
LL	N 26°49' 11" E 30.20
MM	N 19°37' 40" E 99.17
NN	N 15°00' 18" E 34.77
OO	N 60°05' 46" W 37.72
PP	S 57°34' 41" W 32.50
QQ	S 51°46' 36" W 77.41
RR	S 35°59' 48" W 34.79
SS	S 52°10' 28" W 33.25
TT	N 83°29' 52" W 36.21
UU	N 17°45' 25" E 43.13
VV	N 77°07' 38" W 39.72
WW	N 46°42' 42" W 41.27
XX	N 68°20' 35" W 45.70

100-YEAR FLOOD COURSE TABLE	
COURSE	BEARING & DISTANCE
1	N 53°49' 21" W 272.79
2	N 71°03' 16" W 303.71
3	N 77°02' 58" W 212.10
4	N 76°28' 59" W 252.87
5	S 78°03' 34" W 61.45
6	S 53°19' 50" W 45.45
7	S 83°01' 53" W 83.42
8	S 69°13' 18" W 64.33
9	S 49°21' 12" W 86.24
10	S 31°20' 35" W 53.16



83



CERTIFIED SURVEY MAP NO. 11464

A PART OF THE NORTHWEST 1/4 AND SOUTHWEST 1/4 OF THE NORTHEAST 1/4 OF
SECTION 6, TOWNSHIP 7 NORTH, RANGE 19 EAST, IN THE CITY OF PEWAUKEE,
WAUKESHA COUNTY, WISCONSIN

SURVEYOR'S CERTIFICATE

State of Wisconsin)
:
Waukesha County)

I, Michael W. Buechl, a professional land surveyor, do hereby certify:

That I have surveyed, divided and mapped a parcel of land being a part of the Northwest 1/4 of the Northeast 1/4 of Section 6, Township 7 North, Range 19 East, in the City of Pewaukee, Waukesha County, Wisconsin, bounded and described as follows:

Commencing at the Northeast corner of said Northeast 1/4; thence South 88°28'30" West, along the North line of said Northeast 1/4, 39.74 feet to the Southeast corner of the Southeast 1/4 of Section 31, Township 8 North, Range 19 East; thence South 88°40'11" West, along said North line 1279.51 feet to the centerline of Yench Road; thence South 00°21'52" East, along said centerline, 445.01 feet to a point; thence South 88°40'11" West, 33.00 feet to a point on the West line of Yench Road and the place of beginning of the lands to be described; thence along said West line the following 3 courses: South 00°21'52" East, 1602.63 feet; South 89°38'08" West, 17.00 feet; South 00°21'52" East, 94.14 feet to the North line of U.S. Highway "16"; thence North 74°12'17" West, along said North line, 1327.91 feet to the West line of said Northeast 1/4; thence North 00°09'15" West, along said West line, 1750.52 feet to the Northwest corner of said Northeast 1/4; thence North 88°40'11" East, along said North line, 794.21 feet; thence South 00°21'52" East, 445.01 feet; thence North 88°40'11" East, 492.00 feet to the place of beginning.

Excepting the Northerly 50 feet for roadway purposes.

Containing 2,251,949 square feet (51.697643 acres) more or less.

That I have made this survey, land division and map by the direction of the owners of said land.

That such map is a correct representation of all exterior boundaries of the land surveyed and the division thereof made.

That I have fully complied with the provisions of Chapter 236 of the Wisconsin Statutes pertaining to Certified Survey Maps (Section 236.34 and the Regulations of the City of Pewaukee in surveying, dividing and mapping the same.

APRIL 15, 2016
Date

Michael W Buechl
Michael W. Buechl, Professional
Wisconsin Land Surveyor (S-1106)



ARCTM
(262) 542-8200

FORM ARC-101

CERTIFIED SURVEY MAP NO. 11464

**A PART OF THE NORTHWEST 1/4 AND SOUTHWEST 1/4 OF THE NORTHEAST 1/4 OF
SECTION 6, TOWNSHIP 7 NORTH, RANGE 19 EAST, IN THE CITY OF PEWAUKEE,
WAUKESHA COUNTY, WISCONSIN**

OWNER'S CERTIFICATE OF DEDICATION

YENCH, LLC, a limited liability company, existing under and by virtue of the Laws of the State of Wisconsin, as Owner, does hereby certify that said Company caused the land described in this document to be surveyed, divided, mapped and dedicated as represented on this map in accordance with the requirements of Chapter 236 of the Wisconsin Statutes pertaining to Certified Survey Maps (Section 236.34) and the Regulations of the City of Pewaukee.

WITNESS the hand and seal of said owner this 22nd day of June, 2016.

In the presence of:

Witness _____

Witness: _____

Yench, LLC

Owner Jeffrey Mierow, member

David Mierow, member



State of Wisconsin)

Waukesha County)

PERSONALLY came before me this 22nd day of June, 2016, Jeffrey Mierow*, to me known to be the person who executed the Owner's Certificate and acknowledged the same.

*and David Mierow

Notary Public: Laura Schroeder

My commission expires: 5/28/17

CERTIFICATE OF APPROVAL

RESOLVED that the above Certified Survey Map which has been filed for approval, be and is hereby approved as required by the Subdivision Regulations and Chapter 236 of Wisconsin Statutes relating to Certified Survey Maps. I hereby certify that the foregoing is a true and correct copy of a resolution adopted by the City Board of the City of Pewaukee on this 20 day of June, 2016.

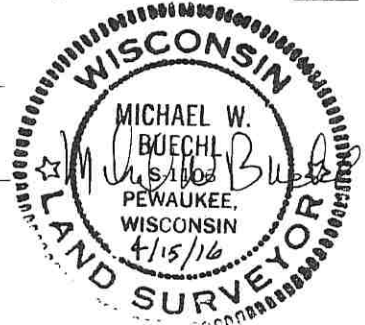


Approved: _____

Chairman, Scott Klein

Signed: _____

Clerk, Kelly Tarczewski



PLANNING COMMISSION APPROVAL

APPROVED by the City of Pewaukee Planning Commission, this 19 day of May, 2016.

Approved: _____

Mayor, Scott Klein

Signed: _____

Secretary, Brian Clinkenbeard
Steve Bierce

4214210

REGISTER OF DEEDS
WAUKESHA COUNTY, WI
RECORDED ON

June 23, 2016 01:06 PM
James R Behrend
Register of Deeds

4 PGS
TOTAL FEE:\$30.00
TRANS FEE:\$0.00

Book 114 Page 82-85

This instrument was drafted by Michael W. Buechl.

Sheet 4 of 4

85



Department of Public Works

W240 N3065 Pewaukee Road
Pewaukee, WI 53072

DPW Main Office: (262)-691-0804 Fax: 691-5729
Water & Sewer Division 691-0804 Fax: 691-5729
Street Division 691-0771 Fax: 691-6079
Engineering Division 691-0804 Fax: 691-5729

TO: Plan Commission

FR: Jeffrey Weigel, Public Works Director

DT: October 15, 2015

RE: CSM 151015-1 Yench LLC on Yench Road

We received a copy of the referenced CSM on October 9, 2015 and offer the following review comments:

- 1) It should be noted that last year the petitioners were approved for the lot abutting proposed Lot 4 as a single parcel land division (CSM 11205).
- 2) This year the petitioners presented the Glacier View Estates preliminary plat for approval. Later, the developer withdrew the plans and plat for Glacier View Estates.
- 3) This new proposal involves the development of 4 lots, constituting a Minor Land Division CSM Platting Procedure under City Ordinances (see section 18.0600).
- 4) The Minor Land Division CSM Procedure generally follows the traditional platting procedure. In this case, however, there may be no public infrastructure required for installation, but storm water management elements may be required. The proposed CSM land division creates 4 lots, and under the City Storm Water Ordinance any *development* that will disturb 1 acre or more (total) will require a storm water management plan. It appears that the development of, and building of the four homes or farms on these parcels will most likely disturb more than 1 acre, total. The land disturbances include grading, excavations and even utility installations such as natural gas to the homes.
- 5) One of our concerns is that Lot 4, although being re-zoned as agricultural, appears to be a single family lot of 2.48 acres. The existing topography for this lot, and of the CSM lot created last year is definitively to the south and west, and will cross parts of proposed Lot 2 and proposed Lot 3. One way to address this cross-lot drainage from the improved lands that include impervious surfaces is to design drainage swales to the south, most likely along the common lot lines of Lots 4, 3 and 2 in dedicated easements; however, it should be noted that the site is heavily treed, and as such, the swale excavations may have significant impacts on the trees and natural state of the lots.
- 6) With approximately 800 feet of frontage, proposed Lot 3 may have the potential for additional subdivision, such as with lots similar to the lot created last year under CSM 11205. Under the DNR's application of the *Common Plan* process, such additional land divisions would require



Department of Public Works

W240 N3065 Pewaukee Road
Pewaukee, WI 53072

DPW Main Office: (262)-691-0804 Fax: 691-5729
Water & Sewer Division 691-0804 Fax: 691-5729
Street Division 691-0771 Fax: 691-6079
Engineering Division 691-0804 Fax: 691-5729

the entire site to be designed, retroactively, so that land division process of 1-CSM at a time does not allow the developer to circumvent the State and City storm water management regulations. In other words, if the developer intends that Lot 3 remain as is, without future development then the appropriate deed restrictions should be in place; if the intent is to reserve the opportunity to further subdivide Lot 3 at some time in the future, then a storm water management plan is required for the entire site as a part of this subdivision application.

- 7) With Lots 1, 2 and 3 all exceeding 10 acres, there is a possibility that each of these lots could prepare and incorporate Stormwater management plans individually on each site; however, we need to check with DNR if this would be either allowable or advisable and as previously stated, we have had less than a week to review and comment on this CSM.
- 8) It should be noted and clarified that agricultural activities (farming, etc.) are exempted from the Storm Water Management regulations (but not fees), but lots that are zoned agricultural, but used as non-agricultural (non-farming) are not exempt.
- 9) As proposed, under the A-2 zoning, all of the trees can be removed from these lots without permission from the City and the buildable area completely enclosed by buildings or pavement. This would obviously require storm water management. Alternatively, all of the trees could be removed and the land could be farmed, that would require no storm water management plan. In either case, however, the property will receive an annual storm water bill for all impervious area on the lot.
- 10) Under all scenarios the developer will need to prepare a grading plan for City review that will appropriately convey the storm water run-off through the development. The development of a storm water management plan requirement will be determined after consultation with local DNR staff.
- 11) We recommend that the construction of the drainage system, presumed to be ditches, will be required before the City executes the CSM unless a separate development agreement between the developer/City is in place to define the necessary construction and timing of same.
- 12) Waukesha County must approved the access locaton and construction on CTH JK.

In attendance: Mayor S. Klein, Aldermen S. Bierce, C. Brown, J. Kara, D. Noll, J. Wamser and B. Bergman. Also present were Clerk/Treasurer K. Tarczewski, City Administrator T. LaBorde, DPW Director J. Weigel, Parks, Recreation & Community Services Director K. Woldanski, Fire Chief K. Bierce, HR Director L. Bergersen and Attorney S. Riffle. City Planner H. Clinkenbeard arrived at 6:38 p.m.

1.0 Call to Order and Pledge of Allegiance - Mayor Klein called the meeting to order at 6:02 p.m.

2.0 Public Comment - None.

3.0 Consent Agenda

- 3.1 Approval of Common Council Meeting Minutes
 - 3.1.1 Meeting Minutes dated October 5, 2015
 - 3.1.2 Meeting Minutes dated October 19, 2015
 - 3.1.3 Special Meeting Minutes dated October 26, 2015
- 3.2 Accounts Payable Summaries
- 3.3 Bartender Licenses
- 3.4 Concur with the Plan Commission to Approve **Ordinance #15-13** to Rezone the property owned by Yench Properties LLC located in the SW Quadrant of the Intersection of Yench Road and Lynndale Road from RS-2 Single-Family Residential to A-2 Agricultural (PWC 0885-996) (**Second Reading**)
- 3.5 Concur with the Plan Commission to **Approve** Certified Survey Map PC#151015-1 to Divide the Yench LLC Property on Yench Road into no more than four parcels (PWC 0885-996)
- 3.6 Concur with the Plan Commission to **Approve** Certified Survey Map PC#151015-2 to Combine Two Lots owned by Thomas Hamm located at W277 N2864 Chicago Avenue (PWC 0934091)

Mayor Klein removed Items 3.4 & 3.5 for discussion.

A motion was made and seconded, (S. Bierce, J. Wamser) to approve the remaining items on the consent agenda. Motion Passed: 6-For, 0-Against. Mr. Noll noted he was abstaining from items 3.1.2 & 3.1.3 since he did not attend those meetings.

- 3.4 Concur with the Plan Commission to Approve Ordinance #15-13 to Rezone the Property owned by Yench Properties LLC located in the SW Quadrant of Intersection of Yench Road and Lynndale Road from RS-2 Single-Family Residential to A-2 Agricultural(PWC 0885-996)
- 3.5 Concur with the Plan Commission to Approve Certified Survey Map PC#151015-1 to Divide the Yench LLC Property on Yench Road into no more than four parcels (PWC 0885-996)

Mr. Weigel stated the Yench CSM is different from the normal CSMs that come before the Common Council. It will be creating (4) parcels; (2) 10-acre parcels, (1) 28-acre parcel and (1) 2 acre parcel. According to the City's land development code the City should treat this as a minor CSM land division. The creation of these (4)

would require the property owner to follow storm water management regulations. Under agricultural the land can change drastically and there are too many variables. Recommendation, pre-CSM subdivision recording which is similar to a preliminary plat approval; which would say, 'yes we are in agreement with the Plan Commission but we have to work out some details before it can come back for final approval'.

Mr. Weigel stated the Engineering Department recommended to the Plan Commission that they follow the procedure outlined in Section 18.0600 of the City Municipal Code – Minor Land Division (CSM) Platting Procedure.

A motion was made and seconded, (S. Bierce, D. Noll) to preliminarily approve the zoning and certified survey map subdividing the property contingent upon it coming back to the Council for final approval once the City Engineer's concerns have been satisfied. Motion Passed: 6-For, 0-Against.

4.0 Public Hearing Regarding the 2016 Proposed Budget

Ms. LaBorde reviewed the changes that were made to the original budget submitted. She reported these changes would produce a \$3.03 tax rate; resulting in a \$6.97 reduction of taxes on a home assessed at \$250,000. She urged the Council to consider taking less of the surplus funds from the unassigned account to supplement the budget because it would bring our reserve down to 16% and she feared the tax levy restrictions will eventually catch up to City.

Mayor Klein opened the Public Hearing. Keith Kramar (1861 River Lakes Road South-Oconomowoc) General Manager from the Marriott Hotel and Chairman of Pewaukee Tourism Committee spoke of the importance to use tourism funds in the proper manner and that was to promote tourism and hotel stays. He questioned the funds that have been used to fund the Sports Complex. Mayor Klein stated we hope that it would be used for tournaments. Mr. Kramar voiced his concerns about potentially increasing the hotel tax. He said the lower tax rate generates more stays and allows Pewaukee to be more competitive with other communities who are higher.

There was further discussion regarding the control of the tourism funds. Previously the City decided how the 70% of tourism funds were distributed based on the criteria that promoted tourism and development. Now the money should be spent on tourism and tourism development. Attorney Riffle stated effective 1/1/2017 the City will have to relinquish Common Council's determination on how the money will be spent and create a commission that will need to contract for secretarial services. He added there will be no changes for 2016; the City of Pewaukee will still be in control of the funds.

Mr. Kramar asked how the City would spend the extra 2% for the increased hotel tax. Mayor Klein stated he wasn't certain. Ms. LaBorde added that previous request for funds required the City to spend funds over what was anticipated to be received.

Mayor Klein closed the public hearing at 6:44 p.m. when no one else expressed an interest in speaking.

Ms. LaBorde reviewed what was spent out of the contingency account in 2015 and the proposed balance of the unassigned fund in 2016. She reminded them that it is ideal to retain 20 – 25% in our undesignated funds per our finance policy. Ms. LaBorde also repeated the recommendation of Ehlers to increase our levy based on what is allowed per our growth.

The hotel tax issue was revisited. Mayor surveyed the alderman; 4 were against raising the

and should not be signed. The Department of Transportation wants the driveway moved to the newly acquired property. The petitioner is aware of this request and is okay of the change. **A motion was made and seconded, (B. Bergman, J. Wamser) to table this item until the new CSM is received and reviewed by professional staff.** Motion Passed: 5-For, 0-Against.

- 5.10 Approval of **Ordinance 16-20** Regarding the Rezoning of the Yench LLC / Mierow Property Identified as PWC 0885-996 From RS-2 Single Family Residential to A-2 Agricultural

- 5.11 Approval of the Certified Survey Map PC #160519-1 to Divide the Yench LLC / Mierow Property into Three (3) Lots

Mr. Weigel stated there are storm water management issues that need to be addressed. He recommended not approving these items until the engineer has time to review. **A motion was made and seconded, (B. Bergman, J. Wamser) to table Items 5.10 & 5.11.** Motion Passed: 5-For, 0-Against.

- 5.12 Approval of the Conditional Use Permit for PBB Pewaukee I LLC to Operate the Point Burger Bar Restaurant and Tavern Located at W229 N1400 Westwood Drive in the Former American TV Building (PWC 0959-988-004) with Recommended Contingencies

Mr. Weigel noted an important condition was not listed on the conditional use permit as discussed at the Plan Commission meeting. He stated he is requesting payment of Reserve Capacity Assessment (RCA) for sewer and water based on the new use and updating the original use of the building because they will be using more water and sewer than before.

Mayor Klein stated another issue was brought up and needs to be discussed. It was noted that during a fire inspection the petitioner was building a patio which was not discussed previously or approved by the Plan Commission as part of their conditional use. **A motion was made and seconded, (S. Bierce, J. Wamser) to deny the conditional use permit and send the petitioner back to the Plan Commission for further discussion.** Motion Passed: 3-For, 2-Against (Brown, Bergman).

- 5.14 Approval of Woodleaf Reserve Addition No. 1 Final Plat
- 5.14.1 Declaration of Protective Covenants, Conditions, Easements and Restrictions
 - 5.14.2 Amendment No. 1 to Storm Water Management Practices Maintenance Agreement

Jim Doering was present for this item. Mr. Weigel reported the paving for the subdivision was completed on Friday, which was a condition for the final approval of the plat. He also recommended approval of the amendment to the Storm Water Management Practices Maintenance Agreement contingent upon approval of professional staff. **A motion was made and seconded, (B. Bergman, C. Brown) to approve the Woodleaf Reserve Addition No. 1 final plat, the declaration of protective covenants, conditions, easements and restrictions as well as the first amendment to the storm water management practices**

In attendance: Mayor S. Klein, Aldermen B. Bergman, S. Bierce, J. Kara, R. Grosch. C. Brown, J. Wamser were absent and excused. Also present were Clerk/Treasurer K. Tarczewski, DPW Director J. Weigel, Attorney S. Riffle, Fire Chief K. Bierce, and City Planner H. Clinkenbeard.

- 1.0 Call to Order and Pledge of Allegiance - Mayor Klein called the meeting to order at 7:00 p.m.
- 2.0 Public Comment – None.

3.0 Consent Agenda – Action

- 3.1 Approval of Common Council Meeting Minutes dated June 6, 2016
- 3.2 Accounts Payable Summaries
- 3.3 Bartender Licenses
- 3.4 Adoption of the City's 2050 Land Use / Transportation Plan for the North Bluemound Neighborhood as Approved and Recommended by the City Plan Commission (PC 6/1/2016)
- 3.5 Approval of the Holding Tank Agreement as Requested by Kave Enterprises for the Property Located at W237 N689 Oakridge Lane (PWC 0967-979-001)
- 3.6 Approval of **Resolution 16-06-07** Related to the DNR Annual Sewer System Compliance Maintenance Annual Report (CMAR)
- 3.7 Approval of **Ordinance 16-19** Regarding the Rezoning of the Jaeschke Property Located at W239 N3368 Pewaukee Road (PWC 0907-997 & PWC 0907-996) from RS-3 Single Family Residential to B-6 Mixed Use Business (*Third reading*)
- 3.8 Approval of Certified Survey Map PC #160519-2 (Revised) to Combine the Janssen Office Property with Two (2) Vacant Lots
- 3.9 Approval of **Ordinance 16-20** Regarding the Rezoning of the Yench LLC / Mierow Property Identified as PWC 0885-996 from RS-2 Single Family Residential to A-2 Agricultural (*Third reading*)
- 3.10 Approval of Certified Survey Map PC #160519-1 to Divide the Yench LLC / Mierow Property into Three (3) Lots

Mayor Klein noted the minutes were not completed as of yet and pulled them from the consent agenda. Mr. Bergman asked that the Items 3.9 and 3.10 be removed for discussion.

A motion was made and seconded, (S. Bierce, R. Grosch) to approve the remaining items on the consent agenda. Motion Passed: 4-For, 0-Against.

- 3.9 Approval of **Ordinance 16-20** Regarding the Rezoning of the Yench LLC / Mierow Property Identified as PWC 0885-996 from RS-2 Single Family Residential to A-2 Agricultural.

- 3.10 Approval of Certified Survey Map PC #160519-1 to Divide the Yench LLC / Mierow Property into Three (3) Lots

Mr. Bergman wanted to verify that the Engineering Department was satisfied with their storm water management plan. Mr. Weigel confirmed everything was submitted properly and they are satisfied.

A motion was made and seconded, (B. Bergman, J. Kara) to approve Ordinance 16-20 and Certified Survey Map PC #160519-1. Motion Passed: 4-For, 0-Against.

4.0 Discussion and Possible Action Regarding the Outdoor Entertainment Permit Requests (PC 5/19/2016)

- 4.1 5 O'Clock Club
- 4.2 Boomer's Sports Pub & Grill
- 4.3 Curly's Waterfront
- 4.4 Duplainville Station
- 4.5 Edgewater of Pewaukee
- 4.6 Gina's Sports Dock
- 4.7 Waukesha Gun Club
- 4.8 Wonderland Tap (PC 6/16/16)

Mr. Bierce stated he noticed that Boomer's wanted to increase the time for their outdoor music. He stated he felt the time should be consistent with the other bars. Mr. Bergman agreed, especially because it was so close to residential areas.

A motion was made and seconded, (S. Bierce, R. Grosch) to approve the outdoor entertainment permits; limiting Boomer's Sports Pub and Grill music permit to 9:00 p.m. during the week (Sunday – Thursday). Taylor Pasdera was present for this item and he stated he only listed those times for the purpose of using lights. He stated he is not planning on having any live music. Motion Passed: 4-For, 0-Against.

5.0 Public Hearing Regarding the Liquor License Applications for the 2016 – 2017 Licensing Period and Possible Action to Issue

Mr. Bierce noted a couple of concerns; he said the Building Inspection Department has been trying to make contact with the Edgewater Bar concerning their remodeling project and no one has been responsive to their attempts. He also said the Point Burger Bar has to go before the Plan Commission again to revise their Conditional Use Permit. He said when it originally went before the Plan Commission there was no discussion regarding a game room or their intention to serve food and alcohol on their patio. Attorney Riffle advised not to table these items, but instead make them contingent upon meeting all City code requirements and appropriate zoning codes. In the instance of the liquor license renewal of Edgewater they could be shut down on July 1st if they fail to meet any of the City codes. A representative from the Point Burger Bar was present. He asked if there would still be a license available if it was tabled to a later date. Attorney Riffle stated the license should be contingent upon them meeting the zoning codes to ensure they get it.

A motion was made and seconded, (S. Bierce, R. Grosch) to approve the liquor licenses with the noted conditions/contingencies. Motion Passed: 4-For, 0-Against.

6.0 Discussion and Possible Action Regarding the City Hall Roof, HVAC Replacement Contract Bids

Mayor Klein explained the scope and phases of the proposed projects. He said the 1st phase consisted of repairing the leaking roof over the City Hall offices, replacing the failing HVAC system and other minor things and the 2nd phase of the project would be to repair the roof over the highway garage, repair the bowing wall and internal remodeling in the garage area. He stated the project has been bid twice due to irregularities related to the form, the low bidder

Chairman Klein read a letter from Mr. Kiser, a citizen who resides in the area, indicating his concerns about various points (see file) and indicated that until there was a more definitive proposal and the people in the neighbor could really see what was being proposed, they were against this rezoning.

Mr. Stollenwerk asked if it was possible to get conceptual approval of this kind of use, rather than a final approval.

At that point, there being no further comments from the audience, Chairman Klein closed the public hearing at 7:30 PM. There was then further discussion. It was pointed out that yes, there is a possibility that they could get conceptual approval in light of the fact that the Plan Commission had talked about changing of the land use plan in this area because of the shallow water table and the fact that it was unlikely that a single-family subdivision could be built on this land. The land was probably best suited for on-slab construction similar to industrial use construction.

It was pointed out that the Plan Commission would either have to approve this tonight or deny it unless the representatives of the two families agreed to ask the City for an extension of this proposal for a period of time. The representatives of the two families indicated that they would be willing to provide an extension if the City would go along with that for some period. There was then some discussion about the amount of time. It was determined that the earliest that any feedback could be had regarding the long range land use plan would be at the February meeting. It was then agreed between the Plan Commission and Mr. Stollenwerk and the Kanters that this proposed rezoning would be brought back up at the February meeting. It was pointed out that at that time, it would have to be determined whether or not there had been enough public input to the plan to make a final determination.

At that point, Chairman Klein asked the Plan Commission members if they had any problems with conceptually approving. The Plan Commission members indicated that probably not.

A motion was made by Mr. Coursin, seconded by Mr. Linsmeier on the request of the petitioners to approve the proposal conceptually but delay any further action or activity on this proposal until the February 2016 meeting of the Plan Commission. There was no discussion regarding this item and the motion passed unanimously.

DISCUSSION & ACTION REGARDING A PETITION FOR A REZONING PUBLIC HEARING FOR YENCH PROPERTIES LLC TO REZONE THEIR PROPERTY IN THE SW QUADRANT OF THE INTERSECTION OF YENCH ROAD & LYNNDAL ROAD FROM RS-2 SINGLE-FAMILY RESIDENTIAL TO A-2 AGRICULTURAL AND CERTIFIED SURVEY MAP PC #151015-1 TO DIVIDE THE YENCH LLC PROPERTY ON YENCH ROAD INTO NO MORE THAN FOUR PARCELS (PWC 0885996)

Mr. Clinkenbeard pointed out the location of the site. Mr. Mierow came forth representing Yench Properties LLC indicating that what they were now proposing was a creation of four lots, two ten-acre lots, a two-acre lot on Yench Road, and then the remainder of the site that would be greater than ten acres. It was noted that the proposed two-acre lot was already zoned Rs-2 so that didn't need to be rezoned. The developable land on the two ten-acre lots and the larger remnant lot are currently

zoned Rs-2 and they were what would be rezoned to A-2 agriculture. Chairman Klein noted that this was one of the first times in a long time that we were being asked to actually 'down-zone' land from residential to agriculture. Mr. Mierow indicated that at some future date, probably beyond a five year period, he would probably want to rezone the larger remnant lot, (lot #3) on the certified survey map, into two or three two-acre lots at that time.

Mr. Weigel pointed out that there were a number of stormwater management concerns about this property in light of the fact that it was a very steep property and the water that comes off the land would flow across multiple properties in the area and something would have to be addressed in that regard, not only for this initial division, but also for future divisions of the land.

There was some more discussion after which Chairman Klein opened the public hearing at 8:01 PM and asked if there was anyone in the audience who wished to speak either in favor of or in opposition to this proposed rezoning. There being none, Chairman Klein closed the public hearing at 8:01 PM.

At that point, a motion was made by Mr. Coursin, seconded by Mr. Klein to rezone the developable land on the property, as indicated and shown on the accompanying certified survey map as lots 1, 2 and 3, from Rs-2 to A-2 Agriculture with the understanding that the stormwater issues as pointed out by the City Engineer would have to be addressed and at the very least, easements would have to be provided across properties to accommodate any future drainage and retention/detention pond requirements in the area. There was no discussion regarding this item and the motion passed unanimously.

A motion was made by Mr. Linsmeier, seconded by Mr. Sullivan to offer preliminary approval of the CSM PC #151015-1 dividing the Yench property as indicated into four parcels. It was pointed out again that there needed to be, at the very least, easements placed on the certified survey map to accommodate cross lot flow of stormwater and the location and building of retention/detention ponds in accordance with the City Engineers concerns. There was no discussion regarding this item and the motion passed unanimously.

DISCUSSION & ACTION REGARDING CERTIFIED SURVEY MAP PC #151015-2 TO COMBINE TWO LOTS OWNED BY THOMAS HAMM LOCATED AT W277 N2864 CHICAGO AVENUE (PWC 0934091)

Mr. Clinkenbeard pointed out that as shown on the aerial photograph they had been provided, Mr. Hamm's house actually straddles a lot line and Mr. Hamm was simply trying to make the lot conforming to City requirements by combining the lots.

A motion was made by Mr. Coursin, seconded by Mr. Sullivan to recommend approval of the certified survey map PC #151015-2 as submitted. There was no discussion regarding this item and the motion passed unanimously.

DISCUSSION & ACTION REGARDING A PETITION FOR A REZONING PUBLIC HEARING FOR YENCH LLC/JEFF MIEROW TO REZONE PROPERTY LOCATED IN THE SOUTHWEST QUADRANT OF YENCH ROAD & LYNNDAL ROAD FROM RS-2 SINGLE-FAMILY RESIDENTIAL TO A-2 AGRICULTURAL & CERTIFIED SURVEY MAP PC #160519-1 TO DIVIDE THE JEFF MIEROW/YENCH LLC PROPERTY INTO THREE LOTS (PWC 0885996)

Mr. Clinkenbeard pointed out that if the zoning is approved, all of the land would revert to A-2 Agricultural except for the current wetlands and floodplain part of the lands which would remain under Floodplain and Lowland Conservancy zoning.

Mr. Weigel indicated that he saw no problem with the certified survey map, which was also on the agenda for this property.

At that point, Chairman Klein opened the public hearing at 7:27 PM and asked if there was anyone in the audience who wished to speak either in favor of or in opposition to the rezoning of the property. There being none, Chairman Klein closed the public hearing at 7:27 PM.

A motion was made by Mr. Coursin, seconded by Ms. Wunder to recommend the zoning of the property from Rs-2 to A-2 Agricultural and also the approval of the certified survey map dividing off two ten-acre lots on the newly rezoned property. There was no discussion regarding this item and the motion passed unanimously.

At that point, Mr. Coursin recused himself from the Plan Commission due to the fact that the business he worked for was involved in the next item on the agenda.

DISCUSSION & ACTION REGARDING A PETITION FOR A REZONING PUBLIC HEARING FOR AUDREY JAESCHKE/NICHOLAS DELTORTO TO REZONE TWO VACANT PARCELS IMMEDIATELY SOUTH OF THE JANNSEN OFFICE PROPERTY AT THE INTERSECTION OF PEWAUKEE ROAD & CAPITOL DRIVE FROM RS-3 SINGLE-FAMILY RESIDENTIAL TO B-6 MIXED USE BUSINESS FOR THE PURPOSE OF EXPANDING THE JANNSEN OFFICE PROPERTY & CERTIFIED SURVEY MAP PC #160519-2 TO COMBINE THE JANNSEN OFFICE PROPERTY WITH THE TWO VACANT PARCELS TO THE SOUTH (PWC 0907997 & PWC 0907996)

Mr. DelTorto came forward indicating that he was buying the Jannsen building and wanted to add the two vacant parcels to the south to the property. The first phase of his project would be simply to add a parking lot on one of the current properties to the south after it had been combined with the Jannsen property.

Mr. Weigel indicated that there was still a question about whether there would be two accesses to the newly created property or only one and what kind of accesses would be there. Plus, there was a need to finish the work with the applicant regarding storm water management. Mr. DelTorto indicated that in either event, if he was denied the southerly access to the property, he still had the northerly access that could be access to the new parking. He realized that the storm water management situation would have to be finalized.

From: [Jeff Mierow](#)
To: [Wagner, Magdelene](#)
Cc: [Fuchs, Nick](#); [Gabbey, Michaelis](#); [J. Pudelko](#); [Jayme Sisel](#); [Wirtz, Rich](#); [Wood, Peter C - DNR](#)
Subject: Re: Yench LLC CSM
Date: Thursday, November 16, 2023 1:58:15 PM

So I advised those owners when they built to asked the city if they had to do any rain gardens or swales when they built and we're told no by the city because they did not disturb enough area how can you try to tell me I should try to figure out how to make water go uphill. Why did the City require that drainage easement on the back of the property? What was the purpose of that?

On Thu, Nov 16, 2023 at 1:46 PM Wagner, Magdelene <wagner@pewaukee.wi.us> wrote:

Hi Pete,

Thank you for the discussion today regarding the Mierow development called Yench LLC in the City of Pewaukee. We have received a new submittal from the developer and his engineers are stating the site is only required to handle the storm water management from the 4 lots they are creating with this current CSM. From the submittal letter dated November 8, 2023: "The stormwater management plan for this project does not include other nearby properties or offsite areas that are downhill or not tributary to this site, as is appropriate and consistent with DNR policy. Pete Wood, a lead Water Resource Engineer at the DNR, has stated via email dated July 7, 2023, that this project is not considered a common plan of development with other homes built by others on lots adjacent to this property. Therefore, this project should not be burdened with retroactively creating storm water management facilities for downhill or offsite areas, and is not able to do so."

The development of this land (all current and previous CSM's) were from Mr. Mierow with the intent of developing the lots. In addition, it was made very clear to Mr. Mierow with the last CSM that if he were to develop this land further, he would have to retroactively address the storm water management for all past developments. The Plan Commission and Common Council approved the last development with this provision in place. In order to meet the City's ordinances and the approvals from the previous development, this site must include the storm water management for the previous development.

Please confirm that while the DNR regulations may not require the storm water management for the previous development, the development must meet the City ordinances, standards and previous approval requirements.

Thank you,

Maggie

Magdelene Wagner, P.E.

Director of Public Works/City Engineer

[City of Pewaukee](#)

[W240N3065 Pewaukee Road](#)

[Pewaukee, WI 53072](#)

262-691-0804

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From: [Wagner, Magdelene](#)
To: [Jeff Mierow](#)
Cc: [Fuchs, Nick](#); [Gabbey, Michaelis](#)
Subject: RE: Yench LLC CSM
Date: Monday, November 20, 2023 3:19:00 PM
Attachments: [Plan Commission Minutes 2015_10_15 Meeting\(316\).pdf](#)

Jeff,

Please find attached the Plan Commission minutes for your first Yench Road LLC CSM. I've highlighted the language I believe requires storm water management for the current and the previous development.

Thank you,
Maggie

Magdelene Wagner, P.E.
Director of Public Works/City Engineer
City of Pewaukee
W240N3065 Pewaukee Road
Pewaukee, WI 53072
262-691-0804

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From: Jeff Mierow <jeffmierow@mierowrealty.com>
Sent: Monday, November 20, 2023 11:18 AM
To: Wagner, Magdelene <wagner@pewaukee.wi.us>
Subject: Re: Yench LLC CSM

I will call you at 2.
Thanks
Jeff

On Mon, Nov 20, 2023 at 11:16 AM Wagner, Magdelene <wagner@pewaukee.wi.us> wrote:

Jeff,

I'm only available this afternoon for this week. Would a 2 pm call work for you?

Thanks,

Maggie

Magdelene Wagner, P.E.

Director of Public Works/City Engineer

City of Pewaukee

W240N3065 Pewaukee Road

Pewaukee, WI 53072

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From: Jeff Mierow <jeffmierow@mierowrealty.com>

Sent: Monday, November 20, 2023 9:06 AM

To: Wagner, Magdelene <wagner@pewaukee.wi.us>

Subject: Re: Yench LLC CSM

Maggie -

Can we please connect over the phone this week to discuss this? Let me know your availability.

Thanks!

On Thu, Nov 16, 2023 at 1:57 PM Jeff Mierow <jeffmierow@mierowrealty.com> wrote:

So I advised those owners when they built to asked the city if they had to do any rain gardens or swales when they built and we're told no by the city because they did not disturb enough area how can you try to tell me I should try to figure out how to make water go uphill. Why did the City require that drainage easement on the back of the property? What was the purpose of that?

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The development of this land (all current and previous CSM’s) were from Mr. Mierow with the intent of developing the lots. In addition, it was made very clear to Mr. Mierow with the last CSM that if he were to develop this land further, he would have to retroactively address the storm water management for all past developments. The Plan Commission and Common Council approved the last development with this provision in place. In order to meet the City’s ordinances and the approvals from the previous development, this site must include the storm water management for the previous development.

Please confirm that while the DNR regulations may not require the storm water management for the previous development, the development must meet the City ordinances, standards and previous approval requirements.

Thank you,
Maggie

Magdelene Wagner, P.E.

Director of Public Works/City Engineer

[City of Pewaukee](#)

[W240N3065 Pewaukee Road](#)

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--

Jeff Mierow
Mierow Realty & Building Co
414.254.0731

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--

Jeff Mierow
Mierow Realty & Building Co
414.254.0731

From: [Wood, Peter C - DNR](#)
To: [Wagner, Magdelene](#)
Cc: [Gabbey, Michaelis](#); [Fuchs, Nick](#); [Jeff Mierow](#); [Wirtz, Rich](#); [J. Pudelko](#); [Jayme Sisel](#)
Subject: RE: Yench LLC CSM
Date: Thursday, November 16, 2023 3:13:56 PM

Maggie,

Any opinion I provided regarding this development was specific to DNR storm water regulations.

I'm not sure if my actual e-mail was provided to you but I was clear in the e-mail that DNR policy may not be the same as the City policy regarding common plan of development. The City is a separate unit of government that needs to implement its own storm water regulations.

Pete Wood, P.E.

Storm Water Engineer – Water Division
Wisconsin Department of Natural Resources
1027 W St Paul Avenue
Milwaukee, WI 53233
Phone: 262-822-8227
Peter.Wood@wisconsin.gov

We are committed to service excellence.

Visit our survey at <http://dnr.wi.gov/customersurvey> to evaluate how I did.

From: Wagner, Magdelene <wagner@pewaukee.wi.us>
Sent: Thursday, November 16, 2023 1:46 PM
To: Wood, Peter C - DNR <Peter.Wood@wisconsin.gov>
Cc: Gabbey, Michaelis <gabbey@pewaukee.wi.us>; Fuchs, Nick <fuchs@pewaukee.wi.us>; Jeff Mierow <jeffmierow@mierowrealty.com>; Wirtz, Rich <wirtz@pewaukee.wi.us>; J. Pudelko <jpudelko@trioeng.com>; Jayme Sisel <Jayme.Sisel@soundstormwater.com>
Subject: Yench LLC CSM

**CAUTION: This email originated from outside the organization.
Do not click links or open attachments unless you recognize the sender and know the content is safe.**

Hi Pete,

Thank you for the discussion today regarding the Mierow development called Yench LLC in the City of Pewaukee. We have received a new submittal from the developer and his engineers are stating the site is only required to handle the storm water management from the 4 lots they are creating with this current CSM. From the submittal letter dated November 8, 2023: "The stormwater management plan for this project does not include other nearby properties or offsite areas that are downhill or not tributary to this site, as is

appropriate and consistent with DNR policy. Pete Wood, a lead Water Resource Engineer at the DNR, has stated via email dated July 7, 2023, that this project is not considered a common plan of development with other homes built by others on lots adjacent to this property. Therefore, this project should not be burdened with retroactively creating storm water management facilities for downhill or offsite areas, and is not able to do so.”

The development of this land (all current and previous CSM’s) were from Mr. Mierow with the intent of developing the lots. In addition, it was made very clear to Mr. Mierow with the last CSM that if he were to develop this land further, he would have to retroactively address the storm water management for all past developments. The Plan Commission and Common Council approved the last development with this provision in place. In order to meet the City’s ordinances and the approvals from the previous development, this site must include the storm water management for the previous development.

Please confirm that while the DNR regulations may not require the storm water management for the previous development, the development must meet the City ordinances, standards and previous approval requirements.

Thank you,
Maggie

Magdelene Wagner, P.E.

Director of Public Works/City Engineer

City of Pewaukee

W240N3065 Pewaukee Road

Pewaukee, WI 53072

262-691-0804

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**CITY OF PEWAUKEE
COMMON COUNCIL AGENDA ITEM 11.**

DATE: January 15, 2024

DEPARTMENT: Planning

PROVIDED BY: Nick Fuchs

SUBJECT:

Discussion and Possible Action Regarding a Certified Survey Map for Yench, LLC for Property Located on the West Side of Yench Road (PWC 0885-996-006) for the Purpose of Further Subdividing the Property Into Two Lots [Fuchs]

BACKGROUND:

At their December 21, 2023, meeting, the Plan Commission unanimously recommended approval of the subject Certified Survey Map.

FINANCIAL IMPACT:

RECOMMENDED MOTION:

Considering the zoning and storm water management concerns, staff is recommending a motion to table the certified survey map to allow the applicant to submit a Rezoning Application and provide a storm water management plan in compliance with City regulations.

Alternatively, if the Common Council wishes to approve the CSM, the recommended motion is to approve the Certified Survey Map for Yench, LLC for property located on the west side of Yench Road for the purpose of further subdividing the property into two lots.

ATTACHMENTS:

Description

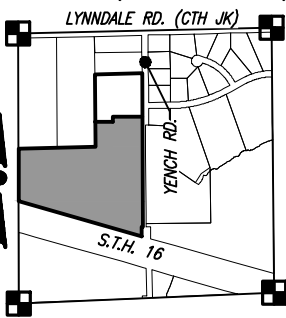
Yench LLC CSM #2

Engineering memo

CERTIFIED SURVEY MAP NO.

BEING A REDIVISION OF LOT 3 OF CERTIFIED SURVEY MAP NUMBER _____, LOCATED IN THE NORTHWEST 1/4 AND THE SOUTHWEST 1/4 OF THE NORTHEAST 1/4 OF SECTION 6, TOWN 7 NORTH, RANGE 19 EAST, IN THE CITY OF PEWAUKEE, WAUKESHA COUNTY, WISCONSIN.

(P.O.C.)
NW. CORNER OF THE
NE 1/4
SEC. 6-7-19
WIS. STATE PLANE
COORD. SYSTEM - SOUTH
ZONE
FND. CONC. MON.
W/ BRASS CAP
N-407,391.66
E-2,455,521.07



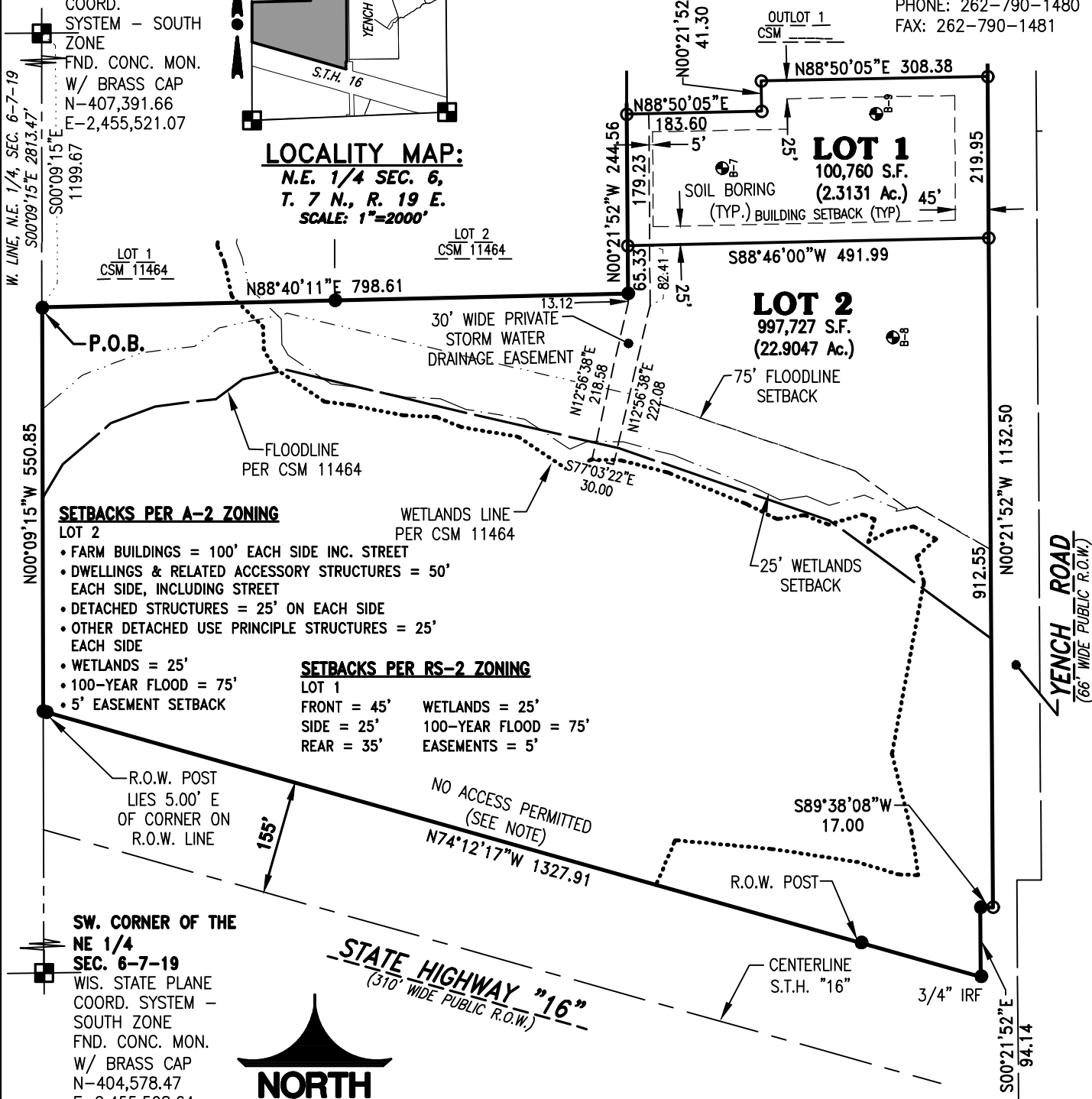
LOCALITY MAP:
N.E. 1/4 SEC. 6,
T. 7 N., R. 19 E.
SCALE: 1"=2000'

PREPARED FOR:

YENCH, LLC
3055 N. BROOKFIELD RD.
BROOKFIELD, WI 53005
PHONE: 414-254-0731

PREPARED BY:

TRIO ENGINEERING, LLC
4100 N. CALHOUN RD.
STE 300
BROOKFIELD, WI 53005
PHONE: 262-790-1480
FAX: 262-790-1481



SETBACKS PER A-2 ZONING

- LOT 2
- FARM BUILDINGS = 100' EACH SIDE INC. STREET
 - DWELLINGS & RELATED ACCESSORY STRUCTURES = 50' EACH SIDE, INCLUDING STREET
 - DETACHED STRUCTURES = 25' ON EACH SIDE
 - OTHER DETACHED USE PRINCIPLE STRUCTURES = 25' EACH SIDE
 - WETLANDS = 25'
 - 100-YEAR FLOOD = 75'
 - 5' EASEMENT SETBACK

SETBACKS PER RS-2 ZONING

- LOT 1
- | | |
|-------------|----------------------|
| FRONT = 45' | WETLANDS = 25' |
| SIDE = 25' | 100-YEAR FLOOD = 75' |
| REAR = 35' | EASEMENTS = 5' |

SW. CORNER OF THE
NE 1/4
SEC. 6-7-19
WIS. STATE PLANE
COORD. SYSTEM -
SOUTH ZONE
FND. CONC. MON.
W/ BRASS CAP
N-404,578.47
E-2,455,528.64



LEGEND:

- INDICATES Section Corner (See Plan for Details)
- INDICATES "Set" 0.750" O.D. X 18" long reinforcing bar weighing 1.502 lbs. per lineal foot.
- INDICATES "Found" 1" iron pipe, unless noted otherwise.

SCALE: 1"=200'



NOTES:

- ALL BEARINGS ARE REFERENCED TO GRID NORTH OF THE WISCONSIN STATE PLANE COORDINATE SYSTEM, SOUTH ZONE (NAD-27), IN WHICH THE WEST LINE OF THE N.E. 1/4 OF SECTION 6, TOWN 7 NORTH, RANGE 19 EAST, BEARS S00°09'15"E.
- ALL TELEPHONE LINES, ELECTRIC LINES, CABLE TELEVISION LINES, TELECOMMUNICATION LINES AND OTHER SIMILAR UTILITY SERVICES SHALL BE PLACED UNDERGROUND UNLESS THE APPLICANT OR UTILITY CAN DEMONSTRATE THAT UNDERGROUNDING IS NOT FEASIBLE.
- WETLANDS AND FLOODLINE SHOWN HEREON ARE FROM CERTIFIED SURVEY MAP NUMBER 11464.
- THERE SHALL BE NO DIRECT VEHICULAR INGRESS OR EGRESS TO STATE HIGHWAY "16" FROM LOT 2. IT BEING EXPRESSLY INTENDED THAT THIS RESTRICTION SHALL CONSTITUTE A RESTRICTION FOR THE BENEFIT OF THE PUBLIC ACCORDING TO S.236.293 OF THE WISCONSIN STATUTES AND SHALL BE ENFORCED BY THE WISCONSIN DEPARTMENT OF TRANSPORTATION AND WAUKESHA COUNTY.

DRAFTED THIS 3RD DAY OF NOVEMBER, 2023. REVISED 11-22-23.
THIS INSTRUMENT WAS DRAFTED BY TED R. INDERMUEHLE, S-3119

JOB NO. 16-015-467-02
SHEET 1 OF 4

**BEING A REDIVISION OF LOT 3 OF CERTIFIED SURVEY MAP NUMBER _____ LOCATED IN THE
NORTHWEST 1/4 AND THE SOUTHWEST 1/4 OF THE NORTHEAST 1/4 OF SECTION 6, TOWN 7 NORTH,
RANGE 19 EAST, IN THE CITY OF PEWAUKEE, WAUKESHA COUNTY, WISCONSIN.**

[illegible]

That I have surveyed, divided, and mapped a redivision of Lot 3 of Certified Survey Map Number _____, recorded in the office of the Register of Deeds, Waukesha County on _____ in Book _____ Pages _____ of Certified Survey Maps, inclusive as document number _____, located in the Northwest 1/4 and the Southwest 1/4 of the Northeast 1/4 of Section 6, Township 7 North, Range 19 East, in the City of Pewaukee, Waukesha County, Wisconsin.

Dated this _____ day of _____, 20____.

Ted R. Indermuehle, P.L.S.
Professional Land Surveyor, S-3119
TRIO ENGINEERING, LLC
4100 N. Calhoun Rd. Suite 300
Brookfield, WI 53005
Phone: (262)790-1480 Fax: (262)790-1481

CERTIFIED SURVEY MAP NO. _____

**BEING A REDIVISION OF LOT 3 OF CERTIFIED SURVEY MAP NUMBER _____ LOCATED IN THE
NORTHWEST 1/4 AND THE SOUTHWEST 1/4 OF THE NORTHEAST 1/4 OF SECTION 6, TOWN 7 NORTH,
RANGE 19 EAST, IN THE CITY OF PEWAUKEE, WAUKESHA COUNTY, WISCONSIN.**

CORPORATE OWNER'S CERTIFICATE OF DEDICATION:

Yench, LLC, a Limited Liability Company duly organized and existing under and by virtue of the laws of the State of Wisconsin, as owner, does hereby certify that said Limited Liability Company has caused the land described on this map to be surveyed, divided, mapped and dedicated as represented on this map in accordance with the provisions of Chapter 236 of the Wisconsin Statutes and the Subdivision Regulations of the City of Pewaukee, this _____ day of _____, 20 ____.

Yench, LLC

Jeff Mierow, Owner

David Mierow, Owner

STATE OF WISCONSIN)
) ss
COUNTY OF)

Personally came before me this _____ day of _____, 20 ____, Jeff Mierow, Member of the above named limited liability company, to me known to be the person who executed the foregoing instrument, and to me known to be such Member of said limited liability company, and acknowledged that he executed the foregoing instrument as such officer as the deed of said limited liability company, by its authority.

Print Name: _____
Notary Public, _____ County, WI
My commission expires: _____

WETLANDS AND FLOODPLAIN PRESERVATION RESTRICTIONS:

Those areas identified as Wetlands and Floodplain on this Plat shall be subject to the following restrictions:

- 1.Grading, filling and removal of topsoil or other earthen materials are prohibited except in connection with the construction of a proposed gravel path, unless specifically authorized by the Municipality in which this land is located and, if applicable, the Waukesha County Department of Parks and Land Use, the Wisconsin Department of Natural Resources and the Army Corps of Engineers.
- 2.The removal or destruction of any vegetative cover, i.e., trees, shrubs, grasses, etc., is prohibited, with the exception that dead, diseased, or dying vegetation may be removed, at the discretion of the landowner and with approval from the Municipality in which this land is located and, if applicable, the Waukesha County Department of Parks and Land Use-Planning and Zoning Division. Silvicultural thinning upon the recommendation of a forester or naturalist and with approval from the Municipality in which this land is located and, if applicable, the Waukesha County Department of Parks and land Use-Planning and Zoning Division shall also be permitted.
- 3.Grazing by domesticated animals, i.e., horses, cows, etc, is prohibited.
- 4.The introduction of plant material not indigenous to the existing environment is prohibited.
- 5.Ponds may be permitted subject to the approval of the Municipality in which they are located and, if applicable, the Waukesha County Department of Parks and Land Use, the Wisconsin Department of Natural Resources and the Army Corps of Engineers.
- 6.The construction of buildings is prohibited.

CERTIFIED SURVEY MAP NO._____

**BEING A REDIVISION OF LOT 3 OF CERTIFIED SURVEY MAP NUMBER _____ LOCATED IN THE
NORTHWEST 1/4 AND THE SOUTHWEST 1/4 OF THE NORTHEAST 1/4 OF SECTION 6, TOWN 7 NORTH,
RANGE 19 EAST, IN THE CITY OF PEWAUKEE, WAUKESHA COUNTY, WISCONSIN.**

PLAN COMMISSION APPROVAL:

APPROVED by the Planning Commission of the City of Pewaukee on this ____ day of _____, 20__.

Steve Bierce, Mayor

Colleen Brown, Plan Commission Secretary

COMMON COUCIL APPROVAL:

Resolved that the Certified Survey map, in the City of Pewaukee, **Yench, LLC**, owner, is hereby approved and dedication accepted by the City Board of the City of Pewaukee by Resolution No._____, on this ____ day of _____, 20__.

All conditions have been met as of this _____ day of _____, 20__.

Date: _____

Steve Bierce, Mayor

Date: _____

Kelly Tarczewski, Clerk/Treasurer



Department of Public Works

W240N3065 Pewaukee Road

Pewaukee, WI 53072

Phone: (262) 691-0804 • Fax: (262) 691-5729

Email: publicworks@pewaukee.wi.us

MEMORANDUM

TO: Common Council

FROM: Magdelene Wagner, Director of Public Works

DATE: January 12, 2024

RE: Yench Road LLC CSM

In 2014, Mr. Mierow received approval (PC 6/19/2014 & CC 7/7/2014) to split a 2.4ac parcel off a larger parcel by CSM 11205 creating PWC0885996002 and 0885996003. A single family home was constructed on the 2.4 ac parcel (Lot 1 of the CSM).

In 2016, Mr. Mierow again received approval to further split the lot, but with conditions if he further subdivides the parcels, storm water management would be required. There were discussions on this matter at several PC and CC meetings (see attached). CSM 11464 was ultimately approved for 2-10 ac lots and 1-30 ac lot, but I believe with conditions that any further split of the land must include stormwater management for all lots previously developed. In order to assist with this, the developer placed a storm water easement across the 2-10 ac parcels on the CSM. Unfortunately, he did not define on the CSM who owns the easement or what can be done on this easement.

The developer is now seeking to split the 30 ac lot into 4 lots. He has attempted and claims that he cannot get stormwater management to meet our Chapter 19 ordinances to meet the conditions of the previous CSM approval. The owners of the 2-10ac parcels with the easements are now refusing him access to their properties within the easement area. He has contacted the WisDNR which only requires water quality for the current development with the statement that they still need to meet City Ordinances (I've had discussions with Pete Wood, WisDNR representative, and he concurs his statements do not preclude them from meeting local ordinances).

The plan commission took action on 12/21/2023 to approve the CSM against Staff's concerns regarding the conditions we believe are in place from the previous approvals. We had a long discussion but didn't feel they could deny this further split based on actions from the previous PC/CC action.

I believe the intent was clear in the previous approvals of land division and that the developer would be required to include stormwater management for all these parcels developed post 2003 if he further subdivided the parcel. I would note that all of these land splits occurred under the same developer.

This development is creating 4 single family lots in addition to the 3 already created from this previously large undeveloped lot. Since all these splits are post 2003, the City will be responsible for all the impervious area on these parcels under our MS4 permits (and especially challenging with the TMDL's that are currently being developed). Our ordinance has very few exceptions for developments of parcels without stormwater management. The previous parcels were developed with the contingency that if further development occurs, they will need to meet storm water for all the previous developments as well to ensure compliance with our ordinances. I don't believe that because the drainage easement that the developer recorded was not clear that it eliminates the conditions placed on this further split.

If stormwater management is not required for this development, the City will be required to compensate for this development somewhere in the City to meet our MS4 permit requirements. The City ordinance was passed in 2003 requiring storm water management for all new developments to protect the existing community from paying for the costs of development. This is extremely important for the City as regulations of storm water management are only increasing and land available for cost effective measures to overcompensate for the non-treated development (ie. Ponds) are limited and becoming costly to purchase. I would further note that this area has substantial wetlands nearby which may not be used for water quality and quantity treatment.

If the Council determines to exempt the current development from providing storm water management from the previous land splits, it should be clearly noted that this current development must meet our storm water ordinances. In addition, any further land division must meet City Ordinances even if it is less than 1 acre to minimize any further impacts to the City by this development.

ARCTM
(262) 542-8200

4092512

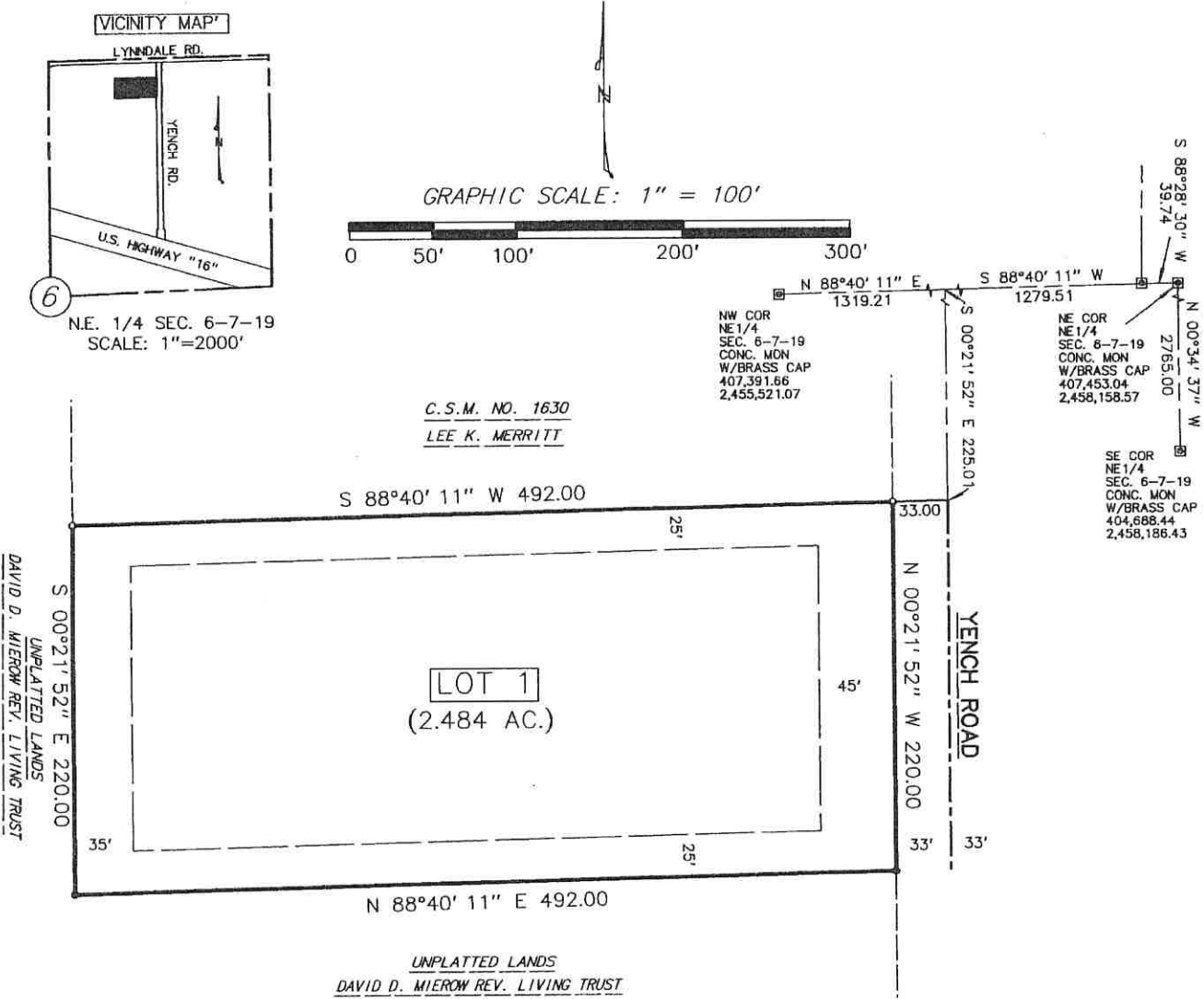


PG #140619-1

FORM ARC-101

CERTIFIED SURVEY MAP NO. 11205

A PART OF THE NORTHWEST 1/4 OF THE NORTHEAST 1/4 OF SECTION 6, TOWNSHIP 7 NORTH
RANGE 19 EAST, IN THE CITY OF PEWAUKEE, WAUKESHA COUNTY WISCONSIN



LEGEND

- - DENOTES 1-INCH DIA. IRON PIPE FOUND
- - DENOTES 1-INCH DIA. IRON PIPE SET
1.13 LB./LINFT
18 INCHES IN LENGTH
- - DENOTES CONC. MON.
W/ BRASS CAP FND.

SURVEYED FOR:
DAVID D. MIEROW REVOCABLE TRUST
3055 N. BROOKFIELD RD.
BROOKFIELD, WI 53045

SURVEYED BY: MICHAEL W. BUECHL, INC.
233 OAKTON AVENUE
PEWAUKEE, WI 53072
262-691-4444

BEARINGS ARE REFERENCED TO THE WIS. STATE
PLANE COORDINATE SYSTEM, SOUTH ZONE (NAD 1927)
WITH THE WEST LINE OF THE NE 1/4 OF SECTION 6
T7N, R19E HAVING A BEARING OF NORTH 00°34' 37" WEST

THIS INSTRUMENT WAS DRAFTED BY MICHAEL W. BUECHL



Michael W Buechl

CERTIFIED SURVEY MAP NO. 11205

A PART OF THE NORTHWEST 1/4 OF THE NORTHEAST 1/4 OF SECTION 6, TOWNSHIP 7 NORTH, RANGE 19 EAST, IN THE CITY OF PEWAUKEE, WAUKESHA COUNTY, WISCONSIN

SURVEYOR'S CERTIFICATE

State of Wisconsin)

Waukesha County)

I, Michael W. Buechl, a registered land surveyor, do hereby certify:

That I have surveyed, divided and mapped a parcel of land being a part of the Northwest 1/4 of the Northeast 1/4 of Section 6, Township 7 North, Range 19 East, in the City of Pewaukee, Waukesha County, Wisconsin, bounded and described as follows:

Commencing at the Northeast corner of said Northeast 1/4; thence South 88°28'30" West, along the North line of said Northeast 1/4, 39.74 feet to a point; thence North 88°40'11" West, along said North line 1279.51 feet to the centerline of Yench Road; thence South 00°21'52" East, along said centerline, 225.01 feet to a point; thence South 88°40'11" West, 33.00 feet to a point on the West line of Yench Road and the place of beginning of the lands to be described; continuing thence South 88°40'11" West, 492.00 feet; thence South 00°21'52" East, 220.00 feet; thence North 88°40'11" East, 492.00 feet to a point of the West line of Yench Road; thence North 00°21'52" West, along said West line, 220.00 feet to the place of beginning.

Containing 108,224 square feet (2.484472 acres) more or less.

That I have made this survey, land division and map by the direction of the owners of said land.

That such map is a correct representation of all exterior boundaries of the land surveyed and the division thereof made.

That I have fully complied with the provisions of Chapter 236 of the Wisconsin Statutes pertaining to Certified Survey Maps (Section 236.34 and the Regulations of the City of Pewaukee in surveying, dividing and mapping the same.

JUNE 3, 2014
Date

Michael W Buechl
Michael W. Buechl, Registered
Wisconsin Land Surveyor (S-1106)



This instrument was drafted by Michael W. Buechl.

Sheet 2 of 3

ARCTM
(262) 542-8200

FORM ARC-101

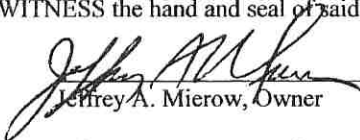
CERTIFIED SURVEY MAP NO. 11205

A PART OF THE NORTHWEST 1/4 OF THE NORTHEAST 1/4 OF SECTION 6, TOWNSHIP 7 NORTH, RANGE 19 EAST, IN THE CITY OF PEWAUKEE, WAUKESHA COUNTY, WISCONSIN

OWNER'S CERTIFICATE OF DEDICATION

AS OWNERS, David D Mierow Revocable Trust & Jeffrey A Mierow Et Al, We hereby certify that we have caused the land described in this document to be surveyed, divided, mapped and dedicated as represented on this map in accordance with the requirements of Chapter 236 of the Wisconsin Statutes pertaining to Certified Survey Maps (Section 236.34) and the Regulations of the City of Pewaukee

WITNESS the hand and seal of said owner this 4th day of June, 2014.


Jeffrey A. Mierow, Owner

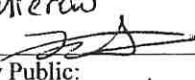

David D Mierow, Owner

State of Wisconsin)

Waukesha County)

PERSONALLY came before me this 4 day of June, 2014, Jeffrey A. Mierow known to be the person who executed the Owner's Certificate and acknowledged the same.
* and David D. Mierow

Notary Public:


Laura Schroeder


My commission expires: 4/28/17



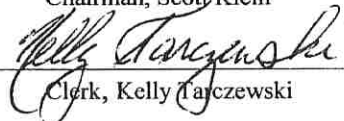
CERTIFICATE OF APPROVAL

RESOLVED that the above Certified Survey Map which has been filed for approval, be and is hereby approved as required by the Subdivision Regulations and Chapter 236 of Wisconsin Statutes relating to Certified Survey Maps. I hereby certify that the foregoing is a true and correct copy of a resolution adopted by the City Board of the City of Pewaukee on this 7th day of July, 2014.

Approved:


Chairman, Scott Klein

Signed:

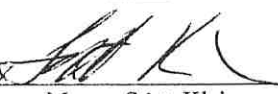

Clerk, Kelly Tarczewski



PLANNING COMMISSION APPROVAL

APPROVED by the City of Pewaukee Planning Commission, this 19th day of June, 2014.

Approved:


Mayor, Scott Klein

Signed:


Secretary





This instrument was drafted by Michael W. Buechl.

4092512

REGISTER OF DEEDS
WAUKESHA COUNTY, WI
RECORDED ON

July 31, 2014 02:52 PM
James R Behrend
Register of Deeds
3 PGS
TOTAL FEE: \$30.00
TRANS FEE: \$0.00

Book 110 Page 136-138

Sheet 3 of 3



4214210

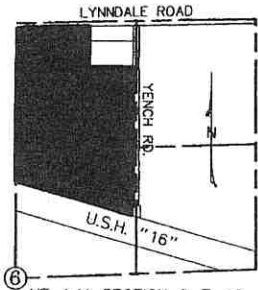
Book 114 Page 82-85



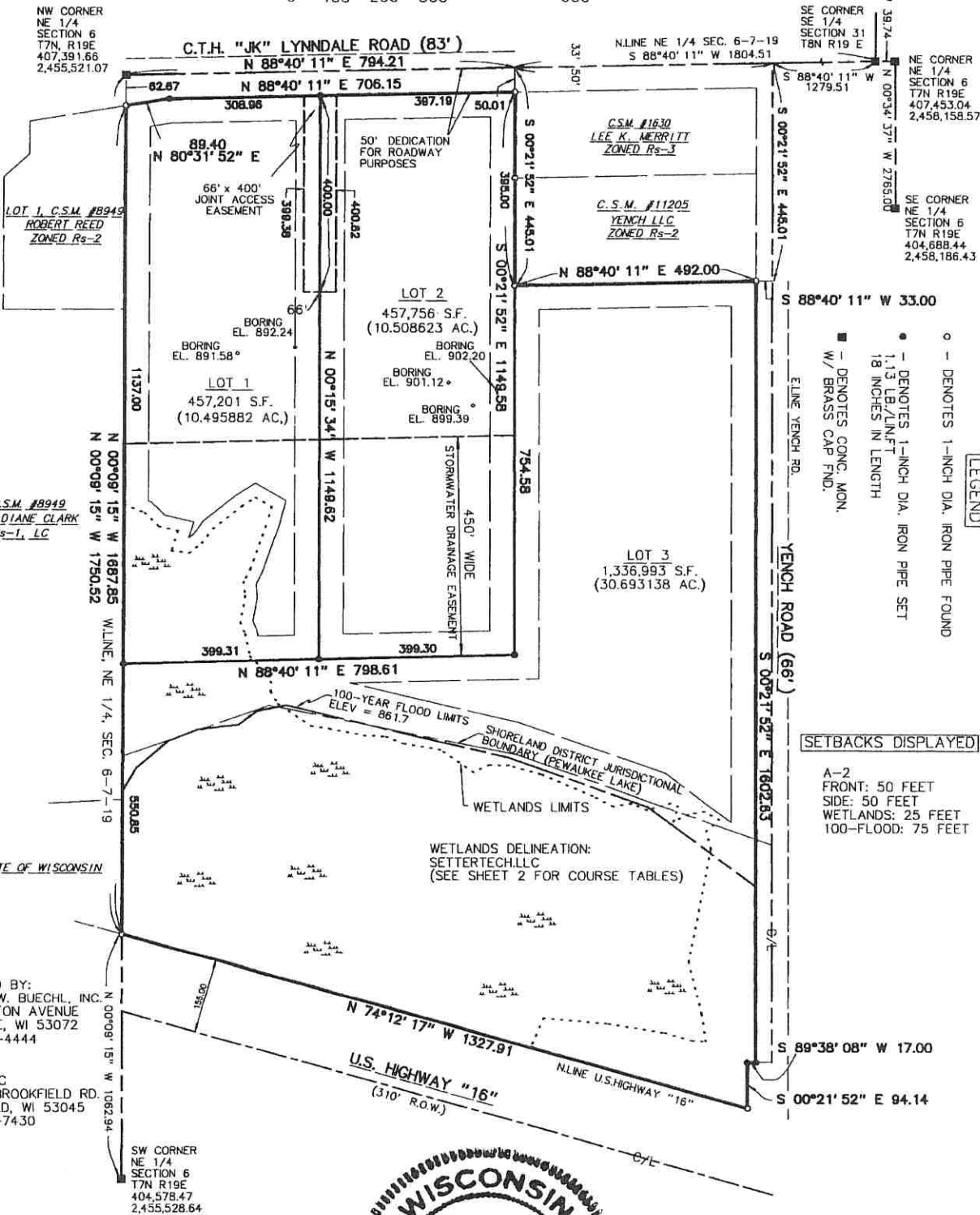
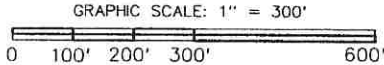
FORM ARC-101

CERTIFIED SURVEY MAP NO. 11404

A PART OF THE NORTHWEST 1/4 AND SOUTHWEST 1/4 OF
THE NORTHEAST 1/4 OF SECTION 6, TOWNSHIP 7 NORTH, RANGE 19 EAST
IN THE CITY OF PEWAUKEE, WAUKESHA COUNTY, WISCONSIN



NE 1/4 SECTION 6-7-19
SCALE: 1" = 2000'
VICINITY SKETCH



SURVEYED BY:
MICHAEL W. BUECHL, INC.
233 OAKTON AVENUE
PEWAUKEE, WI 53072
262-691-4444

OWNERS:
YENCH LLC
3055 N. BROOKFIELD RD.
BROOKFIELD, WI 53045
262-786-7430

BEARINGS ARE REFERENCED TO THE WIS. STATE
PLANE COORDINATE SYSTEM, SOUTH ZONE (NAD 1927)
WITH THE EAST LINE OF THE NE 1/4 OF SECTION 6
T7N, R19E HAVING A BEARING OF NORTH 00°34' 37" WEST

THIS INSTRUMENT WAS DRAFTED BY MICHAEL W. BUECHL

PE-6211-W



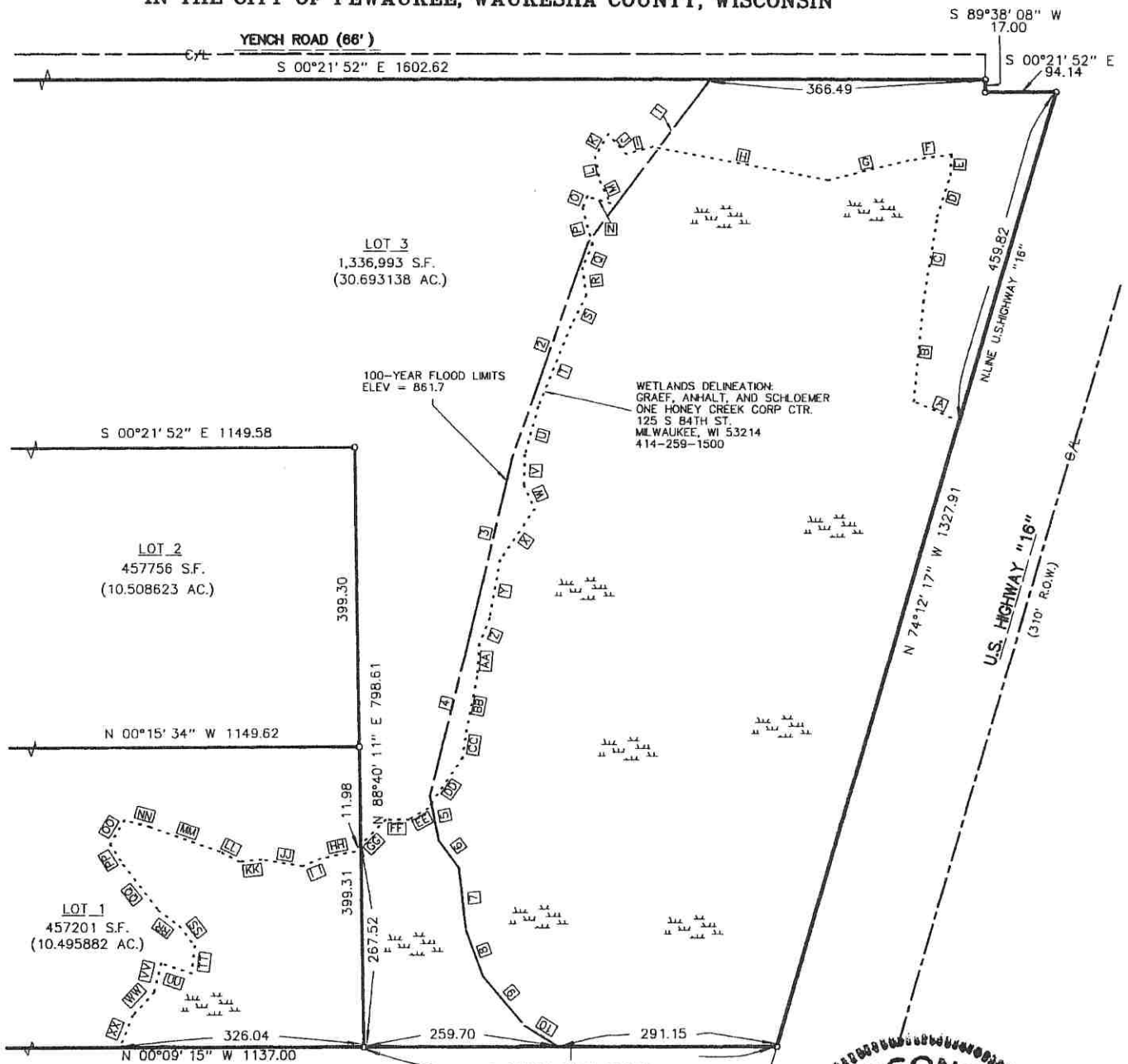
1. F-1 & LC ZONED LANDS SHALL NOT BE FILLED,
EXCAVATED, OR OTHERWISE DISTURBED
IN ANY MANNER.

2. VEHICULAR ACCESS TO C.T.H. "JK" PROHIBITED
EXCEPT THROUGH DESIGNATED JOINT ACCESS
EASEMENT.

SHEET 1 OF 4

CERTIFIED SURVEY MAP NO. 11464

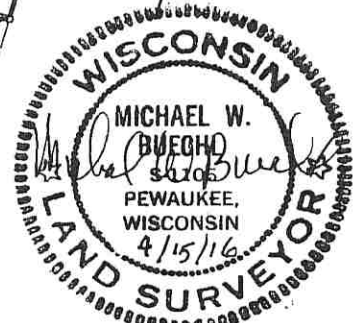
A PART OF THE NORTHWEST 1/4 AND SOUTHWEST 1/4 OF
THE NORTHEAST 1/4 OF SECTION 6, TOWNSHIP 7 NORTH, RANGE 19 EAST
IN THE CITY OF PEWAUKEE, WAUKESHA COUNTY, WISCONSIN



WETLANDS COURSE TABLE	
COURSE	BEARING & DISTANCE
A	N 23°08' 12" E 66.19
B	S 84°51' 33" E 133.64
C	S 81°15' 34" E 116.11
D	S 73°10' 58" E 58.12
E	S 87°33' 17" E 27.69
F	N 08°36' 08" W 58.32
G	N 14°09' 57" W 110.80
H	N 10°32' 38" E 237.87
I	N 16°00' 16" W 36.28
J	N 48°07' 29" E 35.80
K	N 59°52' 48" W 35.05
L	S 78°19' 44" W 34.08
M	S 63°51' 05" W 32.44
N	N 16°30' 08" E 32.09
O	N 67°40' 47" W 17.09
P	S 73°53' 58" W 49.87
Q	N 67°50' 10" W 34.40
R	S 81°36' 53" W 37.50
S	N 64°00' 41" W 46.73
T	N 68°09' 42" W 115.16
U	N 75°13' 52" W 66.50
V	N 88°35' 45" W 39.81
W	S 65°41' 48" W 35.28
X	N 56°11' 23" W 82.10
Y	N 80°25' 53" W 78.54
Z	N 67°53' 38" W 36.40
AA	N 87°07' 23" W 44.61
BB	N 79°00' 05" W 72.01
CC	N 82°14' 50" W 37.36
DD	N 56°14' 12" W 82.65
EE	N 29°29' 12" W 29.00

WETLANDS COURSE TABLE	
COURSE	BEARING & DISTANCE
FF	N 01°06' 33" W 30.85
GG	N 46°03' 03" W 59.32
HH	N 12°19' 14" W 46.55
II	N 22°22' 27" W 26.29
JJ	N 09°27' 06" E 55.46
KK	N 06°07' 28" W 29.10
LL	N 26°49' 11" E 30.20
MM	N 19°37' 40" E 99.17
NN	N 15°00' 18" E 34.77
OO	N 60°05' 46" W 37.72
PP	S 57°34' 41" W 32.50
QQ	S 51°46' 36" W 77.41
RR	S 35°59' 48" W 34.79
SS	S 52°10' 28" W 33.25
TT	N 83°29' 52" W 36.21
UU	N 17°45' 25" E 43.13
VV	N 77°07' 38" W 39.72
WW	N 46°42' 42" W 41.27
XX	N 68°20' 35" W 45.70

100-YEAR FLOOD COURSE TABLE	
COURSE	BEARING & DISTANCE
1	N 53°49' 21" W 272.79
2	N 71°03' 16" W 303.71
3	N 77°02' 58" W 212.10
4	N 76°28' 59" W 252.87
5	S 78°03' 34" W 61.45
6	S 53°19' 50" W 45.45
7	S 83°01' 53" W 83.42
8	S 69°13' 18" W 64.33
9	S 49°21' 12" W 86.24
10	S 31°20' 35" W 53.16





CERTIFIED SURVEY MAP NO. 11464

A PART OF THE NORTHWEST 1/4 AND SOUTHWEST 1/4 OF THE NORTHEAST 1/4 OF
SECTION 6, TOWNSHIP 7 NORTH, RANGE 19 EAST, IN THE CITY OF PEWAUKEE,
WAUKESHA COUNTY, WISCONSIN

SURVEYOR'S CERTIFICATE

State of Wisconsin)
:
Waukesha County)

I, Michael W. Buechl, a professional land surveyor, do hereby certify:

That I have surveyed, divided and mapped a parcel of land being a part of the Northwest 1/4 of the Northeast 1/4 of Section 6, Township 7 North, Range 19 East, in the City of Pewaukee, Waukesha County, Wisconsin, bounded and described as follows:

Commencing at the Northeast corner of said Northeast 1/4; thence South 88°28'30" West, along the North line of said Northeast 1/4, 39.74 feet to the Southeast corner of the Southeast 1/4 of Section 31, Township 8 North, Range 19 East; thence South 88°40'11" West, along said North line 1279.51 feet to the centerline of Yench Road; thence South 00°21'52" East, along said centerline, 445.01 feet to a point; thence South 88°40'11" West, 33.00 feet to a point on the West line of Yench Road and the place of beginning of the lands to be described; thence along said West line the following 3 courses: South 00°21'52" East, 1602.63 feet; South 89°38'08" West, 17.00 feet; South 00°21'52" East, 94.14 feet to the North line of U.S. Highway "16"; thence North 74°12'17" West, along said North line, 1327.91 feet to the West line of said Northeast 1/4; thence North 00°09'15" West, along said West line, 1750.52 feet to the Northwest corner of said Northeast 1/4; thence North 88°40'11" East, along said North line, 794.21 feet; thence South 00°21'52" East, 445.01 feet; thence North 88°40'11" East, 492.00 feet to the place of beginning.

Excepting the Northerly 50 feet for roadway purposes.

Containing 2,251,949 square feet (51.697643 acres) more or less.

That I have made this survey, land division and map by the direction of the owners of said land.

That such map is a correct representation of all exterior boundaries of the land surveyed and the division thereof made.

That I have fully complied with the provisions of Chapter 236 of the Wisconsin Statutes pertaining to Certified Survey Maps (Section 236.34 and the Regulations of the City of Pewaukee in surveying, dividing and mapping the same.

APRIL 15, 2016
Date

Michael W Buechl
Michael W. Buechl, Professional
Wisconsin Land Surveyor (S-1106)



ARCTM
(262) 542-8200

FORM ARC-101

CERTIFIED SURVEY MAP NO. 11464

**A PART OF THE NORTHWEST 1/4 AND SOUTHWEST 1/4 OF THE NORTHEAST 1/4 OF
SECTION 6, TOWNSHIP 7 NORTH, RANGE 19 EAST, IN THE CITY OF PEWAUKEE,
WAUKESHA COUNTY, WISCONSIN**

OWNER'S CERTIFICATE OF DEDICATION

YENCH, LLC, a limited liability company, existing under and by virtue of the Laws of the State of Wisconsin, as Owner, does hereby certify that said Company caused the land described in this document to be surveyed, divided, mapped and dedicated as represented on this map in accordance with the requirements of Chapter 236 of the Wisconsin Statutes pertaining to Certified Survey Maps (Section 236.34) and the Regulations of the City of Pewaukee.

WITNESS the hand and seal of said owner this 22nd day of June, 2016.

In the presence of:

Witness _____

Witness: _____

Yench, LLC

Owner Jeffrey Mierow, member

David Mierow, member



State of Wisconsin)

Waukesha County)

PERSONALLY came before me this 22nd day of June, 2016, Jeffrey Mierow*, to me known to be the person who executed the Owner's Certificate and acknowledged the same.

*and David Mierow

Notary Public: Laura Schroeder

My commission expires: 5/28/17

CERTIFICATE OF APPROVAL

RESOLVED that the above Certified Survey Map which has been filed for approval, be and is hereby approved as required by the Subdivision Regulations and Chapter 236 of Wisconsin Statutes relating to Certified Survey Maps. I hereby certify that the foregoing is a true and correct copy of a resolution adopted by the City Board of the City of Pewaukee on this 20 day of June, 2016.

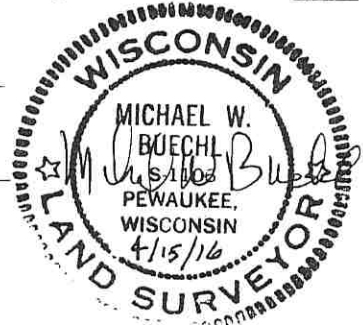


Approved: _____

Chairman, Scott Klein

Signed: _____

Clerk, Kelly Tarczewski



PLANNING COMMISSION APPROVAL

APPROVED by the City of Pewaukee Planning Commission, this 19 day of May, 2016.

Approved: _____

Mayor, Scott Klein

Signed: _____

Secretary, Brian Clinkenbeard
Steve Bierce

4214210

REGISTER OF DEEDS
WAUKESHA COUNTY, WI
RECORDED ON

June 23, 2016 01:06 PM
James R Behrend
Register of Deeds

4 PGS
TOTAL FEE:\$30.00
TRANS FEE:\$0.00

Book 114 Page 82-85

This instrument was drafted by Michael W. Buechl.

Sheet 4 of 4

85



Department of Public Works

W240 N3065 Pewaukee Road
Pewaukee, WI 53072

DPW Main Office: (262)-691-0804 Fax: 691-5729
Water & Sewer Division 691-0804 Fax: 691-5729
Street Division 691-0771 Fax: 691-6079
Engineering Division 691-0804 Fax: 691-5729

TO: Plan Commission

FR: Jeffrey Weigel, Public Works Director

DT: October 15, 2015

RE: CSM 151015-1 Yench LLC on Yench Road

We received a copy of the referenced CSM on October 9, 2015 and offer the following review comments:

- 1) It should be noted that last year the petitioners were approved for the lot abutting proposed Lot 4 as a single parcel land division (CSM 11205).
- 2) This year the petitioners presented the Glacier View Estates preliminary plat for approval. Later, the developer withdrew the plans and plat for Glacier View Estates.
- 3) This new proposal involves the development of 4 lots, constituting a Minor Land Division CSM Platting Procedure under City Ordinances (see section 18.0600).
- 4) The Minor Land Division CSM Procedure generally follows the traditional platting procedure. In this case, however, there may be no public infrastructure required for installation, but storm water management elements may be required. The proposed CSM land division creates 4 lots, and under the City Storm Water Ordinance any *development* that will disturb 1 acre or more (total) will require a storm water management plan. It appears that the development of, and building of the four homes or farms on these parcels will most likely disturb more than 1 acre, total. The land disturbances include grading, excavations and even utility installations such as natural gas to the homes.
- 5) One of our concerns is that Lot 4, although being re-zoned as agricultural, appears to be a single family lot of 2.48 acres. The existing topography for this lot, and of the CSM lot created last year is definitively to the south and west, and will cross parts of proposed Lot 2 and proposed Lot 3. One way to address this cross-lot drainage from the improved lands that include impervious surfaces is to design drainage swales to the south, most likely along the common lot lines of Lots 4, 3 and 2 in dedicated easements; however, it should be noted that the site is heavily treed, and as such, the swale excavations may have significant impacts on the trees and natural state of the lots.
- 6) With approximately 800 feet of frontage, proposed Lot 3 may have the potential for additional subdivision, such as with lots similar to the lot created last year under CSM 11205. Under the DNR's application of the *Common Plan* process, such additional land divisions would require



Department of Public Works

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the entire site to be designed, retroactively, so that land division process of 1-CSM at a time does not allow the developer to circumvent the State and City storm water management regulations. In other words, if the developer intends that Lot 3 remain as is, without future development then the appropriate deed restrictions should be in place; if the intent is to reserve the opportunity to further subdivide Lot 3 at some time in the future, then a storm water management plan is required for the entire site as a part of this subdivision application.

- 7) With Lots 1, 2 and 3 all exceeding 10 acres, there is a possibility that each of these lots could prepare and incorporate Stormwater management plans individually on each site; however, we need to check with DNR if this would be either allowable or advisable and as previously stated, we have had less than a week to review and comment on this CSM.
- 8) It should be noted and clarified that agricultural activities (farming, etc.) are exempted from the Storm Water Management regulations (but not fees), but lots that are zoned agricultural, but used as non-agricultural (non-farming) are not exempt.
- 9) As proposed, under the A-2 zoning, all of the trees can be removed from these lots without permission from the City and the buildable area completely enclosed by buildings or pavement. This would obviously require storm water management. Alternatively, all of the trees could be removed and the land could be farmed, that would require no storm water management plan. In either case, however, the property will receive an annual storm water bill for all impervious area on the lot.
- 10) Under all scenarios the developer will need to prepare a grading plan for City review that will appropriately convey the storm water run-off through the development. The development of a storm water management plan requirement will be determined after consultation with local DNR staff.
- 11) We recommend that the construction of the drainage system, presumed to be ditches, will be required before the City executes the CSM unless a separate development agreement between the developer/City is in place to define the necessary construction and timing of same.
- 12) Waukesha County must approved the access locaton and construction on CTH JK.

In attendance: Mayor S. Klein, Aldermen S. Bierce, C. Brown, J. Kara, D. Noll, J. Wamser and B. Bergman. Also present were Clerk/Treasurer K. Tarczewski, City Administrator T. LaBorde, DPW Director J. Weigel, Parks, Recreation & Community Services Director K. Woldanski, Fire Chief K. Bierce, HR Director L. Bergersen and Attorney S. Riffle. City Planner H. Clinkenbeard arrived at 6:38 p.m.

1.0 Call to Order and Pledge of Allegiance - Mayor Klein called the meeting to order at 6:02 p.m.

2.0 Public Comment - None.

3.0 Consent Agenda

3.1 Approval of Common Council Meeting Minutes

3.1.1 Meeting Minutes dated October 5, 2015

3.1.2 Meeting Minutes dated October 19, 2015

3.1.3 Special Meeting Minutes dated October 26, 2015

3.2 Accounts Payable Summaries

3.3 Bartender Licenses

3.4 Concur with the Plan Commission to Approve **Ordinance #15-13** to Rezone the property owned by Yench Properties LLC located in the SW Quadrant of the Intersection of Yench Road and Lynndale Road from RS-2 Single-Family Residential to A-2 Agricultural (PWC 0885-996) (**Second Reading**)

3.5 Concur with the Plan Commission to **Approve** Certified Survey Map PC#151015-1 to Divide the Yench LLC Property on Yench Road into no more than four parcels (PWC 0885-996)

3.6 Concur with the Plan Commission to **Approve** Certified Survey Map PC#151015-2 to Combine Two Lots owned by Thomas Hamm located at W277 N2864 Chicago Avenue (PWC 0934091)

Mayor Klein removed Items 3.4 & 3.5 for discussion.

A motion was made and seconded, (S. Bierce, J. Wamser) to approve the remaining items on the consent agenda. Motion Passed: 6-For, 0-Against. Mr. Noll noted he was abstaining from items 3.1.2 & 3.1.3 since he did not attend those meetings.

3.4 Concur with the Plan Commission to Approve Ordinance #15-13 to Rezone the Property owned by Yench Properties LLC located in the SW Quadrant of Intersection of Yench Road and Lynndale Road from RS-2 Single-Family Residential to A-2 Agricultural(PWC 0885-996)

3.5 Concur with the Plan Commission to Approve Certified Survey Map PC#151015-1 to Divide the Yench LLC Property on Yench Road into no more than four parcels (PWC 0885-996)

Mr. Weigel stated the Yench CSM is different from the normal CSMs that come before the Common Council. It will be creating (4) parcels; (2) 10-acre parcels, (1) 28-acre parcel and (1) 2 acre parcel. According to the City's land development code the City should treat this as a minor CSM land division. The creation of these (4)

would require the property owner to follow storm water management regulations. Under agricultural the land can change drastically and there are too many variables. Recommendation, pre-CSM subdivision recording which is similar to a preliminary plat approval; which would say, 'yes we are in agreement with the Plan Commission but we have to work out some details before it can come back for final approval'.

Mr. Weigel stated the Engineering Department recommended to the Plan Commission that they follow the procedure outlined in Section 18.0600 of the City Municipal Code – Minor Land Division (CSM) Platting Procedure.

A motion was made and seconded, (S. Bierce, D. Noll) to preliminarily approve the zoning and certified survey map subdividing the property contingent upon it coming back to the Council for final approval once the City Engineer's concerns have been satisfied. Motion Passed: 6-For, 0-Against.

4.0 Public Hearing Regarding the 2016 Proposed Budget

Ms. LaBorde reviewed the changes that were made to the original budget submitted. She reported these changes would produce a \$3.03 tax rate; resulting in a \$6.97 reduction of taxes on a home assessed at \$250,000. She urged the Council to consider taking less of the surplus funds from the unassigned account to supplement the budget because it would bring our reserve down to 16% and she feared the tax levy restrictions will eventually catch up to City.

Mayor Klein opened the Public Hearing. Keith Kramar (1861 River Lakes Road South-Oconomowoc) General Manager from the Marriott Hotel and Chairman of Pewaukee Tourism Committee spoke of the importance to use tourism funds in the proper manner and that was to promote tourism and hotel stays. He questioned the funds that have been used to fund the Sports Complex. Mayor Klein stated we hope that it would be used for tournaments. Mr. Kramar voiced his concerns about potentially increasing the hotel tax. He said the lower tax rate generates more stays and allows Pewaukee to be more competitive with other communities who are higher.

There was further discussion regarding the control of the tourism funds. Previously the City decided how the 70% of tourism funds were distributed based on the criteria that promoted tourism and development. Now the money should be spent on tourism and tourism development. Attorney Riffle stated effective 1/1/2017 the City will have to relinquish Common Council's determination on how the money will be spent and create a commission that will need to contract for secretarial services. He added there will be no changes for 2016; the City of Pewaukee will still be in control of the funds.

Mr. Kramar asked how the City would spend the extra 2% for the increased hotel tax. Mayor Klein stated he wasn't certain. Ms. LaBorde added that previous request for funds required the City to spend funds over what was anticipated to be received.

Mayor Klein closed the public hearing at 6:44 p.m. when no one else expressed an interest in speaking.

Ms. LaBorde reviewed what was spent out of the contingency account in 2015 and the proposed balance of the unassigned fund in 2016. She reminded them that it is ideal to retain 20 – 25% in our undesignated funds per our finance policy. Ms. LaBorde also repeated the recommendation of Ehlers to increase our levy based on what is allowed per our growth.

The hotel tax issue was revisited. Mayor surveyed the alderman; 4 were against raising the

and should not be signed. The Department of Transportation wants the driveway moved to the newly acquired property. The petitioner is aware of this request and is okay of the change. **A motion was made and seconded, (B. Bergman, J. Wamser) to table this item until the new CSM is received and reviewed by professional staff.** Motion Passed: 5-For, 0-Against.

- 5.10 Approval of **Ordinance 16-20** Regarding the Rezoning of the Yench LLC / Mierow Property Identified as PWC 0885-996 From RS-2 Single Family Residential to A-2 Agricultural

- 5.11 Approval of the Certified Survey Map PC #160519-1 to Divide the Yench LLC / Mierow Property into Three (3) Lots

Mr. Weigel stated there are storm water management issues that need to be addressed. He recommended not approving these items until the engineer has time to review. **A motion was made and seconded, (B. Bergman, J. Wamser) to table Items 5.10 & 5.11.** Motion Passed: 5-For, 0-Against.

- 5.12 Approval of the Conditional Use Permit for PBB Pewaukee I LLC to Operate the Point Burger Bar Restaurant and Tavern Located at W229 N1400 Westwood Drive in the Former American TV Building (PWC 0959-988-004) with Recommended Contingencies

Mr. Weigel noted an important condition was not listed on the conditional use permit as discussed at the Plan Commission meeting. He stated he is requesting payment of Reserve Capacity Assessment (RCA) for sewer and water based on the new use and updating the original use of the building because they will be using more water and sewer than before.

Mayor Klein stated another issue was brought up and needs to be discussed. It was noted that during a fire inspection the petitioner was building a patio which was not discussed previously or approved by the Plan Commission as part of their conditional use. **A motion was made and seconded, (S. Bierce, J. Wamser) to deny the conditional use permit and send the petitioner back to the Plan Commission for further discussion.** Motion Passed: 3-For, 2-Against (Brown, Bergman).

- 5.14 Approval of Woodleaf Reserve Addition No. 1 Final Plat
- 5.14.1 Declaration of Protective Covenants, Conditions, Easements and Restrictions
 - 5.14.2 Amendment No. 1 to Storm Water Management Practices Maintenance Agreement

Jim Doering was present for this item. Mr. Weigel reported the paving for the subdivision was completed on Friday, which was a condition for the final approval of the plat. He also recommended approval of the amendment to the Storm Water Management Practices Maintenance Agreement contingent upon approval of professional staff. **A motion was made and seconded, (B. Bergman, C. Brown) to approve the Woodleaf Reserve Addition No. 1 final plat, the declaration of protective covenants, conditions, easements and restrictions as well as the first amendment to the storm water management practices**

In attendance: Mayor S. Klein, Aldermen B. Bergman, S. Bierce, J. Kara, R. Grosch. C. Brown, J. Wamser were absent and excused. Also present were Clerk/Treasurer K. Tarczewski, DPW Director J. Weigel, Attorney S. Riffle, Fire Chief K. Bierce, and City Planner H. Clinkenbeard.

- 1.0 Call to Order and Pledge of Allegiance - Mayor Klein called the meeting to order at 7:00 p.m.
- 2.0 Public Comment – None.

3.0 Consent Agenda – Action

- 3.1 Approval of Common Council Meeting Minutes dated June 6, 2016
- 3.2 Accounts Payable Summaries
- 3.3 Bartender Licenses
- 3.4 Adoption of the City's 2050 Land Use / Transportation Plan for the North Bluemound Neighborhood as Approved and Recommended by the City Plan Commission (PC 6/1/2016)
- 3.5 Approval of the Holding Tank Agreement as Requested by Kave Enterprises for the Property Located at W237 N689 Oakridge Lane (PWC 0967-979-001)
- 3.6 Approval of **Resolution 16-06-07** Related to the DNR Annual Sewer System Compliance Maintenance Annual Report (CMAR)
- 3.7 Approval of **Ordinance 16-19** Regarding the Rezoning of the Jaeschke Property Located at W239 N3368 Pewaukee Road (PWC 0907-997 & PWC 0907-996) from RS-3 Single Family Residential to B-6 Mixed Use Business (*Third reading*)
- 3.8 Approval of Certified Survey Map PC #160519-2 (Revised) to Combine the Janssen Office Property with Two (2) Vacant Lots
- 3.9 Approval of **Ordinance 16-20** Regarding the Rezoning of the Yench LLC / Mierow Property Identified as PWC 0885-996 from RS-2 Single Family Residential to A-2 Agricultural (*Third reading*)
- 3.10 Approval of Certified Survey Map PC #160519-1 to Divide the Yench LLC / Mierow Property into Three (3) Lots

Mayor Klein noted the minutes were not completed as of yet and pulled them from the consent agenda. Mr. Bergman asked that the Items 3.9 and 3.10 be removed for discussion.

A motion was made and seconded, (S. Bierce, R. Grosch) to approve the remaining items on the consent agenda. Motion Passed: 4-For, 0-Against.

- 3.9 Approval of **Ordinance 16-20** Regarding the Rezoning of the Yench LLC / Mierow Property Identified as PWC 0885-996 from RS-2 Single Family Residential to A-2 Agricultural.

- 3.10 Approval of Certified Survey Map PC #160519-1 to Divide the Yench LLC / Mierow Property into Three (3) Lots

Mr. Bergman wanted to verify that the Engineering Department was satisfied with their storm water management plan. Mr. Weigel confirmed everything was submitted properly and they are satisfied.

A motion was made and seconded, (B. Bergman, J. Kara) to approve Ordinance 16-20 and Certified Survey Map PC #160519-1. Motion Passed: 4-For, 0-Against.

4.0 Discussion and Possible Action Regarding the Outdoor Entertainment Permit Requests (PC 5/19/2016)

- 4.1 5 O'Clock Club
- 4.2 Boomer's Sports Pub & Grill
- 4.3 Curly's Waterfront
- 4.4 Duplainville Station
- 4.5 Edgewater of Pewaukee
- 4.6 Gina's Sports Dock
- 4.7 Waukesha Gun Club
- 4.8 Wonderland Tap (PC 6/16/16)

Mr. Bierce stated he noticed that Boomer's wanted to increase the time for their outdoor music. He stated he felt the time should be consistent with the other bars. Mr. Bergman agreed, especially because it was so close to residential areas.

A motion was made and seconded, (S. Bierce, R. Grosch) to approve the outdoor entertainment permits; limiting Boomer's Sports Pub and Grill music permit to 9:00 p.m. during the week (Sunday – Thursday). Taylor Pasdera was present for this item and he stated he only listed those times for the purpose of using lights. He stated he is not planning on having any live music. Motion Passed: 4-For, 0-Against.

5.0 Public Hearing Regarding the Liquor License Applications for the 2016 – 2017 Licensing Period and Possible Action to Issue

Mr. Bierce noted a couple of concerns; he said the Building Inspection Department has been trying to make contact with the Edgewater Bar concerning their remodeling project and no one has been responsive to their attempts. He also said the Point Burger Bar has to go before the Plan Commission again to revise their Conditional Use Permit. He said when it originally went before the Plan Commission there was no discussion regarding a game room or their intention to serve food and alcohol on their patio. Attorney Riffle advised not to table these items, but instead make them contingent upon meeting all City code requirements and appropriate zoning codes. In the instance of the liquor license renewal of Edgewater they could be shut down on July 1st if they fail to meet any of the City codes. A representative from the Point Burger Bar was present. He asked if there would still be a license available if it was tabled to a later date. Attorney Riffle stated the license should be contingent upon them meeting the zoning codes to ensure they get it.

A motion was made and seconded, (S. Bierce, R. Grosch) to approve the liquor licenses with the noted conditions/contingencies. Motion Passed: 4-For, 0-Against.

6.0 Discussion and Possible Action Regarding the City Hall Roof, HVAC Replacement Contract Bids

Mayor Klein explained the scope and phases of the proposed projects. He said the 1st phase consisted of repairing the leaking roof over the City Hall offices, replacing the failing HVAC system and other minor things and the 2nd phase of the project would be to repair the roof over the highway garage, repair the bowing wall and internal remodeling in the garage area. He stated the project has been bid twice due to irregularities related to the form, the low bidder

Chairman Klein read a letter from Mr. Kiser, a citizen who resides in the area, indicating his concerns about various points (see file) and indicated that until there was a more definitive proposal and the people in the neighbor could really see what was being proposed, they were against this rezoning.

Mr. Stollenwerk asked if it was possible to get conceptual approval of this kind of use, rather than a final approval.

At that point, there being no further comments from the audience, Chairman Klein closed the public hearing at 7:30 PM. There was then further discussion. It was pointed out that yes, there is a possibility that they could get conceptual approval in light of the fact that the Plan Commission had talked about changing of the land use plan in this area because of the shallow water table and the fact that it was unlikely that a single-family subdivision could be built on this land. The land was probably best suited for on-slab construction similar to industrial use construction.

It was pointed out that the Plan Commission would either have to approve this tonight or deny it unless the representatives of the two families agreed to ask the City for an extension of this proposal for a period of time. The representatives of the two families indicated that they would be willing to provide an extension if the City would go along with that for some period. There was then some discussion about the amount of time. It was determined that the earliest that any feedback could be had regarding the long range land use plan would be at the February meeting. It was then agreed between the Plan Commission and Mr. Stollenwerk and the Kanters that this proposed rezoning would be brought back up at the February meeting. It was pointed out that at that time, it would have to be determined whether or not there had been enough public input to the plan to make a final determination.

At that point, Chairman Klein asked the Plan Commission members if they had any problems with conceptually approving. The Plan Commission members indicated that probably not.

A motion was made by Mr. Coursin, seconded by Mr. Linsmeier on the request of the petitioners to approve the proposal conceptually but delay any further action or activity on this proposal until the February 2016 meeting of the Plan Commission. There was no discussion regarding this item and the motion passed unanimously.

DISCUSSION & ACTION REGARDING A PETITION FOR A REZONING PUBLIC HEARING FOR YENCH PROPERTIES LLC TO REZONE THEIR PROPERTY IN THE SW QUADRANT OF THE INTERSECTION OF YENCH ROAD & LYNNDAL ROAD FROM RS-2 SINGLE-FAMILY RESIDENTIAL TO A-2 AGRICULTURAL AND CERTIFIED SURVEY MAP PC #151015-1 TO DIVIDE THE YENCH LLC PROPERTY ON YENCH ROAD INTO NO MORE THAN FOUR PARCELS (PWC 0885996)

Mr. Clinkenbeard pointed out the location of the site. Mr. Mierow came forth representing Yench Properties LLC indicating that what they were now proposing was a creation of four lots, two ten-acre lots, a two-acre lot on Yench Road, and then the remainder of the site that would be greater than ten acres. It was noted that the proposed two-acre lot was already zoned Rs-2 so that didn't need to be rezoned. The developable land on the two ten-acre lots and the larger remnant lot are currently

zoned Rs-2 and they were what would be rezoned to A-2 agriculture. Chairman Klein noted that this was one of the first times in a long time that we were being asked to actually ‘down-zone’ land from residential to agriculture. Mr. Mierow indicated that at some future date, probably beyond a five year period, he would probably want to rezone the larger remnant lot, (lot #3) on the certified survey map, into two or three two-acre lots at that time.

Mr. Weigel pointed out that there were a number of stormwater management concerns about this property in light of the fact that it was a very steep property and the water that comes off the land would flow across multiple properties in the area and something would have to be addressed in that regard, not only for this initial division, but also for future divisions of the land.

There was some more discussion after which Chairman Klein opened the public hearing at 8:01 PM and asked if there was anyone in the audience who wished to speak either in favor of or in opposition to this proposed rezoning. There being none, Chairman Klein closed the public hearing at 8:01 PM.

At that point, a motion was made by Mr. Coursin, seconded by Mr. Klein to rezone the developable land on the property, as indicated and shown on the accompanying certified survey map as lots 1, 2 and 3, from Rs-2 to A-2 Agriculture with the understanding that the stormwater issues as pointed out by the City Engineer would have to be addressed and at the very least, easements would have to be provided across properties to accommodate any future drainage and retention/detention pond requirements in the area. There was no discussion regarding this item and the motion passed unanimously.

A motion was made by Mr. Linsmeier, seconded by Mr. Sullivan to offer preliminary approval of the CSM PC #151015-1 dividing the Yench property as indicated into four parcels. It was pointed out again that there needed to be, at the very least, easements placed on the certified survey map to accommodate cross lot flow of stormwater and the location and building of retention/detention ponds in accordance with the City Engineers concerns. There was no discussion regarding this item and the motion passed unanimously.

DISCUSSION & ACTION REGARDING CERTIFIED SURVEY MAP PC #151015-2 TO COMBINE TWO LOTS OWNED BY THOMAS HAMM LOCATED AT W277 N2864 CHICAGO AVENUE (PWC 0934091)

Mr. Clinkenbeard pointed out that as shown on the aerial photograph they had been provided, Mr. Hamm’s house actually straddles a lot line and Mr. Hamm was simply trying to make the lot conforming to City requirements by combining the lots.

A motion was made by Mr. Coursin, seconded by Mr. Sullivan to recommend approval of the certified survey map PC #151015-2 as submitted. There was no discussion regarding this item and the motion passed unanimously.

DISCUSSION & ACTION REGARDING A PETITION FOR A REZONING PUBLIC HEARING FOR YENCH LLC/JEFF MIEROW TO REZONE PROPERTY LOCATED IN THE SOUTHWEST QUADRANT OF YENCH ROAD & LYNNDAL ROAD FROM RS-2 SINGLE-FAMILY RESIDENTIAL TO A-2 AGRICULTURAL & CERTIFIED SURVEY MAP PC #160519-1 TO DIVIDE THE JEFF MIEROW/YENCH LLC PROPERTY INTO THREE LOTS (PWC 0885996)

Mr. Clinkenbeard pointed out that if the zoning is approved, all of the land would revert to A-2 Agricultural except for the current wetlands and floodplain part of the lands which would remain under Floodplain and Lowland Conservancy zoning.

Mr. Weigel indicated that he saw no problem with the certified survey map, which was also on the agenda for this property.

At that point, Chairman Klein opened the public hearing at 7:27 PM and asked if there was anyone in the audience who wished to speak either in favor of or in opposition to the rezoning of the property. There being none, Chairman Klein closed the public hearing at 7:27 PM.

A motion was made by Mr. Coursin, seconded by Ms. Wunder to recommend the zoning of the property from Rs-2 to A-2 Agricultural and also the approval of the certified survey map dividing off two ten-acre lots on the newly rezoned property. There was no discussion regarding this item and the motion passed unanimously.

At that point, Mr. Coursin recused himself from the Plan Commission due to the fact that the business he worked for was involved in the next item on the agenda.

DISCUSSION & ACTION REGARDING A PETITION FOR A REZONING PUBLIC HEARING FOR AUDREY JAESCHKE/NICHOLAS DELTORTO TO REZONE TWO VACANT PARCELS IMMEDIATELY SOUTH OF THE JANNSEN OFFICE PROPERTY AT THE INTERSECTION OF PEWAUKEE ROAD & CAPITOL DRIVE FROM RS-3 SINGLE-FAMILY RESIDENTIAL TO B-6 MIXED USE BUSINESS FOR THE PURPOSE OF EXPANDING THE JANNSEN OFFICE PROPERTY & CERTIFIED SURVEY MAP PC #160519-2 TO COMBINE THE JANNSEN OFFICE PROPERTY WITH THE TWO VACANT PARCELS TO THE SOUTH (PWC 0907997 & PWC 0907996)

Mr. DelTorto came forward indicating that he was buying the Jannsen building and wanted to add the two vacant parcels to the south to the property. The first phase of his project would be simply to add a parking lot on one of the current properties to the south after it had been combined with the Jannsen property.

Mr. Weigel indicated that there was still a question about whether there would be two accesses to the newly created property or only one and what kind of accesses would be there. Plus, there was a need to finish the work with the applicant regarding storm water management. Mr. DelTorto indicated that in either event, if he was denied the southerly access to the property, he still had the northerly access that could be access to the new parking. He realized that the storm water management situation would have to be finalized.

From: [Jeff Mierow](#)
To: [Wagner, Magdelene](#)
Cc: [Fuchs, Nick](#); [Gabbey, Michaelis](#); [J. Pudelko](#); [Jayme Sisel](#); [Wirtz, Rich](#); [Wood, Peter C - DNR](#)
Subject: Re: Yench LLC CSM
Date: Thursday, November 16, 2023 1:58:15 PM

So I advised those owners when they built to asked the city if they had to do any rain gardens or swales when they built and we're told no by the city because they did not disturb enough area how can you try to tell me I should try to figure out how to make water go uphill. Why did the City require that drainage easement on the back of the property? What was the purpose of that?

On Thu, Nov 16, 2023 at 1:46 PM Wagner, Magdelene <wagner@pewaukee.wi.us> wrote:

Hi Pete,

Thank you for the discussion today regarding the Mierow development called Yench LLC in the City of Pewaukee. We have received a new submittal from the developer and his engineers are stating the site is only required to handle the storm water management from the 4 lots they are creating with this current CSM. From the submittal letter dated November 8, 2023: "The stormwater management plan for this project does not include other nearby properties or offsite areas that are downhill or not tributary to this site, as is appropriate and consistent with DNR policy. Pete Wood, a lead Water Resource Engineer at the DNR, has stated via email dated July 7, 2023, that this project is not considered a common plan of development with other homes built by others on lots adjacent to this property. Therefore, this project should not be burdened with retroactively creating storm water management facilities for downhill or offsite areas, and is not able to do so."

The development of this land (all current and previous CSM's) were from Mr. Mierow with the intent of developing the lots. In addition, it was made very clear to Mr. Mierow with the last CSM that if he were to develop this land further, he would have to retroactively address the storm water management for all past developments. The Plan Commission and Common Council approved the last development with this provision in place. In order to meet the City's ordinances and the approvals from the previous development, this site must include the storm water management for the previous development.

Please confirm that while the DNR regulations may not require the storm water management for the previous development, the development must meet the City ordinances, standards and previous approval requirements.

Thank you,

Maggie

Magdelene Wagner, P.E.

Director of Public Works/City Engineer

[City of Pewaukee](#)

[W240N3065 Pewaukee Road](#)

[Pewaukee, WI 53072](#)

262-691-0804

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From: [Wagner, Magdelene](#)
To: [Jeff Mierow](#)
Cc: [Fuchs, Nick](#); [Gabbey, Michaelis](#)
Subject: RE: Yench LLC CSM
Date: Monday, November 20, 2023 3:19:00 PM
Attachments: [Plan Commission Minutes 2015_10_15 Meeting\(316\).pdf](#)

Jeff,

Please find attached the Plan Commission minutes for your first Yench Road LLC CSM. I've highlighted the language I believe requires storm water management for the current and the previous development.

Thank you,
Maggie

Magdelene Wagner, P.E.
Director of Public Works/City Engineer
City of Pewaukee
W240N3065 Pewaukee Road
Pewaukee, WI 53072
262-691-0804

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From: Jeff Mierow <jeffmierow@mierowrealty.com>
Sent: Monday, November 20, 2023 11:18 AM
To: Wagner, Magdelene <wagner@pewaukee.wi.us>
Subject: Re: Yench LLC CSM

I will call you at 2.
Thanks
Jeff

On Mon, Nov 20, 2023 at 11:16 AM Wagner, Magdelene <wagner@pewaukee.wi.us> wrote:

Jeff,

I'm only available this afternoon for this week. Would a 2 pm call work for you?

Thanks,

Maggie

Magdelene Wagner, P.E.

Director of Public Works/City Engineer

City of Pewaukee

W240N3065 Pewaukee Road

Pewaukee, WI 53072

262-691-0804

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From: Jeff Mierow <jeffmierow@mierowrealty.com>

Sent: Monday, November 20, 2023 9:06 AM

To: Wagner, Magdelene <wagner@pewaukee.wi.us>

Subject: Re: Yench LLC CSM

Maggie -

Can we please connect over the phone this week to discuss this? Let me know your availability.

Thanks!

On Thu, Nov 16, 2023 at 1:57 PM Jeff Mierow <jeffmierow@mierowrealty.com> wrote:

So I advised those owners when they built to asked the city if they had to do any rain gardens or swales when they built and we're told no by the city because they did not disturb enough area how can you try to tell me I should try to figure out how to make water go uphill. Why did the City require that drainage easement on the back of the property? What was the purpose of that?

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the DNR, has stated via email dated July 7, 2023, that this project is not considered a common plan of development with other homes built by others on lots adjacent to this property. Therefore, this project should not be burdened with retroactively creating storm water management facilities for downhill or offsite areas, and is not able to do so.”

The development of this land (all current and previous CSM’s) were from Mr. Mierow with the intent of developing the lots. In addition, it was made very clear to Mr. Mierow with the last CSM that if he were to develop this land further, he would have to retroactively address the storm water management for all past developments. The Plan Commission and Common Council approved the last development with this provision in place. In order to meet the City’s ordinances and the approvals from the previous development, this site must include the storm water management for the previous development.

Please confirm that while the DNR regulations may not require the storm water management for the previous development, the development must meet the City ordinances, standards and previous approval requirements.

Thank you,
Maggie

Magdelene Wagner, P.E.

Director of Public Works/City Engineer

City of Pewaukee

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Pewaukee, WI 53072

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--

Jeff Mierow
Mierow Realty & Building Co
414.254.0731

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--

Jeff Mierow
Mierow Realty & Building Co
414.254.0731

From: [Wood, Peter C - DNR](#)
To: [Wagner, Magdelene](#)
Cc: [Gabbey, Michaelis](#); [Fuchs, Nick](#); [Jeff Mierow](#); [Wirtz, Rich](#); [J. Pudelko](#); [Jayme Sisel](#)
Subject: RE: Yench LLC CSM
Date: Thursday, November 16, 2023 3:13:56 PM

Maggie,

Any opinion I provided regarding this development was specific to DNR storm water regulations.

I'm not sure if my actual e-mail was provided to you but I was clear in the e-mail that DNR policy may not be the same as the City policy regarding common plan of development. The City is a separate unit of government that needs to implement its own storm water regulations.

Pete Wood, P.E.

Storm Water Engineer – Water Division
Wisconsin Department of Natural Resources
1027 W St Paul Avenue
Milwaukee, WI 53233
Phone: 262-822-8227
Peter.Wood@wisconsin.gov

We are committed to service excellence.

Visit our survey at <http://dnr.wi.gov/customersurvey> to evaluate how I did.

From: Wagner, Magdelene <wagner@pewaukee.wi.us>
Sent: Thursday, November 16, 2023 1:46 PM
To: Wood, Peter C - DNR <Peter.Wood@wisconsin.gov>
Cc: Gabbey, Michaelis <gabbey@pewaukee.wi.us>; Fuchs, Nick <fuchs@pewaukee.wi.us>; Jeff Mierow <jeffmierow@mierowrealty.com>; Wirtz, Rich <wirtz@pewaukee.wi.us>; J. Pudelko <jpudelko@trioeng.com>; Jayme Sisel <Jayme.Sisel@soundstormwater.com>
Subject: Yench LLC CSM

**CAUTION: This email originated from outside the organization.
Do not click links or open attachments unless you recognize the sender and know the content is safe.**

Hi Pete,

Thank you for the discussion today regarding the Mierow development called Yench LLC in the City of Pewaukee. We have received a new submittal from the developer and his engineers are stating the site is only required to handle the storm water management from the 4 lots they are creating with this current CSM. From the submittal letter dated November 8, 2023: "The stormwater management plan for this project does not include other nearby properties or offsite areas that are downhill or not tributary to this site, as is

appropriate and consistent with DNR policy. Pete Wood, a lead Water Resource Engineer at the DNR, has stated via email dated July 7, 2023, that this project is not considered a common plan of development with other homes built by others on lots adjacent to this property. Therefore, this project should not be burdened with retroactively creating storm water management facilities for downhill or offsite areas, and is not able to do so.”

The development of this land (all current and previous CSM’s) were from Mr. Mierow with the intent of developing the lots. In addition, it was made very clear to Mr. Mierow with the last CSM that if he were to develop this land further, he would have to retroactively address the storm water management for all past developments. The Plan Commission and Common Council approved the last development with this provision in place. In order to meet the City’s ordinances and the approvals from the previous development, this site must include the storm water management for the previous development.

Please confirm that while the DNR regulations may not require the storm water management for the previous development, the development must meet the City ordinances, standards and previous approval requirements.

Thank you,
Maggie

Magdelene Wagner, P.E.

Director of Public Works/City Engineer

City of Pewaukee

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Pewaukee, WI 53072

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**CITY OF PEWAUKEE
COMMON COUNCIL AGENDA ITEM 12.**

DATE: January 15, 2024

DEPARTMENT: Planning

PROVIDED BY: Nick Fuchs

SUBJECT:

Discussion and Possible Action Regarding **Ordinance 24-03** Revising Section 17.0500 of the City's Zoning Code Related to Review and Approval and Public Hearing Requirements for Conditional Use Permits [Fuchs]

BACKGROUND:

At their December 21, 2023, meeting, the Plan Commission unanimously recommended approval of the subject Zoning Code amendment related to public hearing requirements for conditional use permits.

FINANCIAL IMPACT:

RECOMMENDED MOTION:

A motion to approve revisions to Section 17.0500 of the City's Zoning Code related to public hearing requirements for Conditional Use Permits.

ATTACHMENTS:

Description

Ordinance 24-03

CUP hearing amendment staff report

Zoning Code 17.0500 amendment to CUP hearings

ORDINANCE NO. 24-03**AN ORDINANCE AMENDING REQUIREMENTS FOR CONDITIONAL USE
PERMITS IN CHAPTER 17 – ZONING**

WHEREAS, the City of Pewaukee Plan Commission held a public hearing on December 21, 2023 to discuss changes to Conditional Use Permit Regulations;

NOW THEREFORE, The Common Council of the City of Pewaukee, Waukesha County, Wisconsin do ordain the following sections shall be repealed and recreated to read the following:

17.0503 REVIEW AND APPROVAL

The City Plan Commission shall review the site, existing and proposed structures, architectural plans, neighboring uses, parking areas, driveway locations, highway access, traffic generation and circulation, drainage, sewerage and water systems, and the proposed operation prior to Common Council action (Also see sub-section 17.0210).

In addition:

- a. Conditions such as landscaping, architectural design, type of construction, flood proofing, anchoring of structures, construction commencement and completion dates, sureties, lighting, fencing, planting screens, operational control, hours of operation, improved traffic circulation, deed restrictions, signage, highway access restrictions, increased yards, storm water management improvements or parking requirements may be required by the Plan Commission upon its finding that these are necessary to fulfill the purpose and intent of this Ordinance. (Also see sub-section 17.0210)
- b. Notice to DNR: The Plan Commission shall transmit a copy of each application for a conditional use within a floodplain or shoreland area to the Wisconsin Department of Natural Resources (WisDNR) for review and comment. Action on the application shall not be taken for 30 days or until the WisDNR has made its recommendation, whichever comes first. A copy of all floodplain conditional use permits shall be transmitted to the WisDNR within 10 days of the effective date of such permit. (Rep. & Rec. 14-03)

17.0505 COMMON COUNCIL ACTION

- a. Following a public hearing held by the Common Council as set forth in Section 17.1300, and after careful consideration of the Plan Commission's recommendations, the Common Council may grant the Conditional Use Permit as applied for, grant the Conditional Use Permit with conditions deemed appropriate by the Common Council, or deny the permit. Compliance with all other provisions of this ordinance, such as lot width and area, yards, height, parking, loading, traffic, highway access, and

performance standards, shall be required of all conditional uses unless otherwise stipulated in the individual district regulations. Variances shall only be granted as provided in sections 17.1000 and 17.1100 of this ordinance. If the conditional use permit is granted (with or without conditions) official notice of such conditional use shall be transmitted to the County Register of Deeds on forms provided by the City. The County Register of Deeds shall take whatever action necessary to append the permitted conditional use forms to the deed of the property for which a conditional use has been granted. Any approving action by the Common Council will be valid only if the conditional use is recorded by the County Register of Deeds and the use or operation commenced within six (6) months from the date of such action.

- b. When the Common Council makes a determination either by observation or by official citizen complaint that a permitted conditional use is or has been apparently violated, the Common Council may convene a public hearing as set forth in section 17.1300, giving at least 30 days notice to the holder of the conditional use permit and, advising the permit holder as well as property owners within 300 feet of the conditionally permitted use of the purpose of the hearing, which may be alter, rescind, or reaffirm the conditional use permit then in effect. The Common Council, following a recommendation by the Plan Commission, may take action to rescind or alter the conditional use permit then in effect and by doing so direct the City Clerk to take any necessary steps to rescind or alter the covenants or other pertinent documents concerning the conditional use and attached to the property deed held in the office of the County Register of Deeds.
- c. Any conditional use that moves from the premises on which it was originally permitted or that ceases to occupy such premises for a period of three months or as evidenced by legal, authoritative documentation, shall be declared to be no longer a legitimate use of the premises and shall be removed from the list of

SECTION 2: Severability. The sections of this ordinance are declared to be severable. Should any sub-section, clause, or provision of this ordinance be declared by any court of competent jurisdiction to be invalid, the same shall not affect the validity of this ordinance as a whole or any part thereof, other than the part declared invalid. The remainder of the ordinance shall remain in full force and effect.

SECTION 3: Effective Date. This ordinance is effective upon publication or posting as provided by law.

This ordinance passed this 15th day of January 2024.

Steve Bierce, Mayor

ATTEST:

Kelly Tarczewski, City Clerk/Treasurer



Office of the Planner & Community Development Director
W240 N3065 Pewaukee Road
Pewaukee, Wisconsin 53072
Phone (262) 691-0770
fuchs@pewaukee.wi.us

Fax (262) 691-1798

REPORT TO THE PLAN COMMISSION

Meeting of December 21, 2023

Date: December 6, 2023

Project Name: Amendment to Public Hearing Requirements for Conditional Use Permits

Project Address/Tax Key No.: City-wide

Applicant: City of Pewaukee

Background

The attached amendment revises Section 17.0500 of the Zoning Code to require that the Common Council hold public hearings for Conditional Use Permits, opposed to the Plan Commission.

No other changes are proposed related to the Conditional Use process.

Recommendation

It is recommended that the Plan Commission recommend approval of the proposed text amendment to the Zoning Code.

SECTION 17.0500 CONDITIONAL USE REGULATION

17.0501 PERMITS

The City Common Council may authorize the City Planner or Zoning Administrator to issue a conditional use permit for conditional uses after review and a public hearing, provided that such conditional uses and related structures are in accordance with the purpose and intent of this Ordinance and are found to be not hazardous, harmful, offensive, or otherwise adverse to the environment or the value of the neighborhood or the community. Conditional uses are listed in the regulations of each zoning district as set forth in Section 17.0400 or this Chapter.

17.0502 APPLICATION

Applications for conditional use permits shall be made in triplicate to the City Clerk/Treasurer on forms furnished by the City Planner, Zoning Administrator or other designated City employee, and shall include at least the following:

- a. Name and addresses of the applicant, owner of the site, architect, professional engineer, contractor, and all opposite and abutting owners of record of property within 600 feet of the property for which a conditional use is being requested.
- b. Description of the subject site by lot, block, and recorded subdivision or by metes and bounds; address of the subject site; type of structure; proposed operation or use of the structure or site; number of employees; and the zoning district within which the subject site is located. For conditional uses within or in the vicinity of floodplains, such description shall also include information that is necessary for the Plan Commission to determine whether the proposed development will hamper flood flows, impair floodplain storage capacity, or cause danger to human or animal life. This additional information may include plans, certified by a registered professional engineer or land surveyor, showing elevations or contours of the ground at two (2)-foot intervals; fill or storage elevations; first floor elevations of structures; size, location, and spatial arrangement of all existing and proposed structures on the site; location and elevation of streets, water supply, and sanitary facilities; photographs showing existing land uses and vegetation upstream and downstream; soil types and other pertinent information.
- c. Certified survey map (CSM) or Plat of Survey prepared by a registered land surveyor showing all of the information required under Section 17.0203 for a Building and/or Zoning permit as well as the mean and historic high water lines, 100 year recurrence interval base flood floodplains on or within 75 feet of the subject premises, and existing and proposed landscaping.
- d. Additional Information as may be required by the Plan Commission, City Planner, City Engineer, Zoning Administrator, or Building Inspector.

17.0503 REVIEW AND APPROVAL

The City Plan Commission shall review the site, existing and proposed structures, architectural plans, neighboring uses, parking areas, driveway locations, highway access, traffic generation and circulation, drainage, sewerage and water systems, and the proposed operation, ~~and prior to Common Council action, shall hold a public hearing as set forth in Section 17.1300~~ (Also see sub-section 17.0210).

In addition:

- a. Conditions such as landscaping, architectural design, type of construction, flood proofing, anchoring of structures, construction commencement and completion dates, sureties, lighting, fencing, planting screens, operational control, hours of operation, improved traffic circulation, deed restrictions, signage, highway access restrictions, increased yards, storm water management improvements or parking requirements may be required by the Plan Commission upon its finding that these are necessary to fulfill the purpose and intent of this Ordinance. (Also see sub-section 17.0210)
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17.0504 SPECIAL CONDITIONAL USES NOT SET FORTH IN DISTRICT REGULATIONS MAY BE ALLOWED, INCLUDING:

- a. All uses similar in character to the permitted uses on the premises, as determined by the Plan Commission, which meet the intent of the district but which are not specifically listed as permitted principal, accessory or conditional uses within the text of the zoning district classification or other section of this ordinance.
- b. Uses proposed to be located in a Plan Commission approved multi – tenant building that are included on a list of uses for that building previously given ‘blanket’ approval by the Common Council following a conditional use hearing may not require individual conditional use permits but only approval of a Business Plan of Operation as set forth herein. Non-conditional use status of such uses shall be determined by the City Planner or the Plan Commission.

17.0505 COMMON COUNCIL ACTION

- a. Following a public hearing held by the Common Council as set forth in Section 17.1300, and after careful consideration of the Plan Commission's recommendations, the Common Council may grant the Conditional Use Permit as applied for, grant the Conditional Use Permit with conditions deemed appropriate by the Common Council, or deny the permit. Compliance with all other provisions of this ordinance, such as lot width and area, yards, height, parking, loading, traffic, highway access, and performance standards, shall be required of all conditional uses unless otherwise stipulated in the individual district regulations. Variances shall only be granted as provided in sections 17.1000 and 17.1100 of this ordinance. If the conditional use permit is granted (with or without conditions) official notice of such conditional use shall be transmitted to the County Register of Deeds on forms provided by the City. The County Register of Deeds shall take whatever action necessary to append the permitted conditional use forms to the deed of the property for which a conditional use has been granted. Any approving action by the Common Council will be valid only if the conditional use is recorded by the County Register of Deeds and the use or operation commenced within six (6) months from the date of such action.

- b. When the ~~Plan Commission~~Common Council makes a determination either by observation or by official citizen complaint that a permitted conditional use is or has been apparently violated, the ~~Plan Commission~~Common Council may convene a public hearing as set forth in section 17.1300, giving at least 30 days notice to the holder of the conditional use permit and, advising the permit holder as well as property owners within 300 feet of the conditionally permitted use of the purpose of the hearing, which may be to ~~recommend to the Common Council the altering~~, rescinding, or reaffirmation of the conditional use permit then in effect. The Common Council, following a recommendation by the Plan Commission, may take action to rescind or alter the conditional use permit then in effect and by doing so direct the City Clerk to take any necessary steps to rescind or alter the covenants or other pertinent documents concerning the conditional use and attached to the property deed held in the office of the County Register of Deeds.
- c. Any conditional use that moves from the premises on which it was originally permitted or that ceases to occupy such premises for a period of three months or as evidenced by legal, authoritative documentation, shall be declared to be no longer a legitimate use of the premises and shall be removed from the list of current conditional uses.

**CITY OF PEWAUKEE
COMMON COUNCIL AGENDA ITEM 13.**

DATE: January 15, 2024

DEPARTMENT: Planning

PROVIDED BY: Nick Fuchs

SUBJECT:

Discussion and Possible Action Regarding **Ordinance 24-04** Revising and Codifying Chapter 17 Zoning as Chapter 340 of the City's Municipal Code [Fuchs]

BACKGROUND:

At their December 21, 2023, meeting, the Plan Commission unanimously recommended approval of the subject revisions and codification of the zoning code.

FINANCIAL IMPACT:

RECOMMENDED MOTION:

A motion to approve revisions and codification of Chapter 17 Zoning as Chapter 340 of the City's Municipal Code.

ATTACHMENTS:

Description

Zoning code adoption staff report

Ordinance 24-04

Zoning code ordinance schedule A



Office of the Planner & Community Development Director
W240 N3065 Pewaukee Road
Pewaukee, Wisconsin 53072
Phone (262) 691-0770
fuchs@pewaukee.wi.us

Fax (262) 691-1798

REPORT TO THE PLAN COMMISSION

Meeting of December 21, 2023

Date: December 6, 2023

Project Name: Amendment to Public Hearing Requirements for Conditional Use Permits

Project Address/Tax Key No.: City-wide

Applicant: City of Pewaukee

Background

The City has been working with a company, General Code, to review and update the entire Municipal Code and to provide a digital version of the code through their eCode360 platform.

As part of that review, many changes were made to the Zoning Code. These are generally technical changes, legal updates, or clarifications that were needed in the ordinance. The attached Schedule A outlines these changes.

It is anticipated that the codification and adoption of the entire Municipal Code will be on the January 15, 2024, Common Council agenda. Amendments to the Zoning Code, however, require a public hearing to be held by the Plan Commission, prior to review and adoption by the Common Council. As such, just the Zoning Code changes are coming before the Plan Commission for review.

Note that the Municipal Code will be re-numbered as part of this project. Chapter 17 Zoning will become Chapter 340 Zoning in the updated ordinance.

Recommendation

It is recommended that the Plan Commission recommend approval of the revisions to the Zoning Code.

ORDINANCE 24-04**AN ORDINANCE ADOPTING A REVISION AND CODIFICATION OF THE ORDINANCES
OF THE CITY OF PEWAUKEE, WISCONSIN**

The Common Council of the City of Pewaukee does hereby ordain as follows:

§ 1-5. Code adopted; existing ordinances continued.

Pursuant to § 66.0103, Wis. Stats., the Zoning Code of the City of Pewaukee of a general and permanent nature adopted by the Common Council of the City of Pewaukee, as revised and codified as Chapter 340, are hereby approved, adopted, ordained and enacted as the Code of the City of Pewaukee, hereinafter referred to as the "Code." The provisions of the Code, insofar as they are substantively the same as those of the ordinances in force immediately prior to the enactment of the Code by this ordinance, are intended as a continuation of such ordinances and not as new enactments. This ordinance and the Code adopted hereby shall supersede and replace the prior Code of Ordinances, as amended and supplemented.

§ 1-6. Code on file; additions and amendments.

- A. In accordance with § 66.0103, Wis. Stats., a copy of the Code has been filed in the office of the City Clerk-Treasurer and made available for public inspection for not less than two weeks prior to adoption of this ordinance, and following adoption of this ordinance such copy shall be maintained and available for public inspection in the office of the City Clerk-Treasurer.
- B. Additions or amendments to the Code, when adopted in such form as to indicate the intent of the Common Council to make them a part thereof, shall be deemed to be incorporated into such Code so that reference to the "Zoning Code of the City of Pewaukee" shall be understood and intended to include such additions and amendments.

§ 1-7. Notice; publication.

The Clerk-Treasurer of the City of Pewaukee shall cause notice of the passage of this ordinance to be given in the manner required by law. The notice of passage of this ordinance coupled with the filing of the Code in the office of the City Clerk-Treasurer as provided in § 1-6 shall be deemed, held and considered to be due and legal publication of all provisions of the Code for all purposes.

§ 1-8. Severability.

Each section of this ordinance and of the Code and every part of each section is an independent section or part of a section, and the holding of any section or a part thereof to be unconstitutional, void or ineffective for any cause shall not be deemed to affect the validity or constitutionality of any other sections or parts thereof. If any provision of this Code or the application thereof to any person or circumstances is held invalid, the remainder of this Code and the application of such provision to other persons or circumstances shall not be affected thereby.

§ 1-9. Repealer.

- A. Ordinances or parts of ordinances inconsistent with the provisions contained in the Code adopted by this ordinance are hereby repealed; provided, however, that such repeal shall only be to the extent of such inconsistency, and any valid legislation of the City of Pewaukee which is not in conflict with the provisions of the Code shall be deemed to remain in full force and effect.

§ 1-10. Ordinances saved from repeal.

The adoption of this Code and the repeal of ordinances provided for in § 1-9 of this ordinance shall not affect the following ordinances, rights and obligations, which are hereby expressly saved from repeal:

- A. Any ordinance adopted subsequent to January 15, 2024.
- B. Any right or liability established, accrued or incurred under any legislative provision prior to the effective date of this ordinance or any action or proceeding brought for the enforcement of such right or liability.
- C. Any offense or act committed or done before the effective date of this ordinance in violation of any legislative provision or any penalty, punishment or forfeiture which may result therefrom.
- D. Any prosecution, indictment, action, suit or other proceeding pending or any judgment rendered prior to the effective date of this ordinance brought pursuant to any legislative provision.
- E. Any franchise, license, right, easement or privilege heretofore granted or conferred.
- F. Any ordinance providing for the laying out, opening, altering, widening, relocating, straightening, establishing grade, naming, improvement, acceptance or vacation of any right-of-way, easement, street, road, highway, park or other public place or any portion thereof.
- G. Ordinances authorizing the purchase, sale, lease or transfer of property or any lawful contract or obligation.
- H. The annexation or dedication of property or approval of preliminary or final subdivision plats.
- I. Any ordinances adopting or amending the Zoning Map or otherwise rezoning property.
- J. Any charter ordinances.
- K. Any ordinance or portion of an ordinance establishing or amending a specific fee amount for any license, permit or service obtained from the City.

§ 1-11. Changes in previously adopted ordinances; new ordinances.

- A. In preparing the revision and codification of the ordinances, certain minor grammatical and nonsubstantive changes were made in one or more of said ordinances, and references to state statutes

and regulations were updated to reflect the numbering and titles of the statutes and regulations as of the publication of this Code. It is the intention of the Common Council that all such changes be adopted as part of the Code as if the ordinances so changed had been formally amended to read as such.

- B. In accordance with § 66.0103, Wis. Stats., the Code of the City of Pewaukee, on file in the office of the City Clerk-Treasurer, is adopted and incorporated by reference, including the revisions set forth in Schedule A attached hereto and made a part hereof. (Chapter and section number references in Schedule A are to the ordinances as they have been renumbered and appear in the Code.)

§ 1-12. Titles and headings; editor's notes.

- A. Chapter and article titles, headings and titles of sections and other divisions of the Code are inserted in the Code and may be inserted in supplements to the Code for the convenience of persons using the Code and are not part of the legislation.
- B. Editor's notes indicating sources of sections, giving other information or referring to the statutes or to other parts of the Code are inserted in the Code and may be inserted in supplements to the Code for the convenience of persons using the Code and are not part of the legislation.

§ 1-13. Altering or tampering with Code; violations and penalties.

It shall be unlawful for anyone to change, alter or tamper with the Code in any manner which will cause the laws of the City of Pewaukee to be misrepresented thereby. Anyone violating this section shall be subject, upon conviction, to a penalty as provided in § 1-4 of the Code.

§ 1-14. When effective.

This ordinance shall take effect upon final passage and publication or posting as provided by law.

Passed and adopted at a regular meeting of the Common Council of the City of Pewaukee this 15th Day of January, 2024.

CITY OF PEWAUKEE

Attest:

Steve Bierce, Mayor

Kelly Tarczewski, Clerk/Treasurer

Ch. 340, Zoning.

Section 340-1.4A is amended to delete "Housing Code" from the last sentence.

Section 340-2.6 is amended to change "occupancy/use permit" to "certificate of occupancy."

Section 340-2.9B(2)(c)[2] is amended to change "ornamental" to "decorative."

Section 340-3.2A is amended to change "shall be required to be paid upon application for a building permit" to "shall be required to be paid at the time of the issuance of a building permit."

Section 340-3.2C is amended to add the following: "At the time that the City collects an impact fee, it shall provide to the developer from which it received the fee an accounting of how the fee will be spent."

Section 340-4.1A is amended to change "Park and Recreation District" to "Park and Open Space District" and to add the HPO Historic Preservation Overlay District.

Section 340-4.1C is amended to change the map date from November 19, 2008, to November 5, 2014.

Section 340-4.3 is amended to change "City Land Use Plan" to "City Comprehensive Plan" at the end of the first sentence.

Sections 340-4.5A(3), 340-4.6A(3), 340-4.7A(3), 340-4.8A(3), 340-4.9A(3), 340-4.10A(3) and 340-4.34A(7) are amended to change "family day-care home" to "family child care home."

Section 340-4.19C(14) is amended to change "Bed-and-breakfast inns" to "Bed-and-breakfast establishments."

Section 340-4.29 is amended to change "Mixed Industrial Use District" to "Mixed-Use Industrial District."

Section 340-4.36 is amended to delete "attached hereto as Appendix A" from the end of the opening paragraph.

Section 340-9.13G is amended to change "Electrical Inspector" to "Building Inspector."

Section 340-10.2B is added to read as follows:

Pursuant to § 62.23(7)(hc), Wis. Stats., and notwithstanding any other provision of this chapter, a nonconforming structure damaged or destroyed by violent wind, vandalism, fire, flood, ice, snow, mold or infestation after March 2, 2006, may be restored to, or replaced at, the size, location, and use that it had immediately before the damage or destruction occurred, and no limits may be imposed on the costs of the repair, reconstruction, or improvement of said structure. The size of the structure may be larger than the size it was immediately before the damage or destruction if necessary for the structure to comply with applicable state or federal requirements.

Section 340-12.3D is amended to read as follows: "If a quorum is present, a majority vote of the members present shall be necessary to correct an error; grant a variance; make an interpretation; and permit a utility, temporary, unclassified, or substituted use."

Section 340-12.4B is amended as follows: "To hear and grant appeals for area and use variances, as defined below, as will not be contrary to the public interest, when, owing to special conditions, a literal enforcement will ~~results:~~ result in practical difficulty or unnecessary physical hardship, so that the spirit and purposes of this chapter shall be observed and the public safety, welfare, and justice secured."

Section 340-12.9C and D are amended to add "The Board shall" at the beginning of the subsection.

Section 340-13.7A is amended to change the map date from November 19, 2008, to November 5, 2014.

The definition of "bed-and-breakfast establishment" in § 340-16.2G is amended to read as follows:

Any place of lodging that satisfies all of the following: provides eight or fewer rooms for rent to no more than a total of 20 tourists or transients; provides no meals other than breakfast and provides the breakfast only to renters of the place; is the owner's personal residence; is occupied by the owner at the time of rental; and was originally built and occupied as a single-family residence or, prior to use as a place of lodging, was converted to use and occupied as a single-family residence.

The definition of "occupancy permit" in § 340-16.2G is amended to "certificate of occupancy" and to change "required permit" to "required certificate."

The definition of "community living arrangement" in § 340-16.2G is amended to change "group foster homes for children under § 48.02(1m) to "group homes for children under § 48.02(7)."

The definition of "dwelling, multiple-family" in § 340-16.2G is amended to read as follows: "A building containing three or more separate residential, dwelling or living units designed for or occupied by three or more families, with the number of families in residence not to exceed the number of dwelling units provided."

The definition of "family" in § 340-16.2G is amended to read as follows: "One or more persons occupying a dwelling unit as a single nonprofit housekeeping unit, who are living together as a bona fide, stable and committed living unit, being a traditional family unit or the functional equivalent thereof, exhibiting the generic character of a traditional family."

The definition of "fence, ornamental" in § 340-16.2G is amended to change "ornamental" to "decorative."

The definition of "fence, privacy" in § 340-16.2G is amended to change "ornamental fence" to "decorative fence."

The definition of "group foster home" in § 340-16.2G is repealed.

The definition of "multiple-family dwelling" in § 340-16.2G is amended to read: See "dwelling, multiple-family."

The definition of "side setback" in § 340-16.2G is amended as follows: "The area prescribed to as the minimum side yard setback distance between the principal or ~~access~~ accessory structure and the side property boundary as set forth in the ~~distant~~ district regulations."

The definition of "two-family dwelling" in § 340-16.2G is amended to read: See "dwelling, two-family."

The definition of "variance" in § 340-16.2G is amended to read as follows:

Permission to depart from the literal requirements of this chapter. An area variance authorizes modification to a dimensional, physical, or locational requirement such as a setback, frontage, height, bulk or density restriction for a structure. A use variance authorizes the use of land for a purpose that is otherwise not allowed or is prohibited by the applicable zoning ordinance.

**CITY OF PEWAUKEE
COMMON COUNCIL AGENDA ITEM 14.**

DATE: January 15, 2024

DEPARTMENT: PW - Engineering

PROVIDED BY: Magdelene Wagner

SUBJECT:

Discussion and Possible Action to Adopt Revised Relocation **Resolution 24-01-04** Busse Road Bridge Replacement [Wagner].

BACKGROUND:

The Common Council previously adopted a relocation resolution 22.02.03. However securing the State funding for the Busse Road Bridge Replacement project, we need to revise the language on the plat to meet the State requirements. As such, we have to adopt a new relocation resolution with the new plat prior to final acquisition.

FINANCIAL IMPACT:

RECOMMENDED MOTION:

Council approve the relocation resolution.

ATTACHMENTS:

Description

Plat (Exhibit A-1)

Resolution 24-01-04

TRANSPORTATION PROJECT PLAT NO: 2717-03-02 - 4.01

THAT PART OF THE LOT 1, IN BLOCK 1, IN SHERWOOD FOREST SUBDIVISION, PART OF THE NORTHEAST QUARTER (1/4) OF THE NORTHWEST QUARTER (1/4) OF SECTION 26, IN TOWNSHIP 7 NORTH, RANGE 19, EAST IN THE TOWN OF PEWAUKEE, WAUKESHA COUNTY, WISCONSIN.

AND

THAT PART OF THE SW (1/4) AND THE SE (1/4) OF SECTION 23, IN TOWNSHIP 7 NORTH, RANGE 19 EAST, IN THE TOWN OF PEWAUKEE, WAUKESHA COUNTY, WISCONSIN.

RELOCATION ORDER BUSSE ROAD FROM PROPOSED STATION 0+00 TO 7+00 WAUKESHA COUNTY

TO PROPERLY ESTABLISH, LAY OUT, WIDEN, ENLARGE, EXTEND, CONSTRUCT, RECONSTRUCT, IMPROVE, OR MAINTAIN A PORTION OF THE HIGHWAY DESIGNATED ABOVE, THE STATE OF WISCONSIN DEPARTMENT OF TRANSPORTATION DEEMS IT NECESSARY TO RELOCATE OR CHANGE SAID HIGHWAY AND ACQUIRE CERTAIN LANDS AND INTERESTS OR RIGHTS IN LANDS FOR THE ABOVE PROJECT.

TO EFFECT THIS CHANGE, PURSUANT TO AUTHORITY GRANTED UNDER SECTION 62.22 AND 32.05, WISCONSIN STATUTES, THE CITY OF PEWAUKEE HEREBY ORDERS THAT:

1. THAT PORTION OF SAID HIGHWAY AS SHOWN ON THIS PLAT IS LAID OUT AND ESTABLISHED TO THE LINES AND WIDTHS AS SO SHOWN FOR THE ABOVE PROJECT.
2. THE LANDS OR INTERESTS OR RIGHTS IN LANDS AS SHOWN ON THIS PLAT ARE REQUIRED BY THE CITY FOR THE ABOVE PROJECT AND SHALL BE ACQUIRED IN THE NAME OF THE CITY OF PEWAUKEE, PURSUANT TO THE PROVISIONS OF SECTION 62.22, WISCONSIN STATUTES.

NOTES:

POSITIONS SHOWN ON THIS PLAT ARE WISCONSIN STATE PLANE COORDINATE SYSTEM, SOUTH ZONE, NORTH AMERICAN DATUM OF 1927, IN U.S. SURVEY FEET. VALUES ARE GRID COORDINATES, GRID BEARINGS, AND GRID DISTANCES. GRID DISTANCES MAY BE USED AS GROUND DISTANCES.

ALL NEW RIGHT-OF-WAY MONUMENTS WILL BE TYPE 2 (TYPICALLY ¾" X 24" IRON REBARS), UNLESS OTHERWISE NOTED, AND WILL BE PLACED PRIOR TO THE COMPLETION OF THE PROJECT.

ALL RIGHT-OF-WAY LINES DEPICTED IN THE NON-ACQUISITION AREAS ARE INTENDED TO RE-ESTABLISH EXISTING RIGHT-OF-WAY LINES AS DETERMINED FROM PREVIOUS PROJECTS, OTHER RECORDED DOCUMENTS, CENTERLINE OF EXISTING PAVEMENTS AND/OR EXISTING OCCUPATIONAL LINES.

RIGHT-OF-WAY BOUNDARIES ARE DEFINED WITH COURSES OF THE PERIMETER OF THE HIGHWAY LANDS REFERENCED TO THE U.S. PUBLIC LAND SURVEY SYSTEM OR OTHER "SURVEYS" OF PUBLIC RECORD.

DIMENSIONING FOR THE NEW RIGHT-OF-WAY IS MEASURED ALONG AND PERPENDICULAR TO THE NEW REFERENCE LINES.

A TEMPORARY LIMITED EASEMENT (TLE) IS A RIGHT FOR CONSTRUCTION PURPOSES, AS DEFINED HEREIN, INCLUDING THE RIGHT TO OPERATE NECESSARY EQUIPMENT THEREON, THE RIGHT OF INGRESS AND EGRESS, AS LONG AS REQUIRED FOR SUCH PUBLIC PURPOSE, INCLUDING THE RIGHT TO PRESERVE, PROTECT, REMOVE, OR PLANT THEREON ANY VEGETATION THAT THE HIGHWAY AUTHORITIES MAY DEEM DESIRABLE. ALL (TLEs) ON THIS PLAT EXPIRE AT THE COMPLETION OF THE CONSTRUCTION PROJECT FOR WHICH THIS INSTRUMENT IS GIVEN.

PROPERTY LINES SHOWN ON THIS PLAT FOR PROPERTIES BEING IMPACTED ARE DRAWN FROM DATA DERIVED FROM FILED/RECORDED MAPS AND DOCUMENTS OF PUBLIC RECORD. THIS PLAT MAY NOT BE A TRUE REPRESENTATION OF EXISTING PROPERTY LINES, EXCLUDING RIGHT-OF-WAY, AND SHOULD NOT BE USED AS A SUBSTITUTE FOR AN ACCURATE FIELD SURVEY.

FOR THE CURRENT ACCESS/DRIVEWAY INFORMATION, CONTACT THE PLANNING UNIT OF THE WISCONSIN DEPARTMENT OF TRANSPORTATION OFFICE IN WAUKESHA.

PARCEL AND UTILITY IDENTIFICATION NUMBERS MAY NOT POINT TO ALL AREAS OF ACQUISITION, AS NOTED ON THE TPP DETAIL PAGES.

FOUND MONUMENT TABLE			
POINT NUMBER	X	Y	DESCRIPTION
101	2476954.970	386600.510	SEWRPC BRASS CAP

FOUND MONUMENT INFORMATION SHOWN REPRESENTS TYPE AND LOCATION OF EXISTING MONUMENTS WITHOUT OPINION AS TO THEIR VALIDITY AND USE AS A PROPERTY CORNER.

UTILITY INTERESTS REQUIRED		
UTILITY NUMBER	OWNER(S)	INTEREST REQUIRED
11	CITY OF PEWAUKEE	Y
12	WE ENERGIES	Y
13	WE ENERGIES	Y
14	AT&T	Y
15	CHARTER	Y

CONVENTIONAL ABBREVIATIONS

ACCESS RIGHTS	AR	POINT OF COMPOUND CURVE	PCC
ACRES	AC	POINT OF INTERSECTION	PI
AHEAD	AH	PROPERTY LINE	PL
ALUMINUM	ALUM	RECORDED AS	(100')
AND OTHERS	ET AL	REEL / IMAGE	R/I
BACK	BK	REFERENCE LINE	R/L
BLOCK	BLK	REMAINING	REM
CENTERLINE	C/L	RESTRICTIVE DEVELOPMENT	RDE
CERTIFIED SURVEY MAP	CSM	EASEMENT	
CONCRETE	CONC	RIGHT	RT
COUNTY	CO	RIGHT OF WAY	R/W
COUNTY TRUNK HIGHWAY	CTH	SECTION	SEC
DISTANCE	DIST	SEPTIC VENT	SEPV
CORNER	COR	SQUARE FEET	SF
DOCUMENT NUMBER	DOC	STATE TRUNK HIGHWAY	STH
EASEMENT	EASE	STATION	STA
EXISTING	EX	TELEPHONE PEDESTAL	TP
GAS VALVE	GV	TEMPORARY LIMITED	TLE
GRID NORTH	GN	EASEMENT	
HIGHWAY EASEMENT	HE	TRANSPORTATION PROJECT PLAT	TPP
IDENTIFICATION	ID	UNITED STATES HIGHWAY	USH
LAND CONTRACT	LC	VOLUME	V
LEFT	LT		
MONUMENT	MON		
NATIONAL GEODETIC SURVEY	NGS		
NUMBER	NO		
OUTLOT	OL		
PAGE	P		
POINT OF TANGENCY	PT		
PERMANENT LIMITED	PLE		
EASEMENT			
POINT OF BEGINNING	POB		
POINT OF CURVATURE	PC		

CURVE DATA ABBREVIATIONS

LONG CHORD	LCH
LONG CHORD BEARING	LCB
RADIUS	R
DEGREE OF CURVE	D
CENTRAL ANGLE	Δ/DELTA
LENGTH OF CURVE	L
TANGENT	T
DIRECTION AHEAD	DA
DIRECTION BACK	DB

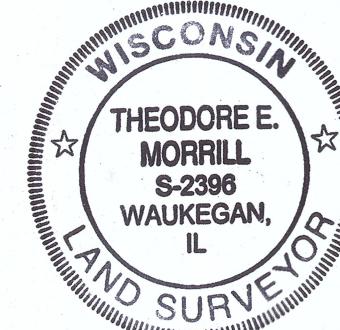
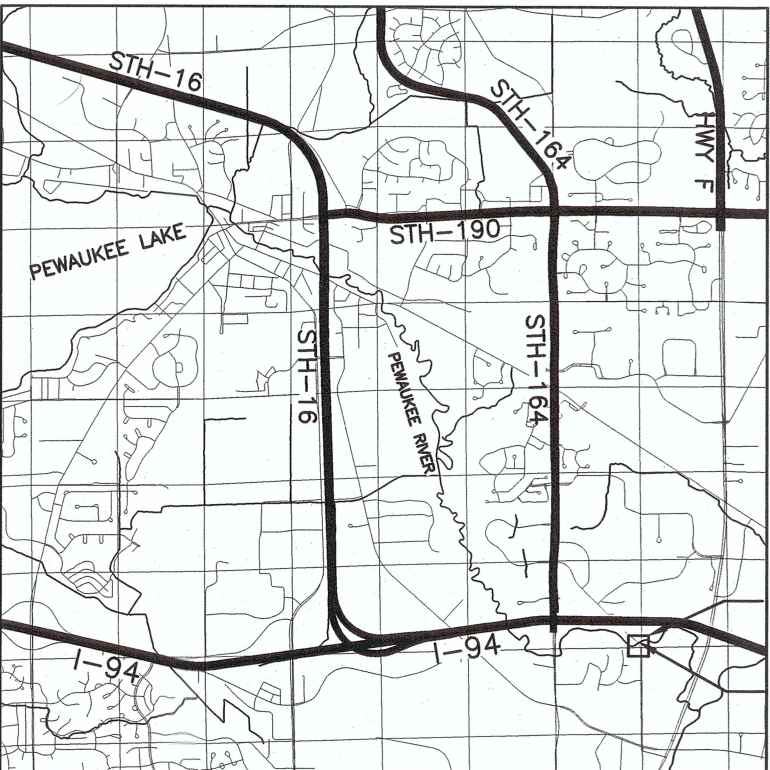
CONVENTIONAL SYMBOLS

SECTION LINE	---	SECTION CORNER SYMBOL		R/W MONUMENT (TO BE SET)	●
QUARTER LINE	---	SECTION CORNER MONUMENT		NON-MONUMENTED R/W POINT	○
SIXTEENTH LINE	---	GEODETIC SURVEY MONUMENT		FOUND IRON PIN (1-INCH UNLESS NOTED)	IP
NEW REFERENCE LINE	---	SIXTEENTH CORNER MONUMENT			
NEW R/W LINE	---	SIGN		OFF-PREMISE SIGN	
EXISTING R/W OR HE LINE	---			COMPENSABLE	
PROPERTY LINE	---			NON-COMPENSABLE	
LOT, TIE & OTHER MINOR LINES	---				
SLOPE INTERCEPT	---				
CORPORATE LIMITS	---				
UNDERGROUND FACILITY (COMMUNICATIONS, ELECTRIC, ETC.)	---				
NEW R/W (FEE OR HE) (HATCHING VARIES BY OWNER)	---				
TEMPORARY LIMITED EASEMENT AREA	---				
EASEMENT AREA (PERMANENT LIMITED OR RESTRICTED DEVELOPMENT)	---				
TRANSMISSION STRUCTURES	---				
BUILDING		TO BE REMOVED			
BRIDGE		CULVERT			

CONVENTIONAL UTILITY SYMBOLS

WATER	---
GAS	---
TELEPHONE	---
OVERHEAD TRANSMISSION LINES	---
ELECTRIC	---
CABLE TELEVISION	---
FIBER OPTIC	---
SANITARY SEWER	---
STORM SEWER	---
ELECTRIC TOWER	---

NOTES:
1. BENCHMARK: WISCONSIN CSDS MONUMENT S ¼ CORNER BEARINGS PER WISCONSIN STATE PLANE NAD 1927 (TO BE PRESERVED)
ELEVATION: 838.92



I, THEODORE E. MORRILL, PROFESSIONAL LAND SURVEYOR, HEREBY CERTIFY THAT IN FULL COMPLIANCE WITH THE PROVISIONS OF SECTION 84.095 OF THE WISCONSIN STATUTES AND UNDER THE DIRECTION OF: CITY OF PEWAUKEE, I HAVE SURVEYED AND MAPPED THIS TRANSPORTATION PROJECT PLAT AND SUCH PLAT CORRECTLY REPRESENTS ALL EXTERIOR BOUNDARIES OF THE SURVEYED LAND.

SIGNATURE: *Theodore E. Morrill* DATE: 01/10/2024

PRINT NAME: THEODORE E. MORRILL
REGISTRATION NUMBER: S-2396

THIS PLAT AND RELOCATION ORDER ARE APPROVED FOR THE CITY OF PEWAUKEE

SIGNATURE: _____ DATE: _____
PRINT NAME: _____

RESOLUTION #24-01-04**RELOCATION ORDER (REVISED) OF THE CITY OF PEWAUKEE
WAUKESHA COUNTY, WISCONSIN
(Busse Road Bridge Replacement)**

NOW COMES the City of Pewaukee, Waukesha County, Wisconsin, by its Common Council and for its Relocation Order hereby resolves as follows:

1. That this Resolution is a Relocation Order in accordance with §32.05(1), Wisconsin Statutes, for the purposes of the within described public improvement project and it is also a determination of necessity for that project in accordance with §32.07(2), Wisconsin Statutes.
2. The City of Pewaukee previously adopted Relocation **Resolution 22-02-03** which is being replaced with this resolution.
3. That the City of Pewaukee hereby determines that it is a necessary and public purpose to construct public improvements as shown on the maps attached hereto, made a part hereof and marked Exhibit "A-1".
4. That said public improvements (bridge replacement) will be built as set forth on the maps which are annexed to this Relocation Order as Exhibits "A-1" and are incorporated herein.
5. That the sites of said public improvements (bridge replacement) are contained in Exhibits "A-1" which are incorporated herein; that the legal descriptions to said site are attached hereto, made a part hereof and marked Exhibits "A-1".
6. That the City of Pewaukee will acquire permanent easements or right of way for public improvements (bridge replacement) as indicated on Exhibits "A-1".

Passed and approved this 15th day of January 2024.

I hereby certify that on this 15th day of January 2024, the Relocation Order was adopted by a vote of ____ ayes to ____ nays by the Common Council of the City of Pewaukee, Waukesha County, Wisconsin.

APPROVED: CITY OF PEWAUKEE

Steven Bierce, Mayor

ATTEST:

Kelly Tarczewski, Clerk/Treasurer

**CITY OF PEWAUKEE
COMMON COUNCIL AGENDA ITEM 15.**

DATE: January 15, 2024

DEPARTMENT: PW - Water/Sewer

PROVIDED BY: Magdelene Wagner

SUBJECT:

Discussion and Possible Action Regarding the Gun Club Replacement Project Temporary Easement [Wagner].

BACKGROUND:

This is a temporary grading easement required to build the Gun Club Replacement lift station. The Council authorized the easement acquisition at their July 17, 2023 meeting. The property owner is allowing the temporary easement without cost to the Utility.

Therefore, we are just seeking approval of the temporary easement for execution and recording.

FINANCIAL IMPACT:

RECOMMENDED MOTION:

Council approve the temporary easement acquisition.

ATTACHMENTS:

Description

Easement Agreement

TEMPORARY LIMITED EASEMENT

Exempt from fee: s. 77.25(2r) Wis. Stats.
Exempt from filing transfer form [s. 77.21(1), 77.22(1) Wis. Stats.]
RE1577 01/2023

THIS EASEMENT, made by **Pilgrim Parkway 96 LLC**, GRANTOR, conveys a temporary limited easement as described below to the **City of Pewaukee**, GRANTEE, for the sum of **One Dollar and Other Valuable Considerations (\$1.00)** for the purpose of grading and as described on the attached legal description.

Any person named in this conveyance may make an appeal from the amount of compensation within six months after the date of recording of this conveyance as set forth in s. 32.05(2a) Wisconsin Statutes. For the purpose of any such appeal, the amount of compensation stated on the conveyance shall be treated as the award, and the date the conveyance is recorded shall be treated as the date of taking and the date of evaluation.


Other persons having an interest of record in the property:

LEGAL DESCRIPTION IS ATTACHED AND MADE A PART OF THIS DOCUMENT BY REFERENCE.

This space is reserved for recording data

Return to
City of Pewaukee
Department of Public Works
W240 N3065 Pewaukee Road
Pewaukee, WI 53072

Parcel Identification Number/Tax Key Number
PWC 0953990


Signature/Acknowledgement by: Julie Ellenbecker-Lipsky Date _____
Print Name and Title: Pilgrim Parkway 96 LLC

Signature _____ Date _____
Print Name and Title _____

_____ Date

State of Wisconsin)
County) ss.

On the above date, this instrument was acknowledged before me by the named person(s).

The signer was:
_____ Physically in my presence. OR
_____ In my presence involving the use of communication technology.

Signature, Notary Public, State of Wisconsin

Print or Type Name, Notary Public, State of Wisconsin

Date Commission Expires

Project ID: NA Contract: 2-2023 Gun Club Pumping Station Replacement	This instrument was drafted by Jim McCarthy of Strand Associates, Inc.® for the City of Pewaukee	Parcel No. Pilgrim Parkway 96 LLC TLE
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**CITY OF PEWAUKEE
COMMON COUNCIL AGENDA ITEM 16.**

DATE: January 15, 2024

DEPARTMENT: Clerk/Treasurer

PROVIDED BY:

SUBJECT:

Discussion and Possible Action Regarding the Accounts Payable Listing Dated January 15, 2024 [Tarczewski]

BACKGROUND:

FINANCIAL IMPACT:

RECOMMENDED MOTION:

ATTACHMENTS:

Description

A/P 1./15/2024

01/11/2024 01:21 PM
User: OMAN
DB: City Of Pewaukee

CHECK DISBURSEMENT REPORT FOR PEWAUKEE
CHECK DATE FROM 12/16/2023 - 01/11/2024

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Check Date	Bank	Check #	Payee	Description	Account	Dept	Amount
Fund: 100 GENERAL FUND							
12/18/2023	100	1478 (E) #	LEASING SERVICES	COURT - OPERATING SUPPLIES	53400	51200	52.00
				POLICE - OPERATING SUPPLIES	53400	52100	52.00
				CHECK 100 1478 (E) TOTAL FOR FUND 100:			104.00
12/19/2023	100	1459 (E)	WI DEPT OF REVENUE	CLERK/TREASURER - OPERATING SUPPLIES	53400	51420	10.00
12/19/2023	100	1473 (E)	STATE OF WI ETF	HEALTH INSURANCE CLEARING	21901	00000	189,016.52
12/20/2023	100	1421 (E)	WISCONSIN RETIREMENT SYSTEM	WRS GENERAL EMPLOYEES			** VOIDED **
				WRS FIRE EMPLOYEES			** VOIDED **
12/20/2023	100	1423 (E)	WISCONSIN RETIREMENT SYSTEM	WRS GENERAL EMPLOYEES	21520	00000	38,197.44
				WRS FIRE EMPLOYEES	21522	00000	53,087.84
				WRS VOLUNTARY CONTRIBUTIONS	21523	00000	178.50
				CHECK 100 1423 (E) TOTAL FOR FUND 100:			91,463.78
12/20/2023	100	1457 (E)	DELTA DENTAL	DENTAL CLEARING	21903	00000	2,289.60
12/21/2023	100	139567	AARON & AMANDA BUSCHE	DEFERRED REVENUES - ADVANCE TAX	26310	00000	181.58
12/21/2023	100	139569	ALAN AND LINDA ANTONIEWICZ	DEFERRED REVENUES - ADVANCE TAX	26310	00000	203.41
12/21/2023	100	139570	AMERINGER TRUST	DEFERRED REVENUES - ADVANCE TAX	26310	00000	120.30
12/21/2023	100	139571	ANDREA FLORIN	DEFERRED REVENUES - ADVANCE TAX	26310	00000	95.37
12/21/2023	100	139572	ANDREW & HEATHER MELVIN	DEFERRED REVENUES - ADVANCE TAX	26310	00000	868.55
12/21/2023	100	139573	ANDREW & JILL JUNG	DEFERRED REVENUES - ADVANCE TAX	26310	00000	359.22
12/21/2023	100	139574	ANDREW & KATE GILSON	DEFERRED REVENUES - ADVANCE TAX	26310	00000	244.29
12/21/2023	100	139575	ANTHONY & MOLLY DIBRITO	DEFERRED REVENUES - ADVANCE TAX	26310	00000	183.08
12/21/2023	100	139576	ASSESSMENTS USA	FIRE ADMINISTRATION - EMPLOYMENT	52150	52210	897.00
12/21/2023	100	139577	BALAJI VENKATES & NISHA	DEFERRED REVENUES - ADVANCE TAX	26310	00000	226.14
12/21/2023	100	139578	BARBARA HOWDER	DEFERRED REVENUES - ADVANCE TAX	26310	00000	88.27
12/21/2023	100	139579	BENJAMIN & DENISE HOWDER	DEFERRED REVENUES - ADVANCE TAX	26310	00000	236.04

Check Date	Bank	Check #	Payee	Description	Account	Dept	Amount
Fund: 100 GENERAL FUND							
12/21/2023	100	139580	BRADY & JADILYN ENDL	DEFERRED REVENUES - ADVANCE TAX	26310	00000	495.91
12/21/2023	100	139581	BRIAN & PATRICIA DZIWULSKI	DEFERRED REVENUES - ADVANCE TAX	26310	00000	108.71
12/21/2023	100	139582	CHRISTOPHER UTZ	DEFERRED REVENUES - ADVANCE TAX	26310	00000	106.75
12/21/2023	100	139583	CONCENTRA MED COMPLIANCE ADMIN	EMPLOYEE SERVICES - EMPLOYMENT EXAMS	52150	51430	110.00
12/21/2023	100	139584	COSTA PROPERTY MAINT LLC	FORESTRY - TREE REMOVAL	53410	56110	4,925.00
12/21/2023	100	139585	CRAIG HALPERT REVOCABLE TRUST	DEFERRED REVENUES - ADVANCE TAX	26310	00000	573.80
12/21/2023	100	139586	DEAN & KAREN NATTERSTAD	DEFERRED REVENUES - ADVANCE TAX	26310	00000	183.23
12/21/2023	100	139587	DIANE DRUMMY	DEFERRED REVENUES - ADVANCE TAX	26310	00000	495.25
12/21/2023	100	139588	DIANE L MCGROARTY	DEFERRED REVENUES - ADVANCE TAX	26310	00000	57.01
12/21/2023	100	139589	ERIC SIPE	DEFERRED REVENUES - ADVANCE TAX	26310	00000	130.74
12/21/2023	100	139590#	FORWARD TS	COURT - OPERATING SUPPLIES	53400	51200	15.26
				POLICE - OPERATING SUPPLIES	53400	52100	15.26
				CHECK 100 139590 TOTAL FOR FUND 100:			30.52
12/21/2023	100	139591	FREDERICK & CHRISTIE BULACAN	DEFERRED REVENUES - ADVANCE TAX	26310	00000	245.62
12/21/2023	100	139592	FROEDTERT HEALTH /	EMPLOYEE SERVICES - EMPLOYEE WELLNESS	52700	51430	227.50
12/21/2023	100	139593	GAVIN MILLAR & KINGA NAGY	DEFERRED REVENUES - ADVANCE TAX	26310	00000	246.84
12/21/2023	100	139594	GERLAD & CAROLYN SCHROEDER	DEFERRED REVENUES - ADVANCE TAX	26310	00000	84.31
12/21/2023	100	139595	GILBERT MIKULA	DEFERRED REVENUES - ADVANCE TAX	26310	00000	110.80
12/21/2023	100	139596	GREG & JULIE BLUNT	DEFERRED REVENUES - ADVANCE TAX	26310	00000	197.30
12/21/2023	100	139597	HECKEL DANCE LLC	RECREATION PROGRAM - CONTRACTED	52190	55300	816.00
12/21/2023	100	139598	HOLZSCHUH REVOCABLE TRUST	DEFERRED REVENUES - ADVANCE TAX	26310	00000	236.80
12/21/2023	100	139599	JACOB & AMY WANNER	DEFERRED REVENUES - ADVANCE TAX	26310	00000	190.09
12/21/2023	100	139600	JAMES & JOSEPHINE AUBUCHON	DEFERRED REVENUES - ADVANCE TAX	26310	00000	100.88

Check Date	Bank	Check #	Payee	Description	Account	Dept	Amount
Fund: 100 GENERAL FUND							
12/21/2023	100	139601	JAMES & LISA HUEBNER	DEFERRED REVENUES - ADVANCE TAX	26310	00000	218.14
12/21/2023	100	139602	JAMES L MORGAN	DEFERRED REVENUES - ADVANCE TAX	26310	00000	437.65
12/21/2023	100	139603	JENNIFER SCHMIDT	DEFERRED REVENUES - ADVANCE TAX	26310	00000	92.85
12/21/2023	100	139604	JEREMY& LESLIE STAVER	DEFERRED REVENUES - ADVANCE TAX	26310	00000	220.25
12/21/2023	100	139605	JILL ANN BUDNIK	DEFERRED REVENUES - ADVANCE TAX	26310	00000	194.40
12/21/2023	100	139606	JOEL & NICOLE REIKOWSKI	DEFERRED REVENUES - ADVANCE TAX	26310	00000	224.28
12/21/2023	100	139607	JOETTA A POLLICH	DEFERRED REVENUES - ADVANCE TAX	26310	00000	159.31
12/21/2023	100	139608	KEITH CHYCINSKI	DEFERRED REVENUES - ADVANCE TAX	26310	00000	18.00
12/21/2023	100	139609	KENNETH & LEAH OLIVER	DEFERRED REVENUES - ADVANCE TAX	26310	00000	397.45
12/21/2023	100	139610	KENNETH HOLTER	DEFERRED REVENUES - ADVANCE TAX	26310	00000	202.46
12/21/2023	100	139611	KWIK TRIP INC.	FIRE PROTECTIVE SERVICES - FUEL	53420	52230	4,455.42
12/21/2023	100	139612	LANALE M. SCHMITT	DEFERRED REVENUES - ADVANCE TAX	26310	00000	130.63
12/21/2023	100	139613	LAURIE A SCHWEIKERT	DEFERRED REVENUES - ADVANCE TAX	26310	00000	834.75
12/21/2023	100	139614	LAWRENCE DUX & KAREN ANDERSON-DUX	DEFERRED REVENUES - ADVANCE TAX	26310	00000	164.51
12/21/2023	100	139615	MARILYN SCHULZ	DEFERRED REVENUES - ADVANCE TAX	26310	00000	93.84
12/21/2023	100	139616	MARK & KELLY PAMPERIN	DEFERRED REVENUES - ADVANCE TAX	26310	00000	218.25
12/21/2023	100	139617	MARY L. TORZALA	DEFERRED REVENUES - ADVANCE TAX	26310	00000	136.99
12/21/2023	100	139618	MARY NESSLAR REVOCABLE TRUST	DEFERRED REVENUES - ADVANCE TAX	26310	00000	252.77
12/21/2023	100	139619	MAY SPAKOWITZ	DEFERRED REVENUES - ADVANCE TAX	26310	00000	40.44
12/21/2023	100	139620	MENARDS	FIRE ADMINISTRATION - OPERATING	53400	52210	30.62
12/21/2023	100	139621	MICHAEL & CYNTHIA CARLSON	DEFERRED REVENUES - ADVANCE TAX	26310	00000	688.48
12/21/2023	100	139622	MICHAEL & MICHELLE GERDES	DEFERRED REVENUES - ADVANCE TAX	26310	00000	189.43
12/21/2023	100	139623	MICHAEL & STEPHANIE CIECIWA	DEFERRED REVENUES - ADVANCE TAX	26310	00000	149.13

Check Date	Bank	Check #	Payee	Description	Account	Dept	Amount
Fund: 100 GENERAL FUND							
12/21/2023	100	139624	MICHAEL GRANDINETTI	DEFERRED REVENUES - ADVANCE TAX	26310	00000	75.82
12/21/2023	100	139625	MICHAEL V RUANE	DEFERRED REVENUES - ADVANCE TAX	26310	00000	210.95
12/21/2023	100	139626	NANCY LINCK	DEFERRED REVENUES - ADVANCE TAX	26310	00000	62.52
12/21/2023	100	139627	NATYNSKI REVOCABLE TRUST	DEFERRED REVENUES - ADVANCE TAX	26310	00000	187.03
12/21/2023	100	139628	ODP BUSINESS SOLUTIONS LLC	BUILDING SERVICES - OPERATING SUPPLIES	53400	52400	66.80
12/21/2023	100	139629	PATRICIA GIBBONS	DEFERRED REVENUES - ADVANCE TAX	26310	00000	104.32
12/21/2023	100	139630	PAUL & SUSAN ASCHENBRENNER	DEFERRED REVENUES - ADVANCE TAX	26310	00000	139.93
12/21/2023	100	139631	PAUL COBURN & SARAH KROHN	DEFERRED REVENUES - ADVANCE TAX	26310	00000	148.39
12/21/2023	100	139632	PETER & JILL OPIE	DEFERRED REVENUES - ADVANCE TAX	26310	00000	87.57
12/21/2023	100	139633	PETER & KAREN KLATT	DEFERRED REVENUES - ADVANCE TAX	26310	00000	97.81
12/21/2023	100	139634	PHILIP & AMY VETTERKIND	DEFERRED REVENUES - ADVANCE TAX	26310	00000	5.41
12/21/2023	100	139635	PHILLIP & RACHEL SPEARO	DEFERRED REVENUES - ADVANCE TAX	26310	00000	146.06
12/21/2023	100	139636	PREMIUM WATERS, INC	COURT - OPERATING SUPPLIES	53400	51200	125.99
12/21/2023	100	139637	REINDERS, INC.	PARKS - NEW EQUIPMENT	53950	55200	371.67
12/21/2023	100	139638	RICHARD TORDIK	DEFERRED REVENUES - ADVANCE TAX	26310	00000	71.09
12/21/2023	100	139639	ROBERT & GWEN SIEHR	DEFERRED REVENUES - ADVANCE TAX	26310	00000	100.31
12/21/2023	100	139640	RUNDLE-SPENCE	CITY HALL - BUILDING REPAIRS & MAINT	52410	51600	416.46
12/21/2023	100	139641	STACI JOERS	RECREATION PROGRAM - CONTRACTED	52190	55300	500.00
12/21/2023	100	139642	STATE OF WI COURT FINES & ASSMTS	COURT PENALTIES	45110	00000	4,300.48
12/21/2023	100	139643	STEFANO & CHRISTINE SCHENA	DEFERRED REVENUES - ADVANCE TAX	26310	00000	142.01
12/21/2023	100	139644	STEVEN & JOANNE LASAGE	DEFERRED REVENUES - ADVANCE TAX	26310	00000	100.34
12/21/2023	100	139645	STEVEN WETTSTEIN JR	DEFERRED REVENUES - ADVANCE TAX	26310	00000	121.07
12/21/2023	100	139646	SUMIT & PRACHI PATHAK	DEFERRED REVENUES - ADVANCE TAX	26310	00000	89.46

Check Date	Bank	Check #	Payee	Description	Account	Dept	Amount
Fund: 100 GENERAL FUND							
12/21/2023	100	139647	SUSAN O'BRIEN	DEFERRED REVENUES - ADVANCE TAX	26310	00000	119.49
12/21/2023	100	139648	TED & BRENDA KIEPER	DEFERRED REVENUES - ADVANCE TAX	26310	00000	179.05
12/21/2023	100	139649	TERRY A SCHNEIDER	DEFERRED REVENUES - ADVANCE TAX	26310	00000	92.27
12/21/2023	100	139650	THOMAS & BARBARA SCHINKER	DEFERRED REVENUES - ADVANCE TAX	26310	00000	70.56
12/21/2023	100	139651	THOMAS HINE	DEFERRED REVENUES - ADVANCE TAX	26310	00000	113.53
12/21/2023	100	139652	TONI A WAGNER	DEFERRED REVENUES - ADVANCE TAX	26310	00000	276.74
12/21/2023	100	139653	TYRE & CHILDS PUBLIC SAFETY	FIRE ADMINISTRATION - EMPLOYMENT	52150	52210	500.00
12/21/2023	100	139654	WATTERS PAINTING LLC	FIRE ADMINISTRATION - BUILDING REPAIRS	52410	52210	500.00
12/21/2023	100	139655#	WAUKESHA CO TREASURER	COURT PENALTIES	45110	00000	1,388.80
				POLICE - OVERTIME	52150	52100	14,937.60
				POLICE - CONTRACT FOR SERVICES	52160	52100	275,250.52
				CHECK 100 139655 TOTAL FOR FUND 100:			291,576.92
12/21/2023	100	139656	WESTRIDGE BUILDERS	OCCUPANCY DEPOSITS	23175	00000	500.00
12/21/2023	100	139657	WILLIAM & VIRGINIA SKINNER	DEFERRED REVENUES - ADVANCE TAX	26310	00000	136.71
12/22/2023	100	1456 (E)	ADP INC	IT - SOFTWARE MAINTENANCE & UPDATES	52480	51450	355.54
12/22/2023	100	1458 (E)	DIVERSIFIED BENEFIT SERVICES, INC.	FLEX SPEND	21590	00000	517.06
12/22/2023	100	1469 (E)	ADP INC	IT - SOFTWARE MAINTENANCE & UPDATES	52480	51450	1,362.50
12/22/2023	100	1470 (E) *#	MUTUAL OF OMAHA	DISABILITY INSURANCE	21531	00000	2,039.69
				LTD - FIRE UNION	21533	00000	702.46
				VOLUNTARY LIFE	21534	00000	1,307.30
				COURT - LIFE INSURANCE	51340	51200	12.96
				COURT - DISABILITY INSURANCE	51350	51200	19.66
				ADMINISTRATOR - LIFE INSURANCE	51340	51410	9.86
				ADMINISTRATOR - DISABILITY INSURANCE	51350	51410	27.79
				CLERK/TREASURER - LIFE INSURANCE	51340	51420	73.76
				CLERK/TREASURER - DISABILITY INSURANCE	51350	51420	112.86
				EMPLOYEE SERVICES - LIFE INSURANCE	51340	51430	21.44

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CHECK DISBURSEMENT REPORT FOR PEWAUKEE
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Check Date	Bank	Check #	Payee	Description	Account	Dept	Amount
Fund: 100 GENERAL FUND							
				EMPLOYEE SERVICES - DISABILITY	51350	51430	27.79
				IT - LIFE INSURANCE	51340	51450	19.20
				IT - DISABILITY INSURANCE	51350	51450	27.79
				ASSESSOR - LIFE INSURANCE	51340	51530	45.60
				ASSESSOR - DISABILITY INSURANCE	51350	51530	69.35
				POLICE - LIFE INSURANCE	51340	52100	10.24
				POLICE - DISABILITY INSURANCE	51350	52100	14.61
				FIRE ADMINISTRATION - LIFE INSURANCE	51340	52210	67.36
				FIRE ADMINISTRATION - DISABILITY	51350	52210	83.37
				FIRE PROTECTIVE SERVICES - LIFE	51340	52230	525.28
				FIRE PROTECTIVE SERVICES - DISABILITY	51350	52230	111.16
				BUILDING SERVICES - LIFE INSURANCE	51340	52400	53.60
				BUILDING SERVICES - DISABILITY	51350	52400	82.00
				HIGHWAY - LIFE INSURANCE	51340	53100	122.72
				HIGHWAY - DISABILITY INSURANCE	51350	53100	187.75
				ENGINEERING - LIFE INSURANCE	51340	53110	112.96
				ENGINEERING - DISABILITY INSURANCE	51350	53110	164.32
				PARKS - LIFE INSURANCE	51340	55200	47.52
				PARKS - DISABILITY INSURANCE	51350	55200	70.32
				RECREATION PROGRAM - LIFE INSURANCE	51340	55300	32.96
				RECREATION PROGRAM - DISABILITY	51350	55300	51.71
				PLANNER - LIFE INSURANCE	51340	56300	21.44
				PLANNER - DISABILITY INSURANCE	51350	56300	27.79
				CHECK 100 1470(E) TOTAL FOR FUND 100:			6,304.62
12/27/2023	100	1460(E) #	DELTA DENTAL	DENTAL CLEARING	21903	00000	1,767.53
				INSURANCE CONSULTANT	52150	51930	409.92
				CHECK 100 1460(E) TOTAL FOR FUND 100:			2,177.45
12/28/2023	100	139661	ARAMARK	HIGHWAY - UNIFORMS	53410	53100	94.56
12/28/2023	100	139662*#	ARC DOCUMENT SOLUTIONS LLC	ENGINEERING - OPERATING SUPPLIES	53400	53110	14.72
12/28/2023	100	139664*#	BUMPER TO BUMPER HARTLAND	ENGINEERING - VEHICLE REPAIR & MAINT	52440	53110	73.14

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Check Date	Bank	Check #	Payee	Description	Account	Dept	Amount
Fund: 100 GENERAL FUND							
12/28/2023	100	139666*#	CINTAS CORP	PARKS - BUILDING REPAIRS & MAINT			** VOIDED **
12/28/2023	100	139667	CINTAS CORPORATION #184	HIGHWAY - OPERATING SUPPLIES	53400	53100	107.07
12/28/2023	100	139669	DAN PLAUTZ CLEANING SERVICE	CITY HALL - JANITORIAL SUPPLIES	52400	51600	2,873.00
12/28/2023	100	139671	DIVERSIFIED BENEFIT SERVICES, INC.	INSURANCE CONSULTANT	52150	51930	202.50
12/28/2023	100	139672*#	EHLERS	ENGINEERING - TRAINING & SEMINARS	52980	53110	100.00
12/28/2023	100	139674*#	FORWARD TS	ENGINEERING - OPERATING SUPPLIES	53400	53110	130.88
12/28/2023	100	139678	James Neumann	BOB23-0016	23175	00000	500.00
12/28/2023	100	139679	JOHNS DISPOSAL SERVICE	RECYCLE - GARBAGE COLLECTION	52800	53620	18,174.97
12/28/2023	100	139680	LINCOLN CONTRACTORS	HIGHWAY - SAFETY EQUIPMENT	53480	53100	121.98
12/28/2023	100	139681	LINE-X MILWAUKEE	PARKS - EQUIP REPAIR & MAINT	52430	55200	785.00
12/28/2023	100	139682*#	MENARDS	HIGHWAY - OPERATING SUPPLIES	53400	53100	38.91
				PARKS - VEHICLE REPAIR & MAINT	52440	55200	134.99
				CHECK 100 139682 TOTAL FOR FUND 100:			173.90
12/28/2023	100	139683#	MUNICIPAL LAW & LITIGATION GROUP	GENERAL AFFAIRS ATTORNEY	52100	51300	6,332.80
				GENERAL AFFAIRS ATTORNEY	52100	51300	11,780.00
				POLICE - ATTORNEY	52100	52100	2,930.00
				POLICE - ATTORNEY	52100	52100	3,382.50
				CHECK 100 139683 TOTAL FOR FUND 100:			24,425.30
12/28/2023	100	139685	NICK PHALIN	RECREATION PROGRAM - MILEAGE	53300	55300	33.41
12/28/2023	100	139687*#	ODP BUSINESS SOLUTIONS LLC	ENGINEERING - OPERATING SUPPLIES	53400	53110	50.38
12/28/2023	100	139688	PARTNER2LEARN, LLC	PREPAID EXPENSES	16200	00000	10,400.00
				PREPAID EXPENSES	16200	00000	5,875.00
				CHECK 100 139688 TOTAL FOR FUND 100:			16,275.00

Check Date	Bank	Check #	Payee	Description	Account	Dept	Amount
Fund: 100 GENERAL FUND							
12/28/2023	100	139689*#	PAYNE & DOLAN	HIGHWAY - ROAD REPAIRS	53730	53100	207.20
				HIGHWAY - ROAD REPAIRS	53730	53100	140.00
				CHECK 100 139689 TOTAL FOR FUND 100:			347.20
12/28/2023	100	139690	PETER JENSEN	EMPLOYEE SERVICES - EMPLOYMENT EXAMS	52150	51430	100.00
12/28/2023	100	139691	PEWAUKEE SCHOOL DISTRICT	TAX LEVY DUE SCHOOL DISTRICTS	24600	00000	2,000,000.00
12/28/2023	100	139693	ROBERT KEWAN	IT - MILEAGE	53300	51450	146.72
12/28/2023	100	139695	SAFETY-KLEEN CORP	SERVICE FEES	52900	53635	80.00
12/28/2023	100	139696*#	SEILER INSTRUMENT & MANUFACTURING	ENGINEERING - NEW EQUIPMENT	53950	53110	1,160.00
12/28/2023	100	139697	SHERWIN INDUSTRIES	HIGHWAY - ROAD REPAIRS	53730	53100	771.75
12/28/2023	100	139698	SKIPPER BUD'S	POLICE - COMMUNITY PROGRAM	52180	52100	1,516.52
12/28/2023	100	139699*#	STRAND ASSOCIATES, INC	PREPAID EXPENSES	16200	00000	10,152.40
12/28/2023	100	139701	TYRE & CHILDS PUBLIC SAFETY	FIRE ADMINISTRATION - EMPLOYMENT	52150	52210	500.00
12/28/2023	100	139703*#	WE ENERGIES	HIGHWAY - ELECTRICITY	52210	53100	1,108.15
				GAS FOR HEAT (NATURAL GAS)	52220	53100	497.84
				CHECK 100 139703 TOTAL FOR FUND 100:			1,605.99
12/28/2023	100	139704	WI RURAL WATER ASSOCIATION	PREPAID EXPENSES	16200	00000	110.00
				PREPAID EXPENSES	16200	00000	110.00
				CHECK 100 139704 TOTAL FOR FUND 100:			220.00
12/28/2023	100	139706	WOLDANSKI, KELLEY	EMPLOYEE SERVICES - SERVICE RECOGNITION	53430	51430	51.47
12/28/2023	100	139707*#	WOLF PAVING	HIGHWAY - ROAD REPAIRS	53730	53100	268.50
12/28/2023	100	1480 (E)	LEASING SERVICES	FIRE ADMINISTRATION - OPERATING	53400	52210	134.00
12/29/2023	100	1461 (E)	DIVERSIFIED BENEFIT SERVICES, INC.	FLEX SPEND	21590	00000	1,359.09

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Fund: 100 GENERAL FUND							
12/29/2023	100	1462 (E)	GREAT WEST TRUST - WDC	DEFERRED COMPENSATION	21570	00000	4,622.08
				DEF COMP - ROTH 457	21575	00000	3,018.70
				CHECK 100 1462(E) TOTAL FOR FUND 100:			7,640.78
12/29/2023	100	1463 (E)	GREAT WEST TRUST - WDC	DEFERRED COMPENSATION	21570	00000	4,522.08
				DEF COMP - ROTH 457	21575	00000	3,018.70
				CHECK 100 1463(E) TOTAL FOR FUND 100:			7,540.78
12/29/2023	100	1464 (E)	GREAT WEST TRUST - WDC	DEFERRED COMPENSATION	21570	00000	4,522.08
				DEF COMP - ROTH 457	21575	00000	3,018.70
				CHECK 100 1464(E) TOTAL FOR FUND 100:			7,540.78
12/29/2023	100	1472 (E)	ADP INC	IT - SOFTWARE MAINTENANCE & UPDATES	52480	51450	405.00
12/29/2023	100	1479 (E)	LEASING SERVICES	CLERK/TREASURER - EQUIP REPAIR & MAINT	52430	51420	324.00
01/02/2024	100	1465 (E)	ADP INC	IT - SOFTWARE MAINTENANCE & UPDATES	52480	51450	383.02
01/02/2024	100	1466 (E)	DIVERSIFIED BENEFIT SERVICES, INC.	FLEX SPEND	21590	00000	1,021.90
01/05/2024	100	139717	1ST AYD	FIRE ADMINISTRATION - OPERATING	53400	52210	724.20
01/05/2024	100	139718*#	A&M CLEANING SOLUTIONS	HIGHWAY - CONTRACTED JANITORIAL	52400	53100	1,458.60
01/05/2024	100	139719	AARON & REBEKAH GRACE	DEFERRED REVENUES - ADVANCE TAX	26310	00000	125.75
01/05/2024	100	139720	ABHISHEK AGARWAL	DEFERRED REVENUES - ADVANCE TAX	26310	00000	422.24
01/05/2024	100	139721	ADAM & CARLIE PERRY	DEFERRED REVENUES - ADVANCE TAX	26310	00000	145.92
01/05/2024	100	139722	AIRGAS USA	FIRE PROTECTIVE SERVICES - EMS	53450	52230	156.86
				FIRE PROTECTIVE SERVICES - EMS	53450	52230	379.47
				CHECK 100 139722 TOTAL FOR FUND 100:			536.33
01/05/2024	100	139723	AMANDA TAYLOR	DEFERRED REVENUES - ADVANCE TAX	26310	00000	115.33
01/05/2024	100	139724*#	AMERICAN PAPER & LOGGING	HIGHWAY - CONTRACTED JANITORIAL	52400	53100	120.00

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Fund: 100 GENERAL FUND							
01/05/2024	100	139725	ANTHONY & MARGARET MARINO	DEFERRED REVENUES - ADVANCE TAX	26310	00000	196.67
01/05/2024	100	139726	ASSOCIATED TRUST	INSURANCE CONSULTANT	52150	51930	2,500.00
01/05/2024	100	139727	AT&T MOBILITY	FIRE ADMINISTRATION - PHONE & CELL	52260	52210	857.91
01/05/2024	100	139728	BEVERLY LANGERUD	DEFERRED REVENUES - ADVANCE TAX	26310	00000	168.08
01/05/2024	100	139729	BEVERLY REHFELD	DEFERRED REVENUES - ADVANCE TAX	26310	00000	78.27
01/05/2024	100	139730	BLUE RIBBON FIBERGLASS REPAIR	FIRE PROTECTIVE SERVICES - VEHICLE	52440	52230	875.00
01/05/2024	100	139731	BOB'S GLASS	FIRE ADMINISTRATION - BUILDING REPAIRS	52410	52210	1,460.00
01/05/2024	100	139732	BOJAR FAMILY TRUST	DEFERRED REVENUES - ADVANCE TAX	26310	00000	708.14
01/05/2024	100	139733	BRANDON ARPS & JESSICA KING	DEFERRED REVENUES - ADVANCE TAX	26310	00000	874.47
01/05/2024	100	139734	BRIAN & CHRISTINE FRICK	DEFERRED REVENUES - ADVANCE TAX	26310	00000	149.82
01/05/2024	100	139735	CENTEC SECURITY SYSTEMS	IT - OTHER PROFESSIONAL SERVICES	52190	51450	567.00
01/05/2024	100	139736	CHARLIE DWYER	BUILDING SERVICES - MILEAGE & FUEL	53300	52400	140.83
01/05/2024	100	139737	CHIH CHUAN SUN & YIFANG ZOU	DEFERRED REVENUES - ADVANCE TAX	26310	00000	221.93
01/05/2024	100	139738	CHRISTINE COLBERT	DEFERRED REVENUES - ADVANCE TAX	26310	00000	70.69
01/05/2024	100	139739	CHRISTOPHER & MARI MAGNUSON	DEFERRED REVENUES - ADVANCE TAX	26310	00000	2,040.06
01/05/2024	100	139741	CRAIG WAHLGREN	DEFERRED REVENUES - ADVANCE TAX	26310	00000	763.79
01/05/2024	100	139742	DANIEL & SUZANNE HARDING	DEFERRED REVENUES - ADVANCE TAX	26310	00000	289.51
01/05/2024	100	139743	DANIEL BARWINSKI & LISA	DEFERRED REVENUES - ADVANCE TAX	26310	00000	104.85
01/05/2024	100	139744	DANIEL WOODWARD	DEFERRED REVENUES - ADVANCE TAX	26310	00000	253.05
01/05/2024	100	139745	DARREN & KELSIE FERSTERLING	DEFERRED REVENUES - ADVANCE TAX	26310	00000	144.07
01/05/2024	100	139746	DAVID & JOYCE JOUNTSON	DEFERRED REVENUES - ADVANCE TAX	26310	00000	180.88
01/05/2024	100	139747	DAVID & TRACI KNEBES	DEFERRED REVENUES - ADVANCE TAX	26310	00000	164.35
01/05/2024	100	139748	DAVID CASEY	DEFERRED REVENUES - ADVANCE TAX	26310	00000	229.48

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Fund: 100 GENERAL FUND							
01/05/2024	100	139749	DEBRA STEPHENSON	DEFERRED REVENUES - ADVANCE TAX	26310	00000	250.32
01/05/2024	100	139750	DENNIS ZENIECKI	DEFERRED REVENUES - ADVANCE TAX	26310	00000	84.07
01/05/2024	100	139751	DONALD & ANGELA DORFF	DEFERRED REVENUES - ADVANCE TAX	26310	00000	24.19
01/05/2024	100	139752	DONNA BOROWSKI	DEFERRED REVENUES - ADVANCE TAX	26310	00000	4.29
01/05/2024	100	139753	DONNA R. LITZOW REVOCABLE TRUST	DEFERRED REVENUES - ADVANCE TAX	26310	00000	86.26
01/05/2024	100	139754	DOUANGMALA PHRASAVATH	DEFERRED REVENUES - ADVANCE TAX	26310	00000	612.65
01/05/2024	100	139755	ELEVITY GORDON FLESCH CO INC	IT - SOFTWARE MAINTENANCE & UPDATES	52480	51450	136.00
				IT - SOFTWARE MAINTENANCE & UPDATES	52480	51450	4,520.80
				CHECK 100 139755 TOTAL FOR FUND 100:			4,656.80
01/05/2024	100	139756	ERIC & ELIZABETH CHUDZIK	DEFERRED REVENUES - ADVANCE TAX	26310	00000	244.27
01/05/2024	100	139757	ERIC & EMILY TROYK	DEFERRED REVENUES - ADVANCE TAX	26310	00000	256.93
01/05/2024	100	139758	ERIC & MELISSA CORBEG	DEFERRED REVENUES - ADVANCE TAX	26310	00000	242.38
01/05/2024	100	139759	ERIK & KAREN EIDEM	DEFERRED REVENUES - ADVANCE TAX	26310	00000	158.36
01/05/2024	100	139760	ERIK & SHANAN BORRESEN	DEFERRED REVENUES - ADVANCE TAX	26310	00000	165.90
01/05/2024	100	139761	EUGENE & SUSAN ANN BEST	DEFERRED REVENUES - ADVANCE TAX	26310	00000	88.27
01/05/2024	100	139762	EVERDRY	DEFERRED REVENUES - ADVANCE TAX	26310	00000	1,053.72
01/05/2024	100	139764	FIRE SERVICE INC	FIRE PROTECTIVE SERVICES - VEHICLE	52440	52230	1,930.10
				FIRE PROTECTIVE SERVICES - VEHICLE	52440	52230	10,089.34
				CHECK 100 139764 TOTAL FOR FUND 100:			12,019.44
01/05/2024	100	139765	GALLS	FIRE PROTECTIVE SERVICES - UNIFORMS	53410	52230	438.57
				FIRE PROTECTIVE SERVICES - UNIFORMS	53410	52230	70.78
				FIRE PROTECTIVE SERVICES - UNIFORMS	53410	52230	56.89
				FIRE PROTECTIVE SERVICES - UNIFORMS	53410	52230	63.98
				FIRE PROTECTIVE SERVICES - UNIFORMS	53410	52230	298.90

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Fund: 100 GENERAL FUND				CHECK 100 139765 TOTAL FOR FUND 100:			929.12
01/05/2024	100	139766	GARABET & KARA BOGHOSSIAN	DEFERRED REVENUES - ADVANCE TAX	26310	00000	195.67
01/05/2024	100	139767	GARY & BARBARA PLUSTER	DEFERRED REVENUES - ADVANCE TAX	26310	00000	78.75
01/05/2024	100	139768	GENETTE M SANTHARAM	DEFERRED REVENUES - ADVANCE TAX	26310	00000	228.24
01/05/2024	100	139769	GINA HOBBS	DEFERRED REVENUES - ADVANCE TAX	26310	00000	83.06
01/05/2024	100	139770	GLORIA BROOKS	DEFERRED REVENUES - ADVANCE TAX	26310	00000	69.27
01/05/2024	100	139771	GOPAL & KAUSHALYA IYENGAR	DEFERRED REVENUES - ADVANCE TAX	26310	00000	425.33
01/05/2024	100	139772	GRANT & MEGAN PETERSON	DEFERRED REVENUES - ADVANCE TAX	26310	00000	113.16
01/05/2024	100	139773	GREGORY & CHRISTINR GRADECKI	DEFERRED REVENUES - ADVANCE TAX	26310	00000	155.19
01/05/2024	100	139774	GRIFFIN & DANA PROCHNOW	DEFERRED REVENUES - ADVANCE TAX	26310	00000	173.56
01/05/2024	100	139775	HEARTLAND BUSINESS SYSTEMS	IT - OTHER PROFESSIONAL SERVICES	52190	51450	23.00
01/05/2024	100	139776	HEIDI BECKER	DEFERRED REVENUES - ADVANCE TAX	26310	00000	40.65
01/05/2024	100	139777	IS OUTFITTERS	IT - OTHER PROFESSIONAL SERVICES	52190	51450	1,720.00
				IT - OTHER PROFESSIONAL SERVICES	52190	51450	217.50
				IT - SOFTWARE MAINTENANCE & UPDATES	52480	51450	6,440.55
				CHECK 100 139777 TOTAL FOR FUND 100:			8,378.05
01/05/2024	100	139778	JACK & KENDELL STRAEHLER	DEFERRED REVENUES - ADVANCE TAX	26310	00000	168.59
01/05/2024	100	139779	JACOB & MEGHAN JEHOWSKI	DEFERRED REVENUES - ADVANCE TAX	26310	00000	81.32
01/05/2024	100	139780	JAMES & ALLISON BLACKWOOD	DEFERRED REVENUES - ADVANCE TAX	26310	00000	220.99
01/05/2024	100	139781	JAMES DENNIK	DEFERRED REVENUES - ADVANCE TAX	26310	00000	54.49
01/05/2024	100	139782	JAMES IMAGING SYSTEMS	IT - OTHER PROFESSIONAL SERVICES	52190	51450	65.00
01/05/2024	100	139783	JASON & TRACY RYAN LIVING TRUST	DEFERRED REVENUES - ADVANCE TAX	26310	00000	330.05
01/05/2024	100	139784	JEFFERY HERNSHEIM	DEFERRED REVENUES - ADVANCE TAX	26310	00000	280.48
01/05/2024	100	139785	JEFFREY & JESSICA BURNS	DEFERRED REVENUES - ADVANCE TAX	26310	00000	82.63

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Fund: 100 GENERAL FUND							
01/05/2024	100	139786	JEFFREY & KARI RIDOLFI	DEFERRED REVENUES - ADVANCE TAX	26310	00000	230.57
01/05/2024	100	139787	JEFFREY & KATIE LAUTERBACH	DEFERRED REVENUES - ADVANCE TAX	26310	00000	171.17
01/05/2024	100	139788	JEFFREY & SANDRA SCHOENMANN	DEFERRED REVENUES - ADVANCE TAX	26310	00000	294.55
01/05/2024	100	139789	JEFFREY SOMENSKE	DEFERRED REVENUES - ADVANCE TAX	26310	00000	187.68
01/05/2024	100	139790	JILL M PURKO	DEFERRED REVENUES - ADVANCE TAX	26310	00000	210.42
01/05/2024	100	139791	JOEL & LINDSAY MEINCKE	DEFERRED REVENUES - ADVANCE TAX	26310	00000	168.44
01/05/2024	100	139792	JOHN & DIANE FAY	DEFERRED REVENUES - ADVANCE TAX	26310	00000	105.76
01/05/2024	100	139793	JOHN & EMILY LEHNEN	DEFERRED REVENUES - ADVANCE TAX	26310	00000	108.45
01/05/2024	100	139794	JOHN & ERICA ELLENBERGER	DEFERRED REVENUES - ADVANCE TAX	26310	00000	153.25
01/05/2024	100	139795	JOHN & JUDITH PATIN	DEFERRED REVENUES - ADVANCE TAX	26310	00000	133.52
01/05/2024	100	139796	JOHN & SHELLIE LOSSMAN	DEFERRED REVENUES - ADVANCE TAX	26310	00000	705.52
01/05/2024	100	139797	JOHN J SCHULTZ REVOCABLE TRUST	DEFERRED REVENUES - ADVANCE TAX	26310	00000	117.70
01/05/2024	100	139798	JOHN R LOEW & PAULA M VERBOOMEN	DEFERRED REVENUES - ADVANCE TAX	26310	00000	249.32
01/05/2024	100	139799	JON & COURTNEY IGNATOWSKI	DEFERRED REVENUES - ADVANCE TAX	26310	00000	190.89
01/05/2024	100	139800	JONATHAN DAVIS & LAURA GRESSER	DEFERRED REVENUES - ADVANCE TAX	26310	00000	84.01
01/05/2024	100	139801	JOSEPH PROPERTY DEVELOPMENT	DEFERRED REVENUES - ADVANCE TAX	26310	00000	5,236.22
01/05/2024	100	139802	JOSEPH PROPERTY DEVELOPMENT	DEFERRED REVENUES - ADVANCE TAX	26310	00000	4,224.15
01/05/2024	100	139803	JOSEPH PROPERTY DEVELOPMENT	DEFERRED REVENUES - ADVANCE TAX	26310	00000	3,012.52
01/05/2024	100	139804	JUDITH RISIC	DEFERRED REVENUES - ADVANCE TAX	26310	00000	315.26
01/05/2024	100	139805	JURGEN SEEBERGER & STACY SEE	DEFERRED REVENUES - ADVANCE TAX	26310	00000	387.00
01/05/2024	100	139806	JUSTIN & REBECCA LACHOWIN	DEFERRED REVENUES - ADVANCE TAX	26310	00000	189.32
01/05/2024	100	139807	KATHERINE ROSE	DEFERRED REVENUES - ADVANCE TAX	26310	00000	59.54
01/05/2024	100	139808	KEITH & SANDRA TADEYESKE	DEFERRED REVENUES - ADVANCE TAX	26310	00000	128.23

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Fund: 100 GENERAL FUND							
01/05/2024	100	139809	KELLY & JEANNE BIEVER	DEFERRED REVENUES - ADVANCE TAX	26310	00000	191.32
01/05/2024	100	139810	KELLY KINGSTON	DEFERRED REVENUES - ADVANCE TAX	26310	00000	105.45
01/05/2024	100	139811	KENNETH & DEBRA ELLENBERGER	DEFERRED REVENUES - ADVANCE TAX	26310	00000	12.52
01/05/2024	100	139812	KEVAN HENDERSON	DEFERRED REVENUES - ADVANCE TAX	26310	00000	395.97
01/05/2024	100	139813	KEVIN & LAURI ECKMANN	DEFERRED REVENUES - ADVANCE TAX	26310	00000	51.21
01/05/2024	100	139814	KIM SAWASKY	DEFERRED REVENUES - ADVANCE TAX	26310	00000	291.85
01/05/2024	100	139815	KLEIN FAMILY TRUST	DEFERRED REVENUES - ADVANCE TAX	26310	00000	389.06
01/05/2024	100	139816	KURT & LINDA KRAHN	DEFERRED REVENUES - ADVANCE TAX	26310	00000	132.97
01/05/2024	100	139817	KYLE & STACY SODERSTROM	DEFERRED REVENUES - ADVANCE TAX	26310	00000	300.26
01/05/2024	100	139818	LAUREL HONAKER	DEFERRED REVENUES - ADVANCE TAX	26310	00000	64.67
01/05/2024	100	139819*#	LIFE-ASSIST INC	FIRE PROTECTIVE SERVICES - EMS	53450	52230	549.31
				FIRE PROTECTIVE SERVICES - EMS	53450	52230	526.06
				FIRE PROTECTIVE SERVICES - EMS	53450	52230	401.06
				CHECK 100 139819 TOTAL FOR FUND 100:			1,476.43
01/05/2024	100	139820	LINDA AUBRY	DEFERRED REVENUES - ADVANCE TAX	26310	00000	84.38
01/05/2024	100	139821	LINDA COONEY	DEFERRED REVENUES - ADVANCE TAX	26310	00000	83.16
01/05/2024	100	139822	LISA SCHOWALTER	DEFERRED REVENUES - ADVANCE TAX	26310	00000	531.22
01/05/2024	100	139824	LORAN & CHERYL REEVES TRUST	DEFERRED REVENUES - ADVANCE TAX	26310	00000	53.59
01/05/2024	100	139825	MARILYM HACKNEY	DEFERRED REVENUES - ADVANCE TAX	26310	00000	177.97
01/05/2024	100	139826	MARK & LINDA STEMPEL	DEFERRED REVENUES - ADVANCE TAX	26310	00000	479.58
01/05/2024	100	139827	MARK HARTER & DIANE BELLIN	DEFERRED REVENUES - ADVANCE TAX	26310	00000	199.19
01/05/2024	100	139828	MARTHA BALFANY	DEFERRED REVENUES - ADVANCE TAX	26310	00000	129.67
01/05/2024	100	139829	MATTHEW & CHRISTINE WAYDA	DEFERRED REVENUES - ADVANCE TAX	26310	00000	131.45
01/05/2024	100	139830	MATTHEW & KATHERINE SCALLON	DEFERRED REVENUES - ADVANCE TAX	26310	00000	188.34

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Fund: 100 GENERAL FUND							
01/05/2024	100	139831	MATTHEW & TANIA ANDERSON	DEFERRED REVENUES - ADVANCE TAX	26310	00000	315.38
01/05/2024	100	139832	MATTHEW LOCASHA & SHAUNA DAUL	DEFERRED REVENUES - ADVANCE TAX	26310	00000	79.99
01/05/2024	100	139833	MENARDS	FIRE ADMINISTRATION - OPERATING	53400	52210	24.97
				FIRE ADMINISTRATION - OPERATING	53400	52210	27.24
				FIRE ADMINISTRATION - OPERATING	53400	52210	26.97
				CHECK 100 139833 TOTAL FOR FUND 100:			79.18
01/05/2024	100	139834	MICHAEL & KARLIE KACZMAREK	DEFERRED REVENUES - ADVANCE TAX	26310	00000	480.92
01/05/2024	100	139835	MICHAEL & KATHERINE WEIMER	DEFERRED REVENUES - ADVANCE TAX	26310	00000	179.77
01/05/2024	100	139836	MICHAEL & LAUREN MONFELI	DEFERRED REVENUES - ADVANCE TAX	26310	00000	377.77
01/05/2024	100	139837	MICHAEL NELSON	DEFERRED REVENUES - ADVANCE TAX	26310	00000	69.27
01/05/2024	100	139838	MICHAEL OLKWITZ	DEFERRED REVENUES - ADVANCE TAX	26310	00000	184.86
01/05/2024	100	139839	MICHELLE BOURGET	DEFERRED REVENUES - ADVANCE TAX	26310	00000	361.61
01/05/2024	100	139841	MOTION CONNECTED	EMPLOYEE SERVICES - EMPLOYEE WELLNESS	52700	51430	1,905.00
				EMPLOYEE SERVICES - EMPLOYEE WELLNESS	52700	51430	405.00
				CHECK 100 139841 TOTAL FOR FUND 100:			2,310.00
01/05/2024	100	139842	NATHANIEL & KATHERINE STUDER	DEFERRED REVENUES - ADVANCE TAX	26310	00000	30.33
01/05/2024	100	139843	NICHOLAS & AMANDA PASSINT	DEFERRED REVENUES - ADVANCE TAX	26310	00000	270.38
01/05/2024	100	139844	NICHOLAS & ANGELA FERRI	DEFERRED REVENUES - ADVANCE TAX	26310	00000	322.27
01/05/2024	100	139845	NIKOLA & ZORICA MOMCILOVIC	DEFERRED REVENUES - ADVANCE TAX	26310	00000	199.22
01/05/2024	100	139846	NOEL LYNN PETZOLD	DEFERRED REVENUES - ADVANCE TAX	26310	00000	189.02
01/05/2024	100	139847	ODP BUSINESS SOLUTIONS LLC	BUILDING SERVICES - OPERATING SUPPLIES	53400	52400	66.80
01/05/2024	100	139848	ONSOLVE	HIGHWAY - PHONE & CELL	52260	53100	604.52
01/05/2024	100	139849	PAMELA ARTEMAN	DEFERRED REVENUES - ADVANCE TAX	26310	00000	194.68
01/05/2024	100	139850	PATRICK & MARIA KUCHE	DEFERRED REVENUES - ADVANCE TAX	26310	00000	207.74

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Fund: 100 GENERAL FUND							
01/05/2024	100	139851	PATRICK MISCHKER	DEFERRED REVENUES - ADVANCE TAX	26310	00000	134.66
01/05/2024	100	139852	PAUL & CHRISTINE JACKSON	DEFERRED REVENUES - ADVANCE TAX	26310	00000	347.38
01/05/2024	100	139853	PAUL & JOHNNELL EALES	DEFERRED REVENUES - ADVANCE TAX	26310	00000	142.18
01/05/2024	100	139854	PAUL & KATHLEEN THOMAS	DEFERRED REVENUES - ADVANCE TAX	26310	00000	104.75
01/05/2024	100	139856	PETER & JENNIFER KNOKE	DEFERRED REVENUES - ADVANCE TAX	26310	00000	635.64
01/05/2024	100	139857	PHILLIP & TRICIA JANOSKA	DEFERRED REVENUES - ADVANCE TAX	26310	00000	190.14
01/05/2024	100	139858	PROHEALTH PHARMACY WAUKESHA	FIRE PROTECTIVE SERVICES - EMS	53450	52230	1,858.80
01/05/2024	100	139859	RAMANI RAMCHANDRAN	DEFERRED REVENUES - ADVANCE TAX	26310	00000	13.00
01/05/2024	100	139860	RAMENDRA AND NICOLE ANAND	DEFERRED REVENUES - ADVANCE TAX	26310	00000	259.35
01/05/2024	100	139861	RANDALL & SUSAN VANAARTSEN	DEFERRED REVENUES - ADVANCE TAX	26310	00000	484.25
01/05/2024	100	139862	REBECCA SHEPERD LIVING TRUST	DEFERRED REVENUES - ADVANCE TAX	26310	00000	619.03
01/05/2024	100	139863	RICHARD & HEATHER GRINDATTI	DEFERRED REVENUES - ADVANCE TAX	26310	00000	547.10
01/05/2024	100	139864	RICHARD & NANCY RAFENSTEIN	DEFERRED REVENUES - ADVANCE TAX	26310	00000	116.05
01/05/2024	100	139865	RICHARD & SHARON PERKINS	DEFERRED REVENUES - ADVANCE TAX	26310	00000	180.72
01/05/2024	100	139866	ROBERT & SHANNON MYHRE LIVING	DEFERRED REVENUES - ADVANCE TAX	26310	00000	402.70
01/05/2024	100	139867	RONALD & JANE BUXBAUM	DEFERRED REVENUES - ADVANCE TAX	26310	00000	180.72
01/05/2024	100	139868	RONALD & TERRI DRAPER	DEFERRED REVENUES - ADVANCE TAX	26310	00000	178.81
01/05/2024	100	139869	RYAN & ALYSSA COUTTS	DEFERRED REVENUES - ADVANCE TAX	26310	00000	103.70
01/05/2024	100	139870	SAFETY-KLEEN CORP	SERVICE FEES	52900	53635	68.77
01/05/2024	100	139871	SCOTT & CAROL WILLE	DEFERRED REVENUES - ADVANCE TAX	26310	00000	192.47
01/05/2024	100	139872	SCOTT & KATHLEEN CIESIELCZYK	DEFERRED REVENUES - ADVANCE TAX	26310	00000	150.50
01/05/2024	100	139873	SCOTT & MICHELLE ZAJIC	DEFERRED REVENUES - ADVANCE TAX	26310	00000	926.15
01/05/2024	100	139874	SCOTT DIECKELMAN	DEFERRED REVENUES - ADVANCE TAX	26310	00000	63.41

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01/05/2024	100	139875	SCOTT& SHELLY REID	DEFERRED REVENUES - ADVANCE TAX	26310	00000	235.51
01/05/2024	100	139876	SEAN & SARAH LUEDKE	DEFERRED REVENUES - ADVANCE TAX	26310	00000	205.54
01/05/2024	100	139877	SHANE & LISA PIEPER	DEFERRED REVENUES - ADVANCE TAX	26310	00000	232.45
01/05/2024	100	139878	SHARE CORPORATION	HIGHWAY - OPERATING SUPPLIES	53400	53100	225.59
01/05/2024	100	139880	STEPHANIE BIEDLINGMAIER	DEFERRED REVENUES - ADVANCE TAX	26310	00000	166.11
01/05/2024	100	139881	STEVE & JODI SWANSON	DEFERRED REVENUES - ADVANCE TAX	26310	00000	113.31
01/05/2024	100	139883	SUMAN & ANUPAMA CHEAKURI	DEFERRED REVENUES - ADVANCE TAX	26310	00000	427.65
01/05/2024	100	139884	SUNDANCE LAND CO. INC.	DEFERRED REVENUES - ADVANCE TAX	26310	00000	671.89
01/05/2024	100	139885	SUSAN COOLEY	DEFERRED REVENUES - ADVANCE TAX	26310	00000	97.39
01/05/2024	100	139886	TEDD JACOBSON	DEFERRED REVENUES - ADVANCE TAX	26310	00000	122.80
01/05/2024	100	139887	THOMAS & KIERRYNN MALONE	DEFERRED REVENUES - ADVANCE TAX	26310	00000	131.59
01/05/2024	100	139888	THOMAS & LEANNE GEORGE	DEFERRED REVENUES - ADVANCE TAX	26310	00000	212.82
01/05/2024	100	139889	THOMAS BROCHHAUSEN	DEFERRED REVENUES - ADVANCE TAX	26310	00000	69.83
01/05/2024	100	139890	THOMAS KRUMRAI	DEFERRED REVENUES - ADVANCE TAX	26310	00000	262.32
01/05/2024	100	139891	TIMOTHY & KATHERINE CAPREZ	DEFERRED REVENUES - ADVANCE TAX	26310	00000	283.61
01/05/2024	100	139892	TIMOTHY & VILMA KOLARIK	DEFERRED REVENUES - ADVANCE TAX	26310	00000	204.11
01/05/2024	100	139893	TODD CLAREY	DEFERRED REVENUES - ADVANCE TAX	26310	00000	70.87
01/05/2024	100	139894	TOM CASANOVA & MERRICK PEARSON	DEFERRED REVENUES - ADVANCE TAX	26310	00000	114.57
01/05/2024	100	139895	UNIFIRST CORP	FIRE ADMINISTRATION - OPERATING	53400	52210	95.27
01/05/2024	100	139896	WASTE MANAGEMENT	SERVICE FEES	52900	53635	3,665.17
01/05/2024	100	139898	WILLIAM & BETH REDMOND	DEFERRED REVENUES - ADVANCE TAX	26310	00000	524.50
01/05/2024	100	139899	WILLIAM & BETH REDMOND	DEFERRED REVENUES - ADVANCE TAX	26310	00000	802.05
01/05/2024	100	139900	WILLIAM & RACHELLE KOHLER	DEFERRED REVENUES - ADVANCE TAX	26310	00000	45.00

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01/05/2024	100	139901	WILLIAM R BRAUNSCHEWIG	DEFERRED REVENUES - ADVANCE TAX	26310	00000	60.23
01/05/2024	100	139902	ZACHARY & ROCHELLE YANASAK	DEFERRED REVENUES - ADVANCE TAX	26310	00000	433.07
01/05/2024	100	1467 (E)	DELTA DENTAL	DENTAL CLEARING	21903	00000	1,165.36
01/09/2024	100	139903	ARROWHEAD SCHOOL DISTRICT	TAX LEVY DUE SCHOOL DISTRICTS	24600	00000	15,438.20
01/09/2024	100	139904	HAMILTON SCHOOL DISTRICT	TAX LEVY DUE SCHOOL DISTRICTS	24600	00000	598,179.70
01/09/2024	100	139905	PEWAUKEE SCHOOL DISTRICT	TAX LEVY DUE SCHOOL DISTRICTS	24600	00000	6,691,002.94
01/09/2024	100	139906	RICHMOND SCHOOL DISTRICT	TAX LEVY DUE SCHOOL DISTRICTS	24600	00000	31,199.39
01/09/2024	100	139907	WAUKESHA CO TECHNICAL COLLEGE	TAX LEVY DUE SCHOOL DISTRICTS	24600	00000	569,842.32
01/09/2024	100	139908	WAUKESHA CO TREASURER	TAX LEVY DUE COUNTY	24310	00000	3,163,324.55
01/09/2024	100	139909	WAUKESHA SCHOOL DISTRICT	TAX LEVY DUE SCHOOL DISTRICTS	24600	00000	5,146,542.35
01/10/2024	100	1484 (E)	DELTA DENTAL	DENTAL CLEARING	21903	00000	2,709.85
01/11/2024	100	139910	AIRGAS USA	FIRE PROTECTIVE SERVICES - EMS	53450	52230	276.51
				FIRE PROTECTIVE SERVICES - EMS	53450	52230	195.26
				CHECK 100 139910 TOTAL FOR FUND 100:			471.77
01/11/2024	100	139911	ALBERT & KELLY LAGORE	REAL ESTATE TAXES RECEIVABLE	12100	00000	177.95
01/11/2024	100	139912	ALEXANDER & MARISSA URBAN	REAL ESTATE TAXES RECEIVABLE	12100	00000	288.37
01/11/2024	100	139914	ALL-WAYS CONTRACTORS, INC	PARKS - GROUNDS MAINTENANCE	52420	55200	126.00
01/11/2024	100	139915	AMERICAN TIRE DISTRUTORS	HIGHWAY - EQUIP REPAIR & MAINT	52430	53100	499.62
				HIGHWAY - EQUIP REPAIR & MAINT	52430	53100	499.62
				CHECK 100 139915 TOTAL FOR FUND 100:			999.24
01/11/2024	100	139916	ANTHONY & TRACY SHERMAN	DEFERRED REVENUES - ADVANCE TAX	26310	00000	101.05
01/11/2024	100	139917	APEX SOFTWARE	IT - SOFTWARE MAINTENANCE & UPDATES	52480	51450	780.00
01/11/2024	100	139918	ARAMARK	HIGHWAY - UNIFORMS	53410	53100	94.56

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Fund: 100 GENERAL FUND				HIGHWAY - UNIFORMS	53410	53100	94.56
				HIGHWAY - UNIFORMS	53410	53100	94.56
				CHECK 100 139918 TOTAL FOR FUND 100:			283.68
01/11/2024	100	139919*	AT&T MOBILITY	ADMINISTRATOR - TELEPHONE & CELL	52260	51410	33.49
				CLERK/TREASURER - TELEPHONE & CELL	52260	51420	33.49
				EMPLOYEE SERVICES - PHONE & CELL	52260	51430	43.47
				IT - PHONE & CELL	52260	51450	82.01
				IT - NEW EQUIPMENT	53950	51450	692.54
				FIRE ADMINISTRATION - PHONE & CELL	52260	52210	133.96
				BUILDING SERVICES - PHONE & CELL	52260	52400	153.92
				HIGHWAY - PHONE & CELL	52260	53100	38.47
				HIGHWAY - PHONE & CELL	52260	53100	62.74
				PARKS - TELEPHONE & INTERNET	52260	55200	86.94
				RECREATION PROGRAM - PHONE & CELL	52260	55300	86.94
				CHECK 100 139919 TOTAL FOR FUND 100:			1,447.97
01/11/2024	100	139920	B&B TRAILERS	PARKS - NEW EQUIPMENT	53950	55200	4,752.00
01/11/2024	100	139921	BRIAN & CAROLYN BUTLER	DEFERRED REVENUES - ADVANCE TAX	26310	00000	840.02
01/11/2024	100	139922	BRIAN SWITALA AND ABBY MEDDAUGH	DEFERRED REVENUES - ADVANCE TAX	26310	00000	7,529.89
01/11/2024	100	139923	CARLIN SALES CORPORATION	PARKS - NEW EQUIPMENT	53950	55200	227.19
01/11/2024	100	139924	CAROLYN & ANDREW PALMER	REAL ESTATE TAXES RECEIVABLE	12100	00000	196.48
01/11/2024	100	139925	CHARLES & HANNAH FALKNER	DEFERRED REVENUES - ADVANCE TAX	26310	00000	2,301.16
01/11/2024	100	139927	CORELOGIC, INC	REAL ESTATE TAXES RECEIVABLE	12100	00000	2,545.31
01/11/2024	100	139928	COREY OIL	HIGHWAY - FUEL	53420	53100	1,236.79
01/11/2024	100	139929	CORY AND SARAH KLOEFFLER	REAL ESTATE TAXES RECEIVABLE	12100	00000	241.13
01/11/2024	100	139930	CTACCESS	IT - SOFTWARE MAINTENANCE & UPDATES	52480	51450	25,750.00
01/11/2024	100	139931	DAN SHUDY & STEPHANIE STEGER	DEFERRED REVENUES - ADVANCE TAX	26310	00000	362.36
01/11/2024	100	139932	DANIEL & BETH HLAVA	DEFERRED REVENUES - ADVANCE TAX	26310	00000	425.33

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01/11/2024	100	139933	DAVID & JENNIFER PINE	DEFERRED REVENUES - ADVANCE TAX	26310	00000	163.42
01/11/2024	100	139934	DEAN & SUSAN ZWICK	DEFERRED REVENUES - ADVANCE TAX	26310	00000	92.50
01/11/2024	100	139936	DIVERSIFIED BENEFIT SERVICES, INC.	INSURANCE CONSULTANT	52150	51930	100.00
01/11/2024	100	139937	DOUGLAS HASKINS	DEFERRED REVENUES - ADVANCE TAX	26310	00000	1,943.53
01/11/2024	100	139938	EDWARD & BRENDA MANNING	DEFERRED REVENUES - ADVANCE TAX	26310	00000	307.25
01/11/2024	100	139939	ELLIOTTS ACE HARDWARE	HIGHWAY - OPERATING SUPPLIES	53400	53100	8.97
01/11/2024	100	139940	EMMONS BUSINESS INTERIORS	HIGHWAY - NEW EQUIPMENT	53950	53100	368.00
01/11/2024	100	139941	FIRE & POLICE SELECTION, INC	FIRE ADMINISTRATION - EMPLOYMENT	52150	52210	60.00
01/11/2024	100	139942	FIRE SERVICE INC	FIRE PROTECTIVE SERVICES - VEHICLE	52440	52230	600.00
				FIRE PROTECTIVE SERVICES - VEHICLE	52440	52230	1,250.00
				FIRE PROTECTIVE SERVICES - VEHICLE	52440	52230	1,100.00
				CHECK 100 139942 TOTAL FOR FUND 100:			2,950.00
01/11/2024	100	139944	FISCHER PROPERTIES ROUNDY CIRCLE	DEFERRED REVENUES - ADVANCE TAX	26310	00000	286.28
01/11/2024	100	139945	FORCE AMERICA, INC.	HIGHWAY - EQUIP REPAIR & MAINT	52430	53100	88.13
01/11/2024	100	139946	FORT DEARBORN CO.	DEFERRED REVENUES - ADVANCE TAX	26310	00000	1,166.11
01/11/2024	100	139947#	FORWARD TS	COURT - OPERATING SUPPLIES	53400	51200	18.05
				POLICE - OPERATING SUPPLIES	53400	52100	18.04
				RECREATION PROGRAM - OPERATING SUPPLIES	53400	55300	60.99
				CHECK 100 139947 TOTAL FOR FUND 100:			97.08
01/11/2024	100	139948	GARGI & VIGNESH SHETTY	DEFERRED REVENUES - ADVANCE TAX	26310	00000	8,083.60
01/11/2024	100	139949	GLENN GEITHMAN	REAL ESTATE TAXES RECEIVABLE	12100	00000	80.55
01/11/2024	100	139950#	GRAINGER	FIRE PROTECTIVE SERVICES - EQUIP REPAIR	52430	52230	27.24
				HIGHWAY - OPERATING SUPPLIES	53400	53100	55.28
				HIGHWAY - OPERATING SUPPLIES	53400	53100	140.22
CHECK 100 139950 TOTAL FOR FUND 100:							222.74

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01/11/2024	100	139951	GRENZ SERVICE CO. LLC	FIRE ADMINISTRATION - BUILDING REPAIRS	52410	52210	5,500.00
01/11/2024	100	139953	HEIN ELECTRIC SUPPLY	FIRE ADMINISTRATION - BUILDING REPAIRS	52410	52210	13.00
				FIRE ADMINISTRATION - BUILDING REPAIRS	52410	52210	74.41
				CHECK 100 139953 TOTAL FOR FUND 100:			87.41
01/11/2024	100	139954	HUMANE ANIMAL WELFARE SOCIETY	ANIMAL CONTROL - CONTRACTED SERVICES	52990	54150	6,699.00
01/11/2024	100	139956	JACOB & ERICA LEECH	DEFERRED REVENUES - ADVANCE TAX	26310	00000	191.71
01/11/2024	100	139957	JACOB & LAURA POLANCICH	DEFERRED REVENUES - ADVANCE TAX	26310	00000	178.08
01/11/2024	100	139958	JACOB BAKKUM	DEFERRED REVENUES - ADVANCE TAX	26310	00000	366.74
01/11/2024	100	139959	JACOB SCHLINDWEIN	REAL ESTATE TAXES RECEIVABLE	12100	00000	284.96
01/11/2024	100	139960	JAMES & CAROLINE SWIFT	DEFERRED REVENUES - ADVANCE TAX	26310	00000	7,355.00
01/11/2024	100	139961	JEAN A LANG	DEFERRED REVENUES - ADVANCE TAX	26310	00000	343.28
01/11/2024	100	139962	JOHN & CAROLINE MILLER	DEFERRED REVENUES - ADVANCE TAX	26310	00000	217.61
01/11/2024	100	139963	JOHN & JENNIFER EITEL	DEFERRED REVENUES - ADVANCE TAX	26310	00000	208.54
01/11/2024	100	139964	JONATHAN HUNDERMAN &	REAL ESTATE TAXES RECEIVABLE	12100	00000	99.67
01/11/2024	100	139965	KATHLEEN BARTA & BRYAN JACQUES	DEFERRED REVENUES - ADVANCE TAX	26310	00000	340.29
01/11/2024	100	139966	KEITH & JEAN LOUISE VANLANDEGHEM	DEFERRED REVENUES - ADVANCE TAX	26310	00000	408.39
01/11/2024	100	139967#	KMB ELECTRIC	FIRE ADMINISTRATION - BUILDING REPAIRS	52410	52210	1,200.00
				PARKS - BUILDING REPAIRS & MAINT	52410	55200	1,800.00
				PARKS - BUILDING REPAIRS & MAINT	52410	55200	3,140.20
				CHECK 100 139967 TOTAL FOR FUND 100:			6,140.20
01/11/2024	100	139968	LAKE PEWAUKEE SANITARY DISTRICT	LAKE MANAGEMENT - CONTRACTED SERVICES	52990	53640	260,945.00
01/11/2024	100	139969	LEAGUE OF WI MUNICIPALITIES	COUNCIL - DUES, MEMBERSHIPS &	53200	51110	9,616.61
01/11/2024	100	139970	LEVI QUINNEY & LISA QUINNEY	REAL ESTATE TAXES RECEIVABLE	12100	00000	145.02
01/11/2024	100	139971	LIFE-ASSIST INC	FIRE PROTECTIVE SERVICES - EMS	53450	52230	1,357.52

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01/11/2024	100	139972	LORI MATUSZAK	DEFERRED REVENUES - ADVANCE TAX	26310	00000	141.57
01/11/2024	100	139973	MADHUSUDAN TUMKUR	REAL ESTATE TAXES RECEIVABLE	12100	00000	103.33
01/11/2024	100	139974	MARGARET KAUCIC	DEFERRED REVENUES - ADVANCE TAX	26310	00000	3,077.74
01/11/2024	100	139975	MARK & ALLISON DOWNING	DEFERRED REVENUES - ADVANCE TAX	26310	00000	336.89
01/11/2024	100	139976	MARK & LINDA KLEMM	DEFERRED REVENUES - ADVANCE TAX	26310	00000	119.11
01/11/2024	100	139977	MARK BERKHAHN	DEFERRED REVENUES - ADVANCE TAX	26310	00000	144.05
01/11/2024	100	139978	MATTHEW & NATALIE CARLETON	REAL ESTATE TAXES RECEIVABLE	12100	00000	241.66
01/11/2024	100	139979	MATTHEW STEVENS	HIGHWAY - OPERATING SUPPLIES	53400	53100	75.48
01/11/2024	100	139980*#	MENARDS	HIGHWAY - OPERATING SUPPLIES	53400	53100	27.66
				HIGHWAY - OPERATING SUPPLIES	53400	53100	565.33
				HIGHWAY - OPERATING SUPPLIES	53400	53100	342.51
				PARKS - BUILDING REPAIRS & MAINT	52410	55200	29.76
				PARKS - BUILDING REPAIRS & MAINT	52410	55200	41.53
				PARKS - NEW EQUIPMENT	53950	55200	39.38
				CHECK 100 139980 TOTAL FOR FUND 100:			1,046.17
01/11/2024	100	139981	NATIONWIDE RETIREMENT SOLUTIONS	DEFERRED COMPENSATION	21570	00000	2,169.00
				DEFERRED COMPENSATION	21570	00000	2,169.00
				DEFERRED COMPENSATION	21570	00000	2,169.00
				CHECK 100 139981 TOTAL FOR FUND 100:			6,507.00
01/11/2024	100	139982	NORMAN & EVE BRUCE	DEFERRED REVENUES - ADVANCE TAX	26310	00000	132.10
01/11/2024	100	139984#	ODP BUSINESS SOLUTIONS LLC	COURT - OPERATING SUPPLIES	53400	51200	257.23
				CLERK/TREASURER - OPERATING SUPPLIES	53400	51420	263.51
				CITY HALL - JANITORIAL SUPPLIES	52400	51600	37.32
				CITY HALL - JANITORIAL SUPPLIES	52400	51600	1,279.14
				CITY HALL - JANITORIAL SUPPLIES	52400	51600	356.50
				CHECK 100 139984 TOTAL FOR FUND 100:			2,193.70

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01/11/2024	100	139985	OIL EQUIPMENT COMPANY, INC.	HIGHWAY - OPERATING SUPPLIES	53400	53100	749.71
01/11/2024	100	139986	OMAIR CHUGHTAI & TAITE KURZKA	DEFERRED REVENUES - ADVANCE TAX	26310	00000	189.02
01/11/2024	100	139987	PARTNER2LEARN, LLC	OTHER PROFESSIONAL SERVICES	52190	51510	4,110.00
01/11/2024	100	139989	PEAK SOFTWARE SYSTEMS, INC	RECREATION PROGRAM - SOFTWARE	52480	55300	3,079.75
01/11/2024	100	139990	POMP'S TIRE SERVICE, INC.	PARKS - EQUIP REPAIR & MAINT	52430	55200	875.00
01/11/2024	100	139991#	PREMIUM WATERS, INC	COURT - OPERATING SUPPLIES	53400	51200	113.99
				PARKS - OPERATING SUPPLIES	53400	55200	52.24
				CHECK 100 139991 TOTAL FOR FUND 100:			166.23
01/11/2024	100	139992	QUENTIN & MELISSA DOLL	DEFERRED REVENUES - ADVANCE TAX	26310	00000	320.20
01/11/2024	100	139993	RANDALL RZAD	DEFERRED REVENUES - ADVANCE TAX	26310	00000	145.09
01/11/2024	100	139994	RANDY WILLIAMS	DEFERRED REVENUES - ADVANCE TAX	26310	00000	65.85
01/11/2024	100	139995	REBECCA ARCHIBALD	DEFERRED REVENUES - ADVANCE TAX	26310	00000	5,451.44
01/11/2024	100	139996	REINDERS, INC.	PARKS - EQUIP REPAIR & MAINT	52430	55200	1,409.10
				PARKS - NEW EQUIPMENT	53950	55200	591.14
				CHECK 100 139996 TOTAL FOR FUND 100:			2,000.24
01/11/2024	100	139997	ROBERT & JOAN HART	DEFERRED REVENUES - ADVANCE TAX	26310	00000	184.70
01/11/2024	100	139998	ROBERT DIERINGER	DEFERRED REVENUES - ADVANCE TAX	26310	00000	3,697.26
01/11/2024	100	140000	RYAN RACHONER	REAL ESTATE TAXES RECEIVABLE	12100	00000	152.70
01/11/2024	100	140001	SHANNON RICE	DEFERRED REVENUES - ADVANCE TAX	26310	00000	72.62
01/11/2024	100	140002	SHAWNS DEER PICK UP	DEER CONTROL - CONTRACTED SERVICES	52990	54160	600.00
01/11/2024	100	140003	SOFT WATER, INC.	FIRE ADMINISTRATION - OPERATING	53400	52210	40.00
01/11/2024	100	140004	ST. BARTHOLOMEW'S EPISCOPAL CHURCH	RECREATION PROGRAM - PROGRAM EXPENSES	53430	55300	200.00
01/11/2024	100	140005	STATE OF WI COURT FINES & ASSMTS	COURT PENALTIES	45110	00000	4,641.74
01/11/2024	100	140006	STEPHEN & JULIE DECHERS	DEFERRED REVENUES - ADVANCE TAX	26310	00000	2.37

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Fund: 100 GENERAL FUND							
01/11/2024	100	140007	STEVEN & ANNETTE AFFELDT	DEFERRED REVENUES - ADVANCE TAX	26310	00000	3,465.75
01/11/2024	100	140008	Steven Lacy & Melissa Kaufmann	BEB22-0062	23173	00000	2,000.00
01/11/2024	100	140009	STRYKER SALES CORPORATION	FIRE PROTECTIVE SERVICES - EMS	53450	52230	299.94
01/11/2024	100	140010	SUE WEHNER	DEFERRED REVENUES - ADVANCE TAX	26310	00000	189.02
01/11/2024	100	140011	SUSAN BREZINSKI	DEFERRED REVENUES - ADVANCE TAX	26310	00000	81.18
01/11/2024	100	140012	TIA FIORENTINO	CLERK/TREASURER - MILEAGE	53300	51420	19.00
				CLERK/TREASURER - OPERATING SUPPLIES	53400	51420	47.48
				CHECK 100 140012 TOTAL FOR FUND 100:			66.48
01/11/2024	100	140013	TITAN PUBLIC SAFETY SOLUTIONS	IT - SOFTWARE MAINTENANCE & UPDATES	52480	51450	10,198.00
01/11/2024	100	140014	TONI GARSOMBKE & BRIAN NUETZEL	REAL ESTATE TAXES RECEIVABLE	12100	00000	176.01
01/11/2024	100	140015	TOSEEF KHAN	DEFERRED REVENUES - ADVANCE TAX	26310	00000	1,912.51
01/11/2024	100	140017	TRACY BIEKKOLA	DEFERRED REVENUES - ADVANCE TAX	26310	00000	5,350.16
01/11/2024	100	140018	TRIPLE CROWN PRODUCTS	HIGHWAY - UNIFORMS	53410	53100	88.32
				HIGHWAY - UNIFORMS	53410	53100	163.42
				HIGHWAY - SAFETY EQUIPMENT	53480	53100	1,534.70
				CHECK 100 140018 TOTAL FOR FUND 100:			1,786.44
01/11/2024	100	140019	TYLER & JAMIE KLOSE	DEFERRED REVENUES - ADVANCE TAX	26310	00000	406.04
01/11/2024	100	140020	TYLER & JAMIE KLOSE	DEFERRED REVENUES - ADVANCE TAX	26310	00000	208.72
01/11/2024	100	140021	ULINE	PARKS - GROUNDS MAINTENANCE	52420	55200	5,646.25
				PARKS - NEW EQUIPMENT	53950	55200	5,417.00
				CHECK 100 140021 TOTAL FOR FUND 100:			11,063.25
01/11/2024	100	140022	VALERIE SMITH	DEFERRED REVENUES - ADVANCE TAX	26310	00000	75.33
01/11/2024	100	140023#	VILLAGE OF PEWAUKEE	BUILDING SERVICES DUE TO VILLAGE	24400	00000	11,894.63
				PRIVATE FIRE PROTECTION PLAN REVIEW FEE	46240	00000	3,025.00

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Fund: 100 GENERAL FUND							
				CONTRACTED BUILDING INSPECTION	47380	00000	(1,248.48)
				LIBRARY - OPERATING SUPPLIES	53400	55110	74,722.91
				CHECK 100 140023 TOTAL FOR FUND 100:			<u>88,394.06</u>
01/11/2024	100	140024	WASTE MANAGEMENT	SERVICE FEES	52900	53635	4,492.32
01/11/2024	100	140026	WAUKESHA CO TREASURER	REAL ESTATE TAXES RECEIVABLE	12100	00000	2,347.05
				COURT PENALTIES	45110	00000	1,799.33
				CHECK 100 140026 TOTAL FOR FUND 100:			<u>4,146.38</u>
01/11/2024	100	140028	WILLIAM KLINGSPORN	DEFERRED REVENUES - ADVANCE TAX	26310	00000	103.20
01/11/2024	100	140030	WISCONSIN DEPARTMENT OF	EMPLOYEE SERVICES - EMPLOYMENT EXAMS	52150	51430	17.00
				Total for fund 100 GENERAL FUND			19,582,798.61

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Fund: 230 STORM WATER MANAGEMENT							
12/28/2023	100	139658*#	AECOM TECHNICAL SERVICES, INC	PROJECTS - CITY STORM WATER STUDY	58210	57340	33,214.74
				PROJECTS - CITY STORM WATER STUDY	58210	57340	14,169.57
				PROJECTS - YENCH ROAD CULVERT	58210	57355	9,061.96
				PROJECTS - WAGNER PARK POND	58200	57367	9,870.03
				PROJECTS - SPRINGDALE DRAINAGE EASEMENT	58210	57370	424.08
				CHECK 100 139658 TOTAL FOR FUND 230:			66,740.38
12/28/2023	100	139660	ALL-WAYS CONTRACTORS, INC	PROJECTS - LAKSO DRAINAGE	58200	57369	197,589.72
12/28/2023	100	139662*#	ARC DOCUMENT SOLUTIONS LLC	OPERATING SUPPLIES	53400	53650	14.73
12/28/2023	100	139664*#	BUMPER TO BUMPER HARTLAND	EQUIPMENT REPAIR & MAINT	52430	53650	73.15
12/28/2023	100	139665	BURKE TRUCK & EQUIPMENT	EQUIPMENT REPAIR & MAINT	52430	53650	1,762.59
12/28/2023	100	139670*#	DIGGERS HOTLINE	STORM SEWER MAINT - DIGGER'S HOTLINE	52201	53651	234.34
12/28/2023	100	139672*#	EHLERS	TRAINING & SEMINARS	52980	53650	100.00
12/28/2023	100	139674*#	FORWARD TS	OPERATING SUPPLIES	53400	53650	130.88
12/28/2023	100	139676*#	IMEG CORP	PROJECTS - BUSSE RD BRIDGE CULVERT	58210	57348	28,362.16
				PROJECTS - BUSSE RD BRIDGE CULVERT	58210	57348	1,940.00
				CHECK 100 139676 TOTAL FOR FUND 230:			30,302.16
12/28/2023	100	139687*#	ODP BUSINESS SOLUTIONS LLC	OPERATING SUPPLIES	53400	53650	50.38
12/28/2023	100	139696*#	SEILER INSTRUMENT & MANUFACTURING	NEW EQUIPMENT	53950	53650	1,160.00
12/28/2023	100	139700*#	TOTAL TOOL	OPERATING SUPPLIES	53400	53650	40.26
12/28/2023	100	139702	UPI, LLC	PROJECTS - YENCH ROAD CULVERT	58210	57355	135,075.11

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Fund: 230 STORM WATER MANAGEMENT							
12/28/2023	100	139703*#	WE ENERGIES	ELECTRICITY	52210	53650	554.08
				GAS FOR HEAT (NATURAL GAS)	52220	53650	248.92
				CHECK 100 139703 TOTAL FOR FUND 230:			803.00
12/28/2023	100	139705*#	WISCONSIN DEPARTMENT OF	PROJECTS - BUSSE RD BRIDGE CULVERT	58210	57348	350.91
01/05/2024	100	139718*#	A&M CLEANING SOLUTIONS	CONTRACTED JANITORIAL	52400	53650	751.40
01/05/2024	100	139724*#	AMERICAN PAPER & PACKAGING	CONTRACTED JANITORIAL	52400	53650	60.00
01/05/2024	100	139897	WAUKESHA LIME & STONE CO.	DITCH & CULVERT MAINT - CULVERT	53510	53652	1,809.35
01/11/2024	100	139919*#	AT&T MOBILITY	TELEPHONE & CELL	52260	53650	144.70
01/11/2024	100	139988	PAYNE & DOLAN	CATCH BASIN MAINT - CATCH BASIN	53510	53655	264.34
				Total for fund 230 STORM WATER MANAGEMENT			437,457.40

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Fund: 280 STATE EMS FUNDING ASSISTANCE							
01/05/2024	100	139819*#	LIFE-ASSIST INC	STATE EMS FUNDING OPERATING SUPPLIES	53400	52300	1,140.00
				Total for fund 280 STATE EMS FUNDING ASSISTANCE			1,140.00

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Fund: 420 CAPTIAL ROAD PROJECTS							
12/28/2023	100	139658*#	AECOM TECHNICAL SERVICES, INC	LEXINGTON/TACOMA	58210	57426	4,218.33
12/28/2023	100	139676*#	IMEG CORP	BUSSE RD BRIDGE	58210	57419	28,362.16
				BUSSE RD BRIDGE	58210	57419	1,940.00
				CHECK 100 139676 TOTAL FOR FUND 420:			30,302.16
12/28/2023	100	139689*#	PAYNE & DOLAN	SPICE CREEK/MEADOWBROOK FARMS #1	58210	57554	11,373.21
12/28/2023	100	139699*#	STRAND ASSOCIATES, INC	DUPLAINVILLE TRACKS TO WEYER	58210	57420	10,001.01
				APPLE TREE/PEAR TREE	58210	57561	3,601.82
				CHECK 100 139699 TOTAL FOR FUND 420:			13,602.83
12/28/2023	100	139705*#	WISCONSIN DEPARTMENT OF	BUSSE RD BRIDGE	58210	57419	350.91
12/28/2023	100	139707*#	WOLF PAVING	SPICE CREEK/MEADOWBROOK FARMS #3	58210	57556	217,685.35
				Total for fund 420 CAPTIAL ROAD PROJECTS			277,532.79

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Fund: 480 DPW BUILDING							
12/21/2023	100	139568	ABSOLUTE CONSTRUCTION	DPW BUILDING	58100	57700	36,344.94
01/11/2024	100	139980*#	MENARDS	DPW BUILDING	58100	57700	106.30
				DPW BUILDING	58100	57700	11.16
				DPW BUILDING	58100	57700	120.40
				DPW BUILDING	58100	57700	348.63
				DPW BUILDING	58100	57700	54.99
				CHECK 100 139980 TOTAL FOR FUND 480:			641.48
				Total for fund 480 DPW BUILDING			36,986.42

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Fund: 490 CAPTIAL EQUIPMENT							
01/05/2024	100	139879	SIGNS & LINES BY STRETCH	FIRE ADMINISTRATION - CAPITAL EQUIPMENT	58100	57220	610.28
				Total for fund 490 CAPTIAL EQUIPMENT			610.28

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Fund: 600 WATER UTILITY							
12/19/2023	100	139566*	POSTMASTER	BILLING	53300	00903	1,300.00
12/22/2023	100	1470 (E) *#	MUTUAL OF OMAHA	BENEFITS - LIFE INSURANCE	51340	00926	82.16
				BENEFITS - DISABILITY INSURANCE	51350	00926	111.57
				CHECK 100 1470 (E) TOTAL FOR FUND 600:			193.73
12/23/2023	100	1471 (E) *	AT&T	SCADA/TELEMETARY	52100	00950	86.93
12/28/2023	100	139659*	ALL CITY COMMUNICATIONS INC.	ANSWERING SERVICE	52331	00921	50.87
12/28/2023	100	139663*#	AT&T MOBILITY	TELEPHONE	52330	00921	178.25
				SCADA/TELEMETARY	52100	00950	25.46
				CHECK 100 139663 TOTAL FOR FUND 600:			203.71
12/28/2023	100	139666*#	CINTAS CORP	UNIFORMS			** VOIDED **
				UNIFORMS			** VOIDED **
12/28/2023	100	139668	CONLEY MEDIA	REGULATORY & COMMISSION EXPENSE	52381	00928	183.12
12/28/2023	100	139670*#	DIGGERS HOTLINE	DIGGERS HOTLINE	52440	00950	234.33
12/28/2023	100	139673*	EZ MAIL	BILLING	53300	00903	150.00
12/28/2023	100	139675	HAWKINS, INC.	CHEMICALS	53200	00631	2,592.83
12/28/2023	100	139682*#	MENARDS	TRANSPORTATION EXPENSE - REPAIR &	52400	00933	24.99
12/28/2023	100	139692	PUBLIC SERVICE COMMISSION OF WI	PSC WATER RCA STUDY	52382	00928	621.23
12/28/2023	100	139694	RUEKERT & MIELKE, INC.	PSC WATER RCA STUDY	52382	00928	85.00
				PSC WATER RCA STUDY	52382	00928	4,738.45
				CHECK 100 139694 TOTAL FOR FUND 600:			4,823.45

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Fund: 600 WATER UTILITY							
12/28/2023	100	139699*#	STRAND ASSOCIATES, INC	WATER FACILITIES PLAN	52399	00923	980.00
12/28/2023	100	139700*#	TOTAL TOOL	BUILDING AND GROUNDS	52310	00623	90.75
01/05/2024	100	139740*#	CINTAS CORP	UNIFORMS	53410	00641	51.25
				UNIFORMS	53410	00641	51.25
				CHECK 100 139740 TOTAL FOR FUND 600:			102.50
01/05/2024	100	139763	FERGUSON WATERWORKS #1476	REPAIRS - HYDRANTS	52400	00654	1,650.00
01/05/2024	100	139823*	LITHO-CRAFT	BILLING	53300	00903	248.50
01/05/2024	100	139840	MIDWEST TESTING LLC	REPAIRS - METERS	52400	00653	1,545.00
01/05/2024	100	139855*	PEACHTREE DATA	BILLING	53300	00903	37.50
01/05/2024	100	139882#	STRAND ASSOCIATES, INC	PFAS TREATMET	12818	00107	4,510.00
				OUTSIDE PROFESSIONAL SERVICES-ENGINEER	52380	00923	5,020.00
				CHECK 100 139882 TOTAL FOR FUND 600:			9,530.00
01/11/2024	100	139913*	ALL CITY COMMUNICATIONS INC.	ANSWERING SERVICE	52331	00921	59.57
01/11/2024	100	139919*#	AT&T MOBILITY	OFFICE EQUIPMENT WATER	18572	00391	1,582.80
01/11/2024	100	139926*#	CINTAS CORPORATION #184	UNIFORMS	53410	00641	51.25
				UNIFORMS	53410	00641	51.25
				TRANS & DIST OPS-UNIFORMS	53410	10665	51.25
				CHECK 100 139926 TOTAL FOR FUND 600:			153.75
01/11/2024	100	139952	HAWKINS, INC.	CHEMICALS	53200	00631	3,956.02
01/11/2024	100	139955	HYDROCORP	CROSS CONNECTION INSPECTION	52381	00641	1,288.00
01/11/2024	100	139980*#	MENARDS	REPAIRS AND MAINTENANCE	52400	00635	34.89

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Fund: 600 WATER UTILITY				REPAIRS AND MAINTENANCE	52400	00635	6.84
				CHECK 100 139980 TOTAL FOR FUND 600:			41.73
01/11/2024	100	139983	NORTHERN LAKE SERVICE, INC	WATER TESTING	52310	00632	1,517.20
01/11/2024	100	139999	RUEKERT & MIELKE, INC.	PSC WATER RCA STUDY	52382	00928	8,197.50
01/11/2024	100	140016*#	TOTAL TOOL	OPERATING SUPPLIES & EXPENSES	53400	00602	168.87
				OPERATING SUPPLIES & EXPENSE	53400	00623	168.87
				OPERATING SUPPLIES AND EXPENSE	53400	00641	168.87
				CHECK 100 140016 TOTAL FOR FUND 600:			506.61
01/11/2024	100	140025	WATER REMEDIATION TECHNOLOGY	TREATMENT MAINT-WRT RADIUM TREATMENT	52900	10652	4,230.35
01/11/2024	100	140027*	WI RURAL WATER ASSOCIATION	ADMIN & GEN OPS-UTILITY MEMB & CONT	53200	10930	307.50
				Total for fund 600 WATER UTILITY			46,490.47

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Fund: 650 SEWER UTILITY							
12/19/2023	100	139566*	POSTMASTER	BILLING	53300	00903	1,300.00
12/22/2023	100	1470 (E) *#	MUTUAL OF OMAHA	BENEFITS - LIFE INSURANCE	51340	00926	44.24
				BENEFITS - DISABILITY INSURANCE	51350	00926	60.08
				CHECK 100 1470 (E) TOTAL FOR FUND 650:			104.32
12/23/2023	100	1471 (E) *	AT&T	SCADA/TELEMETARY	52100	00950	86.93
12/28/2023	100	139659*	ALL CITY COMMUNICATIONS INC.	ANSWERING SERVICE	52331	00921	50.88
12/28/2023	100	139663*#	AT&T MOBILITY	TELEPHONE	52330	00921	178.26
				SCADA/TELEMETARY	52100	00950	25.47
				CHECK 100 139663 TOTAL FOR FUND 650:			203.73
12/28/2023	100	139666*#	CINTAS CORP	Uniforms & Protective Equipment			** VOIDED **
				Uniforms & Protective Equipment			** VOIDED **
12/28/2023	100	139670*#	DIGGERS HOTLINE	DIGGERS HOTLINE	52440	00950	234.33
12/28/2023	100	139673*	EZ MAIL	BILLING	53300	00903	150.00
12/28/2023	100	139677	J. MILLER ELECTRIC	MAINTENANCE OF PUMPING EQUIPMENT	52400	01832	360.83
12/28/2023	100	139682*#	MENARDS	TRANSPORTATION EXPENSE - REPAIR &	52400	00933	24.99
				MAINTENANCE OF PUMPING EQUIPMENT	52400	01832	41.94
				CHECK 100 139682 TOTAL FOR FUND 650:			66.93
12/28/2023	100	139684	NAPA	MAINTENANCE OF GENERAL PLANT STRUCTURE	52400	01834	39.97
12/28/2023	100	139686	NORTHWESTERN ELEVATOR II CO, LLC	MAINTENANCE OF GENERAL PLANT STRUCTURE	52400	01834	3,376.00
12/28/2023	100	139699*#	STRAND ASSOCIATES, INC	Gun Club Sewer Study	12831	00107	5,745.00
				OPS - SEWER STUDIES	52100	01850	500.00

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Fund: 650 SEWER UTILITY							
CHECK 100 139699 TOTAL FOR FUND 650:							6,245.00
12/28/2023	100	139700*#	TOTAL TOOL	MAINTENANCE OF GENERAL PLANT STRUCTURE	52400	01834	90.76
01/05/2024	100	139740*#	CINTAS CORP	Uniforms & Protective Equipment	53410	01827	51.25
				Uniforms & Protective Equipment	53410	01827	51.25
CHECK 100 139740 TOTAL FOR FUND 650:							102.50
01/05/2024	100	139823*	LITHO-CRAFT	BILLING	53300	00903	248.50
01/05/2024	100	139855*	PEACHTREE DATA	BILLING	53300	00903	37.50
01/11/2024	100	139913*	ALL CITY COMMUNICATIONS INC.	ANSWERING SERVICE	52331	00921	59.58
01/11/2024	100	139919*#	AT&T MOBILITY	OFFICE EQUIP - SEWER	18572	01372	1,582.80
01/11/2024	100	139926*#	CINTAS CORPORATION #184	Uniforms & Protective Equipment	53410	01827	51.25
				Uniforms & Protective Equipment	53410	01827	51.25
				Uniforms & Protective Equipment	53410	01827	51.25
CHECK 100 139926 TOTAL FOR FUND 650:							153.75
01/11/2024	100	139935	Demlang Builders, Inc.	Sewer Connection	46314	01623	3,377.00
01/11/2024	100	140016*#	TOTAL TOOL	SUPPLIES AND EXPENSES	53400	01827	168.89
01/11/2024	100	140027*	WI RURAL WATER ASSOCIATION	ADMIN & GEN OPS- MEMBERSHIP & CONT EDU	53200	10930	307.50
01/11/2024	100	140029	WILLIAM/REID	MAINTENANCE OF PUMPING EQUIPMENT	52400	01832	116.34
Total for fund 650 SEWER UTILITY							18,464.04

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Fund: 800 CEMETERY							
12/28/2023	100	139699*#	STRAND ASSOCIATES, INC	CAPITAL - ROAD RECONSTRUCTION	58200	54910	1,908.49
01/11/2024	100	139943	FIRST AMERICAN TITLE INSURANCE	CEMETERY - OPERATING SUPPLIES	53400	54910	150.00
				Total for fund 800 CEMETERY			2,058.49
TOTAL - ALL FUNDS							20,403,538.50

'*'-INDICATES CHECK DISTRIBUTED TO MORE THAN ONE FUND
'#'-INDICATES CHECK DISTRIBUTED TO MORE THAN ONE DEPARTMENT